

CHICORY VILLAGE SUBDIVISION

TOD Corridor Master Plan

Central Point, Oregon
Bob Fellows Construction, LLC

Prepared by:



CSA Planning, Ltd
4497 Brownridge Terrace
Suite 101
Medford, OR 97504
Telephone 541.779.0569
Fax 541.779.0114
CSAplanning.net

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Central Point, Oregon

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TOD Corridor Master Plan

1. Introduction

The Chicory Village Subdivision is proposed as a 21-unit subdivision that will infill a 3.64 acre area that was annexed and zoned TOD-LMR in 2017. The objective of the subdivision is to provide reasonably priced, entry-level housing for Central Point.

- a. **Duration of Master Plan:** The project is to be constructed in two phases and is planned to be completed within 5 years.
- b. **Site Location:** The subject property is located the east side of Chicory Lane, between the east of the terminus of Lindsay Court and the Central Oregon and Pacific right-of-way. The property is identified as Tax Lots 8300 and 8400 in Township 37 South, Range 02 West (W.M.), Section 11C. The current site addresses are 3428 and 3470 Chicory Lane, Central Point, OR. See, Exhibit 1.
- c. **Land Use and Density:** In the TOD-LMR zone, two types of housing are required for developments with between 16 and 40 units. With 21 units proposed, two types or housing types are included - 19 detached single-family units and 2 attached row house units. The required density for the zone is 6 and 12 units per acre.

Density for the property can be calculated once the net residential area is determined. Net acre is to be calculated as follows:

- (f) Net acre equals the area remaining after deducting environmental lands, exclusive employment areas, exclusive civic areas and right-of-way.

Table below shows how the net acreage for the subject property is calculated:

SUBJECT PROPERTY ACREAGE		
Acreage Type	Net Acres	Percent of gross acres
Residential Area	2.21	61%
Right-of Way/Open Space	1.43	39%
Gross Area	3.64	

The proposed subdivision includes 21 units on 2.21 net acres. 21 divided by 2.21 equals 9.5~ 10 units per acre, meeting the minimum standard for this zone which is 6 to 12 units per acre.

- d. **Adjacent Master Plans:** The subject property is covered by the Central Point TOD Corridor overlay and, as such, a master plan is required for any development that is over 2 acres in size. To the north is the 35.73 acre Snowy Butte Station transit-oriented development which is the only master planned area within 100 feet of the subject property. Much of the Snowy Butte development has been constructed, including the lots immediately to the north of the subject property.

2. Site Analysis

The subject property is a long, narrow rectangular piece that stretches lengthwise from west to east. It is comprised of two parcels, each of which currently contains a residence and related accessory structures. These houses and accessory



structures are in poor condition and are proposed to be removed to make way for the development. Chicory Lane abuts the west property boundary and the western half of the southern boundary. S. Haskell Street terminates at the northeastern corner of the property. The eastern property boundary is angled and parallel to the abutting Central Oregon and Pacific rail line. This angle becomes an issue for layout of the easternmost lots, which is discussed further under Site Design.

- a. **Master Utility Plan:** All public utilities are available to the subject property per review by the Applicant's engineer, John Jensen, PE. See Appendix 1 for his report and maps showing locations of underground utilities around the subject property.
- b. **Adjacent land Use Plan:** The subject property is surrounded on two and a half sides by low density single-family subdivisions. See, Figure 1. Site Analysis Map. Surrounding properties are described below:

North: To the north is the Snowy Butte Station Transit Oriented Development that includes a wide variety of housing types and civic uses. Abutting the subject property are six lots that are part of the Snowy Butte Station TOD. They were constructed in 2009 and 2010, range in size from 7,300 to 7,950 square feet and contain single-family houses with detached ADU units. Also part of the Snowy Butte plan are two parks that abut the subject property at the northeastern corner. One is an approximately 9,890 square foot park area abutting the subject property's north boundary on the west side of S. Haskell Street. On the east side of S. Haskell Street, between the street and the rail line, is a linear open space area.

East: The eastern property boundary abuts the Central Oregon and Pacific Railroad right-of-way on the east. The rail lines are located approximately 65 to 70 feet east of S. Haskell Street.

South: The eastern half of the southern boundary abuts one parcel that is outside the City Limits and zoned General Industrial. This parcel however is used as rural residential and Central Point has the property tentatively designated TOD Corridor and zoned TOD MMR. This parcel contains a residence, with the remainder primarily vacant and appears to be used for pasture. The western half of the southern boundary abuts the Chicory Lane alley.

West: To the west is a residential subdivision with medium-size lots ranging from .18 to .30 acres in size with single-family houses of various ages, built out since the mid-1970's.

3. Transportation and Circulation

Street patterns in this area have already been established by the surrounding developments. S. Haskell Street, a Residential Collector Street, abuts the northeastern corner of the property. Lindsey Court, a Minor Local Street, abuts the center of the western boundary. Applicant proposes to construct an extension of S. Haskell Street to the proposed extension of Lindsey Court as part of the project, subject to standard SDC reimbursements for construction and dedication of collector streets in the City of Central Point. For the section of S. Haskell Street south of Lindsey Court, Applicant will dedicate the right-of-way for extension of S. Haskell Street to facilitate a future a construction project by the City of Central Point. Such dedication will occur at the time of development of Phase II. If the City desires to have the Applicant construct the portion of S. Haskell Street south of Lindsey Court, the same will be by separate cost-sharing agreement that may be

reached between the parties. No improvements to the Chicory Lane alley on the south boundary of the project are proposed or necessary to serve the project

Access to the site to/from the north can occur either via S. Haskell Street or from Lindsey Court to Rochelle Court or from Chicory Lane south briefly to connect via Timothy Street and Glenn Way. Access to the site to/from the south can occur via Chicory Lane and Timothy Street to either Malabar or Farnsworth and then down to Beall Lane. Access is ultimately planned to connect to Beall Lane via S Haskell Street but this requires extension through an intervening property to the south.

A transportation analysis was performed as part of the zone change completed within the last year. The proposed development is consistent with that TIA, and therefore, no further transportation analysis is required.

4. Site Plan

The site design includes 19 subdivision lots lining the north and south sides of a one block extension of Lindsey Court. At the eastern end, Lindsey Court angles north to meet the extension of S. Haskell Street at a right angle. This creates a triangular area south of Lindsey Court which exceeds the typical lot depth for this subdivision. To make efficient use of this area, 2 narrower lots, Lots 20 and 21, are proposed to accommodate attached row houses with a common driveway from Lindsey Court, *See, Figure 2. See, Appendix 2 - Demonstration of Compliance with Applicable Development Standards* for a detailed review of how the site design meets the Central Point Zoning Code TOD Corridor standards Section 17.65.050.

The subdivision is proposed in two contiguous phases. Phase 1 includes the western part of the property with Lots 1 to 8. Phase 2 includes the remainder of the property with Lots 9 to 21.

5. Recreation and Open Space Plan

The project design proposes to provide the open space required for projects in the TOD Corridor zone by extending the existing linear open space buffer abutting the northeast corner of the subject property south, between S. Haskell Street and the rail line, to the southern boundary. The TOD Corridor requires 400 square feet of open space or park per unit be set aside in developments in that area. With 21 residential units at 400 sf per unit, a minimum of 8,400 square feet are required. 10,660 square feet are proposed, with approximately 2,200 serving as a bioswale detention for the development.

This linear open space will provide a visual buffer between the subdivision and the rail lines, as well as continuation of the sidewalk south for the use of recreational walkers. Two benches are proposed for this short stretch of open space. *See, Figure 2.* The open space will be dedicated to the City along with the street right-of-ways. *See, Appendix 2* for how the plan meets CPZC 16.75.060.

6. Building Design Concepts

The Applicant has developed three conceptual design options for detached house designs, ranging from 1,880 to 2,089 square feet. All include 3 bedrooms, 2½ baths, a single-car garage and feature a front porch and covered back patio. *See, Sheets 1 to 3 in Appendix 3.*

A fourth concept plan has been developed for the attached row houses which includes two 1,872 square foot units. Similar to the detached units, each unit includes 3 bedrooms, 2½ baths, a single-car garage and feature a front porch and covered back patio. Designs provided are tentative, but the final designs are expected to be the same or very similar. *See, Sheets 4 and 5 in Appendix 3.*

The proposed house designs are intended to be sympathetic to the surrounding residences. *See*, Appendix 2 for how the plan meets CPZC 16.75.060.

Final house designs may deviate from the particular designs presented in the Master Plan, but all will comply with all applicable regulatory requirements and will be consistent with the concept presented in the master plan to provide attractive entry level housing within this small infill project.

7. Transit Plan

No bus service currently serves the subject property. There has been discussion that a future bus line will run down S. Haskell Street, but no plans are imminent. The extension of S. Haskell Street will be available for use by transit if needed in the future. In residential areas, all that is really required is a small shelter and pad which could be poured in the planter strip adjacent to any future bus stop location on the subject property.

8. Environmental Plan

The subject property has little in the way of topography or environmental features. Sloping very gently to the northeast, the subject property is essentially level.

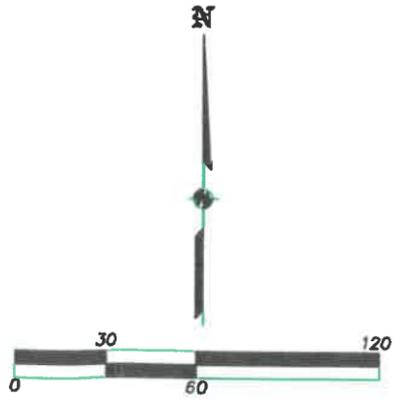
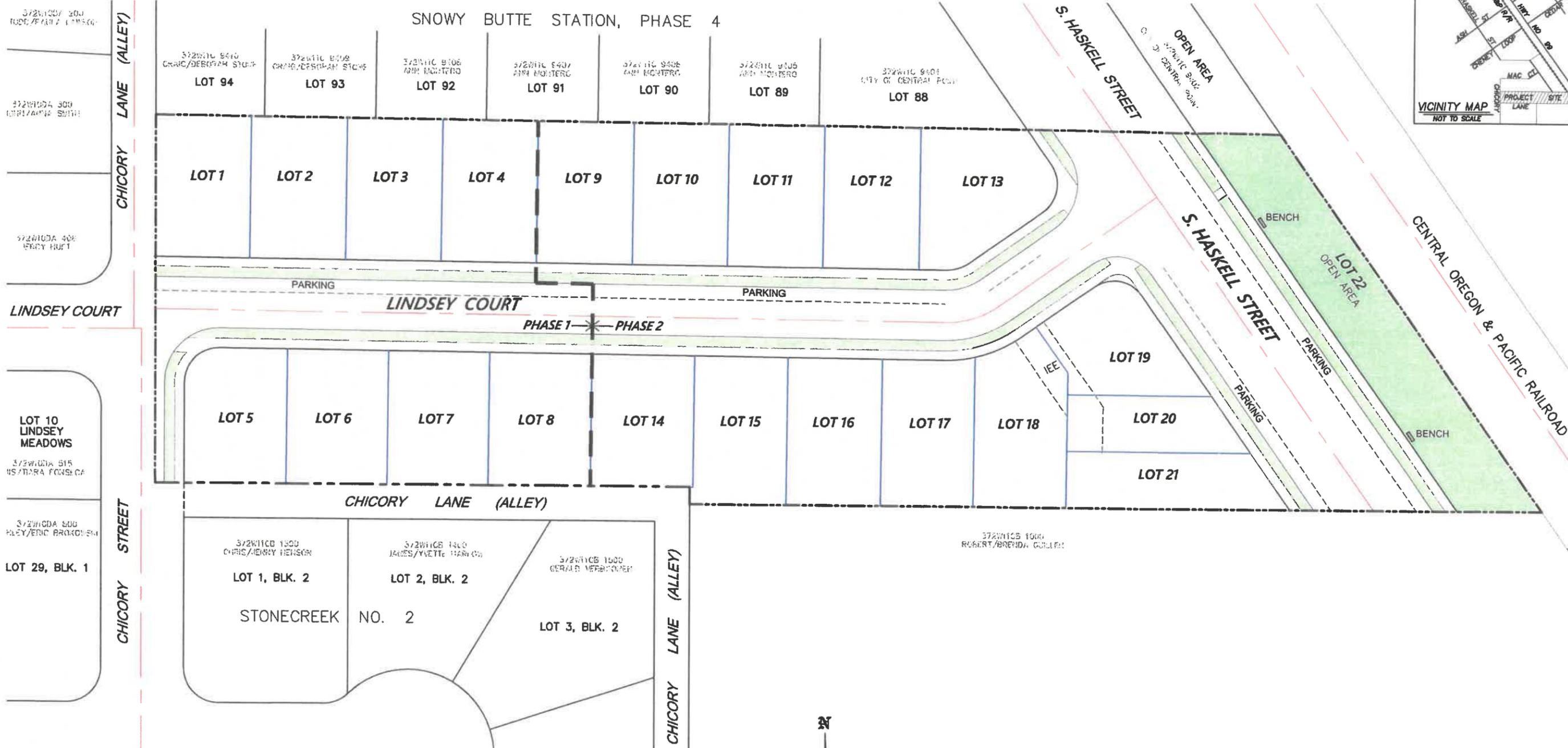
There are no officially mapped wetlands on the site. However, a preliminary wetlands investigation was performed. A potential wetland is being fed by poorly designed storm drainage facilities. There is an outflow pipe from the neighborhood to the south that was intended to sheet flow to existing culvert pipes under the rail line.

Applicant understands that the City is aware of this issue and is actively working on upgrading those culverts and re-routing the storm drainage to them. This area should be reassessed following City action to pipe existing storm drainage outflow locations and make continuous connection to the City's system. Following these storm drainage actions, if there are any wetlands in the area, then the City could incorporate on-site mitigation as a feature of the proposed linear park area between S. Haskell Street and the rail line. *See*, Figures 1 and 2.

Figure 1. Site Analysis Map

Figure 2. Site Plan - Master Plan

FIGURE 2



MASTER PLAN - CHICORY VILLAGE SUBDIVISION
 BOB FELLOWS CONSTRUCTION, LLC

(19) 4,500+SF LOTS PLUS 2 ROWHOUSES = 21 TOTAL UNITS

372W11C TL'S 8300 & 8400 SCALE: 1:60 DATE: 5/22/18

APPENDIX 1. UTILITY MASTER PLAN

- a. *Public Utility Analysis - Jensen & Associates Consulting Civil Engineers, May 3, 2018*
- b. *Central Point Water Line map*
- c. *Central Point Storm Drain System map*
- d. *RVSS Sanitary Sewer Map*

APPENDIX 2. DEMONSTRATION OF COMPLIANCE WITH APPLICABLE STANDARDS

Title 17 Zoning

Chapter 17.65 TOD CORRIDOR MASTER PLAN

17.65.060 Land use--TOD corridor.

Four special zone district categories are applied in the Central Point TOD corridor. The characteristics of these zoning districts are summarized in subsections A through D of this section.

A. Residential (TOD).

1. LMR--Low Mix Residential. This is the lowest density residential zone in the district. Single-family detached residences are intended to be the primary housing type, however attached single-family, and lower density multifamily housing types are also allowed and encouraged. The housing types within this zone are intended to support pedestrian-friendly access beyond five hundred feet of the primary transit route.

Compliance Discussion: Proposed subdivision is comprised primarily of detached single-family residences. Two attached row houses are also proposed. Subject property is located more than 500 feet from the nearest transit route and is connected to that route by sidewalks. Project complies.

17.65.070 Zoning regulations--TOD corridor.

A. Permitted Uses. Permitted uses in Table 4 are shown with a "P." These uses are allowed if they comply with the applicable provisions of this title. They are subject to the same application and review process as other permitted uses identified in this title.

B. Limited Uses. Limited uses in Table 4 are shown with an "L." These uses are allowed if they comply with the specific limitations described in this chapter and the applicable provisions of this title. They are subject to the same application and review process as other permitted uses identified in this title.

C. Conditional Uses. Conditional uses in Table 4 are shown with a "C." These uses are allowed if they comply with the applicable provisions of this title. They are subject to the same application and review process as other conditional uses identified in this title.

**Table 4
TOD Corridor Land Uses**

Use Categories	Zoning District
Residential	LMR
Dwelling, Single-Family	
Large and standard lot	P
Zero lot line, detached	P
Attached row houses	P

Use Categories	Zoning District
Residential	LMR
Dwelling, Multifamily	
Multiplex, apartment	P
Congregate (senior)r housing	P
Open Space	
Parks and Open Space	P

P--Permitted use.

Compliance Discussion: All proposed uses are permitted within the zone. Proposed subdivision is comprised primarily of detached Single-Family Dwellings on Standard Lots. Two attached row houses are also proposed to meet the requirement for a second housing type. Both types of residential units are permitted in the LMR district as shown in Table 4. A linear open space area along the railroad frontage is also included as part of the project. Parks are also allowed in the LMR district as shown in Table 4. Project complies.

D. Density. The allowable residential density and employment building floor area are specified in Table 5.

Compliance Discussion: In the TOD-LMR zone, two types of housing are required for developments with between 16 and 40 units. With 21 units proposed, two types or housing types are included - 19 detached single-family units and 2 attached row house units. The required density for the zone is 6 and 12 units per acre. Density for the property can be calculated once the net residential area is determined. Net acre is to be calculated as follows:

- (f) Net acre equals the area remaining after deducting environmental lands, exclusive employment areas, exclusive civic areas and right-of-way.

Table below shows how the net acreage for the subject property is calculated:

SUBJECT PROPERTY ACREAGE		
Acreage Type	Net Acres	Percent of gross acres
Residential Area	2.21	61%
Right-ofWay/Parks	1.43	39%
Gross Area	3.64	

The subdivision includes 21 units on 2.21 net acres. 21 divided by 2.21 equals 9.5~10 units per acre. Project complies with the minimum standard for this zone which is 6 to 12 units per acre.

Table 5
TOD Corridor Zoning Standards

Standard	Zoning District	Complies?
	LMR	
Density--Units Per Net Acre (f)		
Maximum	12	Meets standard with 10 units/ acre
Minimum	6	
Dimensional Standards		
Minimum Lot or Land Area/Unit		
Standard single-family	3,000 SF	Yes
Attached row houses	2,000 SF	Yes
Average Minimum Lot or Land Area/Unit		
Standard single-family	4,500 SF	Yes
Attached row houses	2,500 SF	Yes
Minimum Lot Width		
Standard single-family	50'	Yes
Attached row houses	24'	Yes
Minimum Lot Depth	50'	Yes
Building Setbacks		
Front (min./max.)	10'/15'	Can & will comply
Side (between bldgs.) (detached/attached)	5' detached 0' attached (a) (c)	Can & will comply
Corner (min./max.)	5'/10'	Can & will comply
Rear	15'	Can & will comply
Garage Entrance	(d)	Can & will comply
Maximum Building Height	35'	Current designs are ~ 25' Can & will comply
Maximum Lot Coverage (g)	80%	Can & will comply
Minimum Landscaped Area (i)	20% of site area	Can & will comply
Housing Mix		
Required housing types as listed under Residential in Table 1.	16--40 units in development: 2 housing types.	21 units Total > 19 Single-family units > 2 Attached row house units

- (a) The five-foot minimum also applies to the perimeter of the attached unit development.
- (c) Setback required is ten feet minimum between units when using zero lot line configurations.
- (d) Ten feet behind building facade facing street.
- (e) Garage entrance shall not protrude beyond the face of the building.
- (f) Net acre equals the area remaining after deducting environmental lands, exclusive employment areas, exclusive civic areas and right-of-way.
- (g) Lot coverage refers to all impervious surfaces, including buildings and paved surfacing.
- (i) Landscaped area shall include living ground cover, shrubs, trees, and decorative landscaping material such as bark, mulch or gravel. No pavement or other impervious surfaces are permitted except for pedestrian pathways and seating areas.



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E. Dimensional Standards. The dimensional standards for lot size, lot dimensions, building setbacks, and building height are specified in Table 5.

Compliance Discussion: Proposed subdivision lots all meet the dimensional standards for the LMR district as defined in Table 5 above. Project complies.

F. Development Standards.

1. Housing Mix. The required housing mix for the TOD district is shown in Table 5.
2. Accessory Units. Accessory units are allowed as indicated in Table 1. Accessory units shall meet the following standards:
 - a. A maximum of one accessory unit is permitted per lot;
 - b. The primary residence and/or the accessory unit on the lot must be owner-occupied;
 - c. An accessory unit shall have a maximum floor area of eight hundred square feet;
 - d. The applicable zoning standards in Table 2 shall be satisfied.

Compliance Discussion: Proposed subdivision is comprised of 19 detached Single-Family Dwellings and two attached Row Houses. Per Table 5- Housing Mix, 2 housing types are required in subdivisions containing 16-40 units. As 2 types of units are proposed in this 21-unit subdivision, the project complies. No accessory units are proposed. Project complies.

3. Parking Standards. Parking standards shall be as specified in Section [17.65.050\(F\)\(3\)](#).

From Section [17.65.050\(F\)\(3\)](#):

3. Parking Standards. The off-street parking and loading requirements in Chapter [17.64](#) shall apply to the TOD district and TOD corridor, except as modified by the standards in Table 3 of this section.

- a. Fifty percent of all residential off-street parking areas shall be covered. Accessory unit parking spaces are not required to be covered.
- b. Parking standards may be reduced when transit service is provided in the TOD district and TOD corridor and meets the following conditions:
 - i. Parking standards may be reduced up to twenty-five percent when transit service is provided in the TOD district and TOD corridor.
 - ii. Parking standards may be reduced up to fifty percent when transit service is provided in the TOD district and TOD corridor and when bus service includes fifteen-minute headways during the hours of seven to nine a.m. and four to six p.m.
- c. Bicycle parking standards in Chapter [17.64](#) shall not be reduced at any time.
- d. Shared parking easements or agreements with adjacent property owners are encouraged to satisfy a portion of the parking requirements for a particular use where compatibility is shown. Parking requirements may be reduced by the city when reciprocal agreements of shared parking are recorded by adjacent users.

**Table 3
TOD District and Corridor Parking Standards**

Use Categories	Minimum Required Parking	Complies?
Residential		
Dwelling, Single-Family Large and standard lot Zero lot line, detached Attached row houses	2 spaces per unit.	Yes



Compliance Discussion: All proposed units will have a 1-car garage (covered parking), plus a driveway that provides parking space for at least one car. No transit service is available to this part of the TOD Corridor. Project complies.

Chapter 17.66

APPLICATION REVIEW PROCESS FOR THE TOD DISTRICT AND CORRIDOR

17.66.040 Parks and open spaces.

Common park and open space shall be provided for all residential development within a TOD district or corridor as per Section [17.67.060](#).

Compliance Discussion: The project design proposes to extend the existing linear open space buffer abutting the northeast corner of the subject property south to the southern boundary between S. Haskell Street and the rail line. See section 17.67.060.

Chapter 17.67

DESIGN STANDARDS – TOD CORRIDOR

17.67.050 Site design standards.

The following standards and criteria shall be addressed in the master plan, land division, and/or site plan review process:

A. Adjacent Off-Site Structures and Uses.

1. All off-site structures, including septic systems, drain fields, and domestic wells (within one hundred feet) shall be identified and addressed in the master plan, land division, or site plan process in a manner that preserves and enhances the livability and future development needs of off-site structures and uses consistent with the purpose of the TOD district and as necessary to improve the overall relationship of a development or an individual building to the surrounding context.
2. Specific infrastructure facilities identified on site in the master plan, land division, and/or site plan shall comply with the underground utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 400, Storm Water Sewer System and, more specifically, Section 420.10.02, Ground Water Control Plan, in order to safeguard the water resources of adjacent uses.

Compliance Discussion: The storm sewer for Phase 1 will be connecting to the existing line east of the property. See Appendix 1. Phase 2, the western half of the project, will be connecting to the existing storm lines that run under the Central Oregon and Pacific rail lines. Design and construction of connecting storm lines will be coordinated with Public Works upgrades to these lines.

B. Natural Features.

1. Buildings should be sited to preserve significant trees.
2. Buildings should be sited to avoid or lessen the impact of development on environmentally critical areas such as steep slopes, wetlands, and stream corridors.
3. Whenever possible, wetlands, groves, and natural areas should be maintained as public preserves and as open space opportunities in neighborhoods.

Compliance Discussion: No significant trees are located on the property. One unmapped potential wetland area is being caused by an un-piped section of the municipal storm water system where S. Haskell Street is to be extended. Once the storm water is

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continuously piped the potential wetland can be reassessed and if any wetland exists at that point, it could be mitigated on-site in the new linear park as an amenity of the park.

C. Topography.

1. Buildings and other site improvements should reflect, rather than obscure, natural topography.

Compliance Discussion: Topography is not an issue on this site. The site slopes gently down from the center to the northeast and the northwest and has no features that will impact site improvements.

D. Solar Orientation.

1. The building design, massing and orientation should enhance solar exposure for the project, taking advantage of the climate of Central Point for sun-tempered design.
2. Where possible, the main elevation should be facing within twenty-five degrees of due south.
3. In residential developments, the location of rooms should be considered in view of solar exposure, e.g., primary living spaces should be oriented south, but a west facing kitchen should be avoided as it may result in summer overheating.
4. Outdoor spaces should be strategically sited for solar access and the cooling summer winds.
5. Shadow impacts, particularly in winter, on adjacent buildings and outdoor spaces should be avoided.

Compliance Discussion: Houses have limited options for their orientation due to the street direction and resulting lot layout. All plans have limited window openings on their east and north facades.

E. Existing Buildings on the Site.

2. New buildings proposed for existing neighborhoods with a well-defined and desirable character should be compatible with or complement the architectural character and siting pattern of neighboring buildings.

Compliance Discussion: The proposed residence designs are complementary to the surrounding neighborhoods in style, with inviting porches and varied roof lines.

H. Adjoining Uses and Adjacent Services.

5. Group mailboxes are limited to the number of houses on any given block of development. Only those boxes serving the units may be located on the block. Multiple units of mailboxes may be combined within a centrally located building of four walls that meets the design guidelines for materials, entrance, roof form, windows, etc. The structure must have lighting both inside and out.

Compliance Discussion: The Applicant is working with the US Postal Service to determine whether group mailboxes will be required or if delivery will be by house. If group mailboxes are required, they will be located mid-block to be easily accessible to the residents of the block.

I. Transitions in Density.

1. Higher density, attached dwelling developments shall minimize impact on adjacent existing lower density, single-family dwelling neighborhoods by adjusting height, massing and materials and/or by providing adequate buffer strips with vegetative screens.
2. Adequate buffer strips with vegetative screens shall be placed to mitigate the impact of higher density development on adjacent lower density development.
5. Dwelling types in a TOD district or corridor shall be mixed to encourage interaction among people of varying backgrounds and income levels.

Compliance Discussion: This subdivision is surrounded on two and a half sides by low density housing. The remaining sides include the rail line and the rural residentially used property to the south. No buffers are required of this property.

To meet the mixed dwelling type requirement for the TOD Corridor, the development includes both detached single-family residences and attached row houses.



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K. Landscaping.

5. Street Trees. Street trees shall be required along both sides of all public streets with a spacing of twenty feet to forty feet on center depending on the mature width of the tree crown, and planted a minimum of two feet from the back of curb. Trees in the right-of-way or sidewalk easements shall be approved according to size, quality, and tree well design, if applicable, and irrigation shall be required. Tree species shall be chosen from the city of Central Point approved street tree list.

Compliance Discussion: Street trees will be planted to meet Central Point standards.

L. Lighting.

1. Minimum Lighting Levels. Minimum lighting levels shall be provided for public safety in all urban spaces open to public circulation.

a. A minimum average light level of one and two-tenths footcandles is required for urban spaces and sidewalks.

b. Metal-halide or lamps with similar color, temperature and efficiency ratings shall be used for general lighting at building exteriors, parking areas, and urban spaces. Sodium-based lamp elements are not allowed.

c. Maximum lighting levels should not exceed six footcandles at intersections or one and one-half footcandles in parking areas.

2. Fixture Design in Public Rights-of-Way.

a. Pedestrian-scale street lighting shall be provided including all pedestrian streets along arterials, major collectors, minor collectors and local streets.

b. Pedestrian street lights shall be no taller than twenty feet along arterials and collectors, and sixteen feet along local streets.

Compliance Discussion: Street lights will be installed to meet Central Point standards.

17.67.060 Public parks and open space design standards.

A. General. Parks and open spaces shall be provided in the TOD districts and TOD corridors and shall be designed to accommodate a variety of activities ranging from active play to passive contemplation for all ages and accessibility.

B. Parks and Open Space Location.

1. Parks and open spaces shall be located within walking distance of all those living, working, and shopping in TOD districts.

2. Parks and open spaces shall be easily and safely accessed by pedestrians and bicyclists.

3. For security purposes, parks and open spaces shall be visible from nearby residences, stores or offices.

4. Parks and open space shall be available for both passive and active use by people of all ages.

5. Parks and open space in predominantly residential neighborhoods shall be located so that windows from the living areas (kitchens, family rooms, living rooms but not bedrooms or bathrooms) of a minimum of four residences face onto it.

Compliance Discussion: The project design proposes to extend the existing linear open space buffer abutting the northeast corner of the subject property south, between S. Haskell Street and the rail line, to the southern boundary. See, Figure 2. The narrow width will make it easy for residents to view the entire open space area for security. This linear open space will provide a visual buffer between the subdivision and the rail lines, as well as continuation of the sidewalk for the use of recreational walkers for the full length of the property, north to south. The proposed benches will provide opportunity for passive use. The living rooms of the four residences across the S. Haskell Street will have windows that will face the open space.

C. Parks and Open Space Amount and Size.

1. Common open spaces will vary in size depending on their function and location.

2. The total amount of common open space provided in a TOD district or corridor shall be adequate to meet the needs of those projected (at the time of build out) to live, work, shop, and recreate there.



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3. All TOD projects requiring master plans shall be required to reserve, improve and/or establish parks and open space which, excluding schools and civic plazas, meet or exceed the following requirements:

- a. For single-family detached and attached residences, including duplex units, townhouses and row houses: four hundred square feet for each dwelling.

Compliance Discussion: The TOD Corridor requires 400 square feet of open space or park per unit be set aside in developments in that area. With 21 residential units at 400 sf per unit, a minimum of 8,400 square feet are required. 10,660 square feet are proposed, with approximately 2,200 serving as a bioswale detention for the development.

D. Parks and Open Space Design.

1. Parks and open spaces shall include a combination garbage/recycling bin and a drinking fountain at a frequency of one combination garbage/recycling bin and one drinking fountain per site or one combination garbage/recycling bin and one drinking fountain per two acres, whichever is less, and at least two of the following improvements:

- a. Benches or a seating wall;
- b. Public art such as a statue;
- c. Water feature or decorative fountain;
- d. Children's play structure including swing and slide;
- e. Gazebo or picnic shelter;
- f. Picnic tables with barbecue;
- g. Open or covered outdoor sports court for one or more of the following: tennis, skateboard, basketball, volleyball, badminton, racquetball, handball/paddleball;
- h. Open or covered outdoor swimming and/or wading pool or play fountain suitable for children to use; or
- i. Outdoor athletic fields for one or more of the following: baseball, softball, Little League, soccer.

2. All multifamily buildings that exceed twenty-five units and may house children shall provide at least one children's play structure on site.

3. For safety and security purposes, parks and open spaces shall be adequately illuminated.

Compliance Discussion: The open space proposed is just under a quarter of an acre in size and as such does not warrant provision of a drinking fountain or full size trash bin. Instead two benches are proposed for this short stretch of open space which will support the use of the space by recreational walkers and be an amenity for the neighborhood. Alternately, if the City prefers, the Applicant could install one bench and a single trash can. Service for this trash can will need to be provided by the City. The required standard street lights will provide more than adequate lighting for the open space

17.67.070 Building design standards.

A. General Design Requirements.

1. In recognition of the need to use natural resources carefully and with maximum benefit, the use of "sustainable design" practices is strongly encouraged. In consideration of the climate and ecology of the Central Point area, a variety of strategies can be used to effectively conserve energy and resources:

- a. Natural ventilation;
- b. Passive heating and cooling;
- c. Daylighting;
- d. Sun-shading devices for solar control;
- e. Water conservation;
- f. Appropriate use of building mass and materials; and
- g. Careful integration of landscape and buildings. It is recommended that an accepted industry standard such as the U.S. Green Building Council's LEED™ program be used to identify the most effective strategies. (Information on the LEED™ program can be obtained from the U.S. Green Building Council's website, www.usgbc.org.)



CHICORY VILLAGE SUBDIVISION

Master Plan

Applicant: Bob Fellows Construction

2. All development along pedestrian routes shall be designed to encourage use by pedestrians by providing a safe, comfortable, and interesting walking environment.
3. Convenient, direct and identifiable building access shall be provided to guide pedestrians between pedestrian streets, accessways, transit facilities and adjacent buildings.
4. Adequate operable windows or roof-lights should be provided for ventilation and summer heat dissipation.

Compliance Discussion: The building layout and design proposed is efficient and uses appropriate massing for the 2-story residential building type. All residences have access to the sidewalks which are provided on both sides of each street. Adequate windows are provided for ventilation and safety. The front porches, half of which face south, provide shade for the front living room windows.

B. Architectural Character.

1. General.

- a. The architectural characteristics of surrounding buildings, including historic buildings, should be considered, especially if a consistent pattern is already established by similar or complementary building articulation, building scale and proportions, setbacks, architectural style, roof forms, building details and fenestration patterns, or materials. In some cases, the existing context is not well defined, or may be undesirable. In such cases, a well-designed new project can establish a pattern or identity from which future development can take its cues.
- c. Attention should be paid to the following architectural elements:
 - i. Building forms and massing;
 - ii. Building height;
 - iii. Rooflines and parapet features;
 - iv. Special building features (e.g., towers, arcades, entries, canopies, signs, and artwork);
 - v. Window size, orientation and detailing;
 - vi. Materials and color; and
 - vii. The building's relationship to the site, climate, topography and surrounding buildings.

Compliance Discussion: The proposed subdivision includes attractive two-story detached residences and attached row houses that have been designed to meet the above design aspirations. The designs are complementary to the surrounding housing in style, with inviting front porches and varied roof lines. Their floorplans are efficient, offering quality housing with higher density. The buildings represent classic residential forms with gabled roofs, front and back porches, and appropriately sized windows that reflect the use within. The 2-story building height is typical for the area and should blend well with the surrounding neighborhoods. The gable end wall will have a decorative treatment, such as shingled hardboard siding to enhance the look of the house.

C. Building Entries.

3. Residential.

- a. The main entrance of each primary structure should face the street the site fronts on, except on corner lots, where the main entrance may face either of the streets or be oriented to the corner. For attached dwellings, duplexes, and multi-dwellings that have more than one main entrance, only one main entrance needs to meet this guideline. Entrances that face a shared landscaped courtyard are exempt.
- b. Residential buildings fronting on a street shall have an entrance to the building opening on to the street.
 - i. Single-family detached, attached and row house/townhouse residential units fronting on a pedestrian street shall have separate entries to each dwelling unit directly from the street.
 - ii. Ground floor and upper story dwelling units in a multifamily building fronting a street may share one or more building entries accessible directly from the street, and shall not be accessed through a side yard except for an accessory unit to a single-family detached dwelling.
- c. The main entrances to houses and buildings should be prominent, interesting, and pedestrian-accessible. A porch should be provided to shelter the main entrance and create a transition from outdoor to indoor space.



CHICORY VILLAGE SUBDIVISION

Master Plan

Applicant: Bob Fellows Construction

- d. Generally, single-dwelling porches should be at least eight feet wide and five feet deep and covered by a roof supported by columns or brackets. If the main entrance is to more than one dwelling unit, the covered area provided by the porch should be at least twelve feet wide and five feet deep.
- e. If the front porch projects out from the building, it should have a roof pitch which matches the roof pitch of the house. If the porch roof is a deck or balcony, it may be flat.
- f. Building elevation changes are encouraged to make a more prominent entrance. The maximum elevation for the entrance should not be more than one-half story in height, or six feet from grade, whichever is less.

Compliance Discussion: Each single-family residence has its own distinct entrance facing the street. The attached row house porches are oriented toward the shared driveway that leads to Lindsey Street. Front entrances have six foot deep porches across the front façade. The porch is one step up from the front path and has a shed roof whose slope which matches the gabled roof slope.

D. Building Facades.

3. Residential.

- a. The facades of single-family attached and detached residences (including duplexes, triplexes, fourplexes, townhouses, and row houses) shall comply with the following standards:
 - i. No more than forty percent of the horizontal length of the ground floor front elevation of a single-family detached or attached dwelling with frontage on a public street, except alleys, shall be an attached garage.
 - ii. Residential building elevations facing a pedestrian route shall not consist of undifferentiated blank walls, but shall be articulated with architectural details such as windows, dormers, porch details, balconies or bays.
 - iii. For any exterior wall which is within twenty feet of and facing onto a street or public open space and which has an unobstructed view of that pedestrian street or public open space, at least twenty percent of the ground floor wall area shall be comprised of either display area, windows, or doorways.
 - iv. Architectural detailing is encouraged to provide variation among attached units. Architectural detailing includes but is not limited to the following: the use of different exterior siding materials or trim, shutters, different window types or sizes, varying roof lines, balconies or porches, and dormers. The overall design shall recognize that color variation, in and of itself, does not meet the requirements of this subsection.
 - v. Fences or hedges in a front yard shall not exceed three feet in height. Side yard fencing shall not exceed three feet in height between the front building facade and the street. Fences beyond the front facade of the building in a sideyard or back yard and along a street, alley, property line, or bike/pedestrian pathway shall not exceed four feet in height. Fences over four feet in height are not permitted and hedges or vegetative screens in no case shall exceed six feet in height.

Compliance Discussion: The proposed designs meet the above standards in that the attached garage on each only constitutes less than 39% or less of the horizontal length of the front elevation. Detailing on the houses will vary by house. The front façade windows and doorways comprise at least 20 % of first floor elevation. Fences are not planned for the front yards.

* * * * *



APPENDIX 3. BUILDING DESIGN CONCEPT PLANS

Sheet No.

1. *Concept Model A - Plot & Floor Plans, Elevations*
2. *Concept Model B - Plot & Floor Plans, Elevations*
3. *Concept Model C - Plot & Floor Plans, Elevations*
4. *Concept Attached Row Houses - Floor Plan & Elevations*
5. *Concept Attached Row House Upper Floor & Plot Plans*



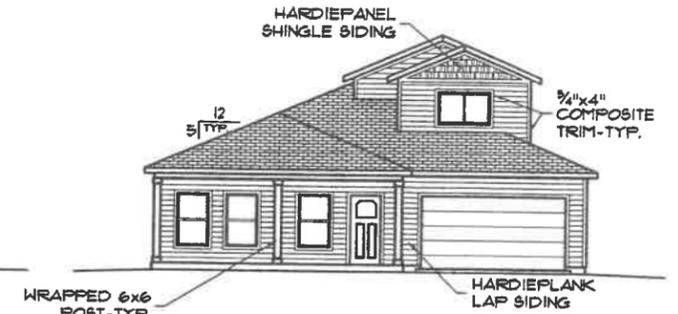
LEFT ELEVATION
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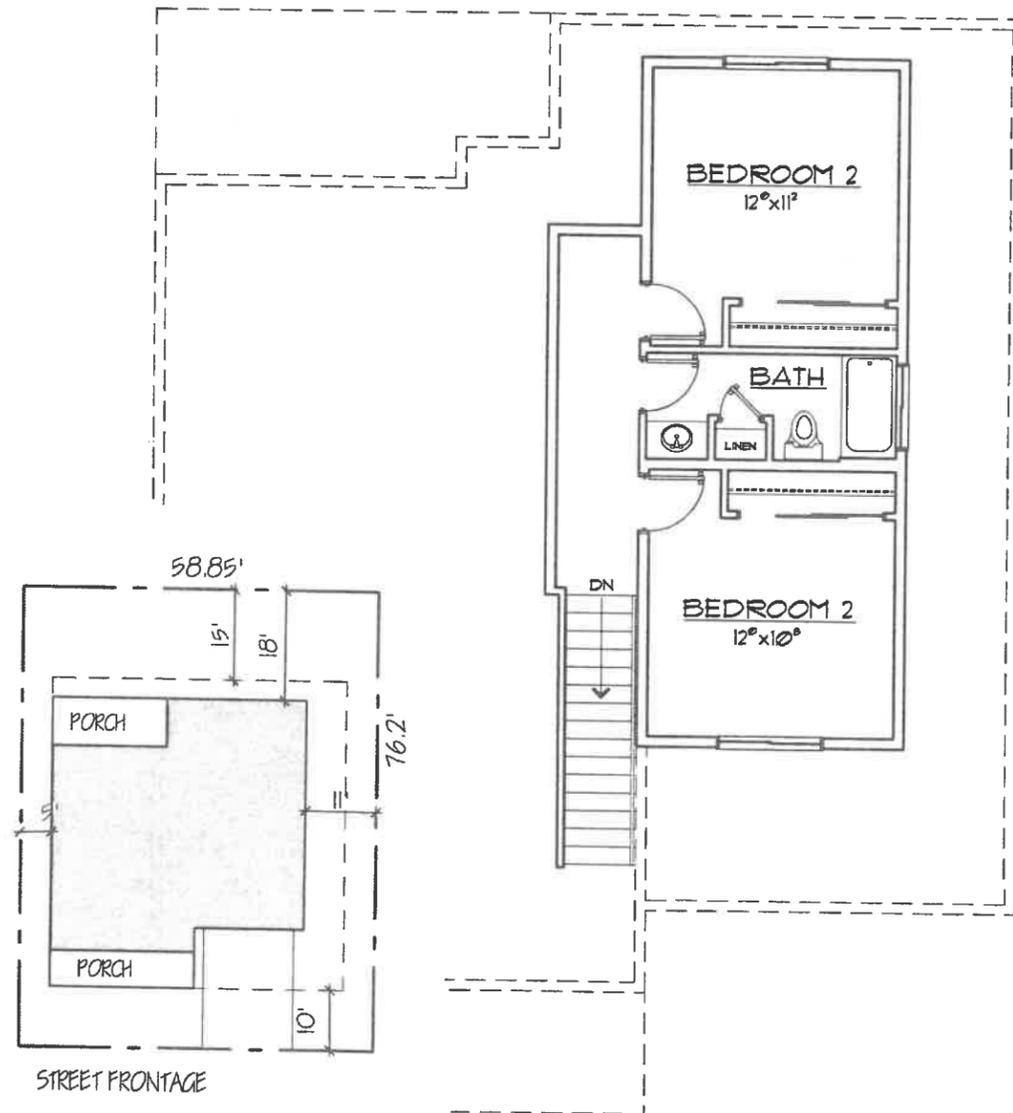
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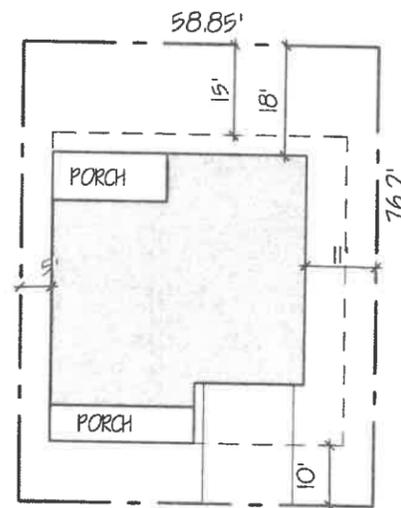
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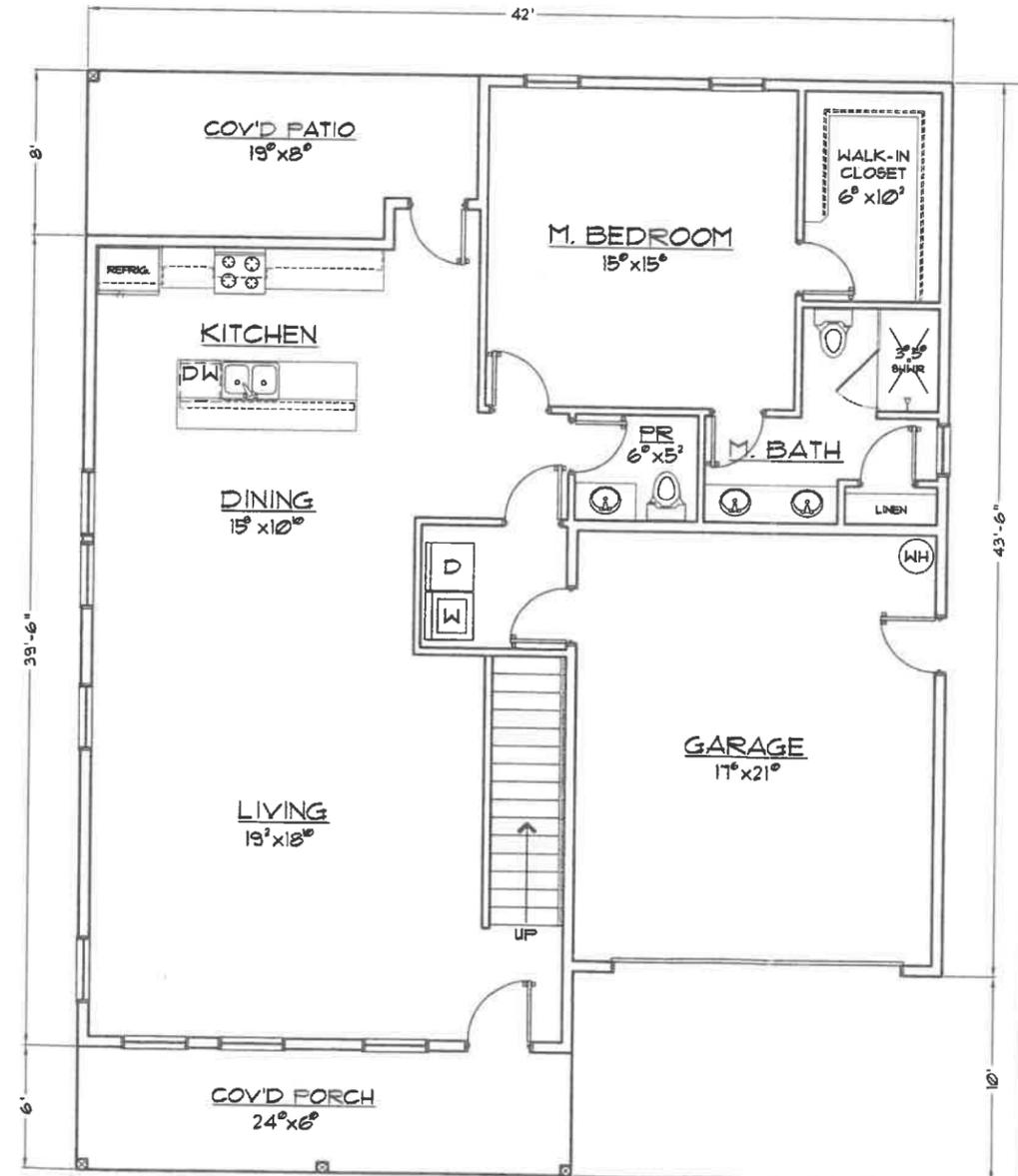
FRONT ELEVATION
1/8" = 1'-0"



UPPER FLOOR PLAN
1/4" = 1'-0"
LIVING: 517*



PLOT PLAN
N15



LOWER FLOOR PLAN
1/4" = 1'-0"
LIVING: 1393*
GARAGE: 387*
COVERED AREAS: 287*
TOTAL LIVING: 1,910*

Model A Concept

NEW 1,910* TOD DISTRICT
RESIDENCE FOR:
**BOB FELLOWS
CONSTRUCTION**

CENTRAL POINT, OR
PROJECT NO: 18-100

PRELIMINARY
NOT FOR CONSTRUCTION

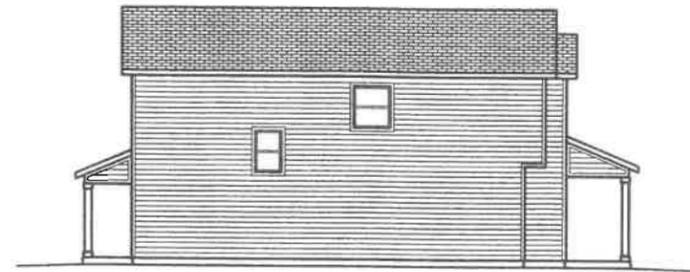


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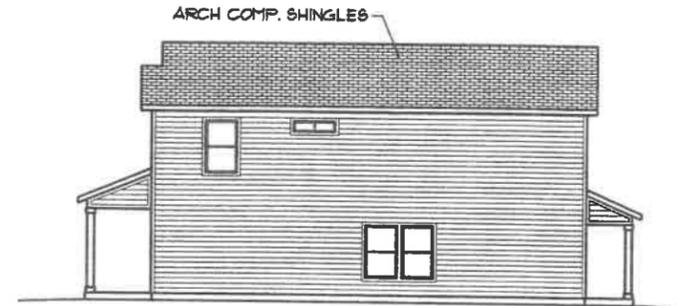
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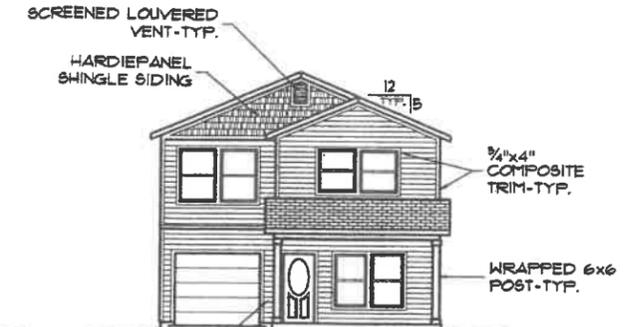
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1/8" = 1'-0"



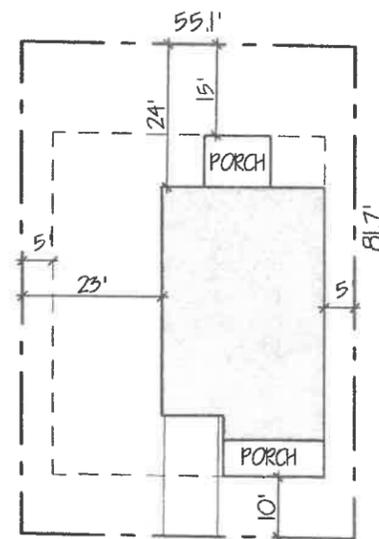
REAR ELEVATION
1/8" = 1'-0"



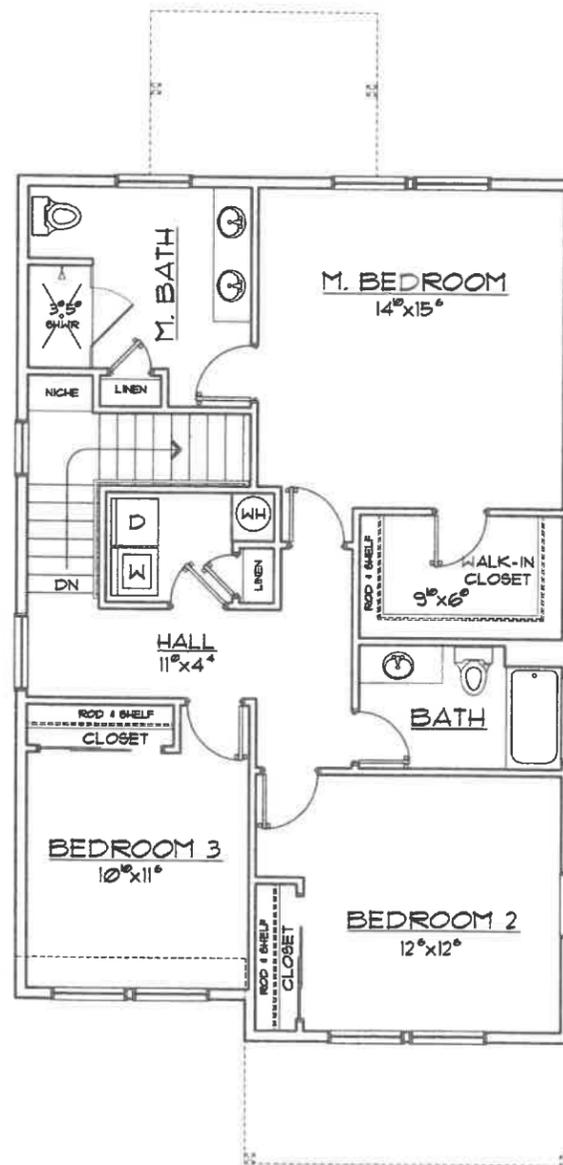
RIGHT ELEVATION
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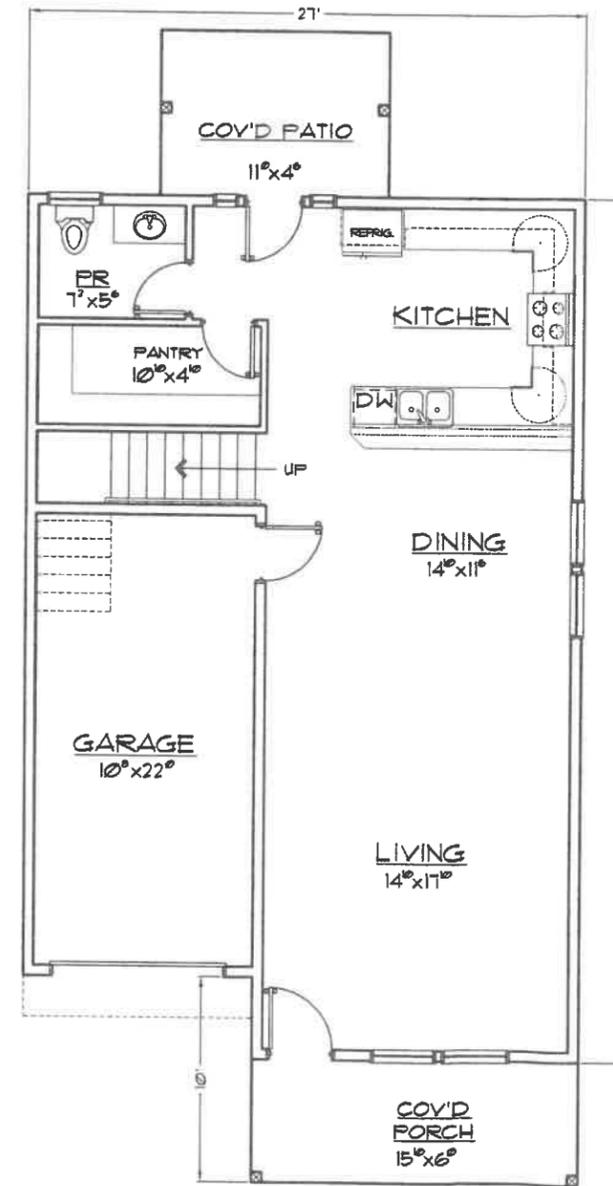
FRONT ELEVATION
1/8" = 1'-0"



STREET FRONTAGE
PLOT PLAN
N/S



UPPER FLOOR PLAN
1/4" = 1'-0"
LIVING: 1042#



LOWER FLOOR PLAN
1/4" = 1'-0"
LIVING: 838#
GARAGE: 251#
COVERED AREAS: 145#
TOTAL LIVING: 1880#

Model B Concept

NEW 1,880# TOD DISTRICT
RESIDENCE FOR:
**BOB FELLOWS
CONSTRUCTION**

CENTRAL POINT, OR
PROJECT NO: 18-100

PRELIMINARY
NOT FOR CONSTRUCTION

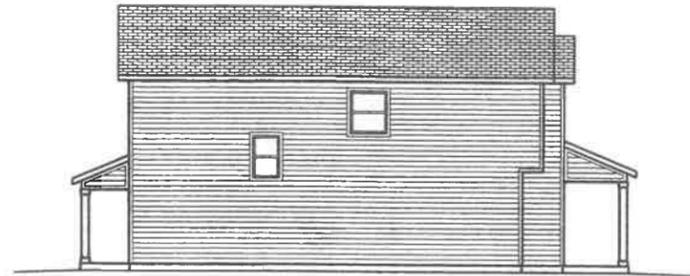


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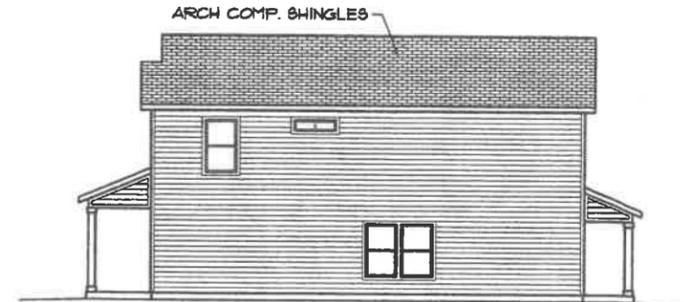
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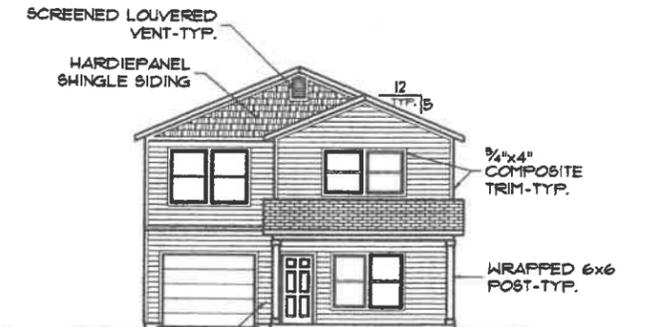
LEFT ELEVATION
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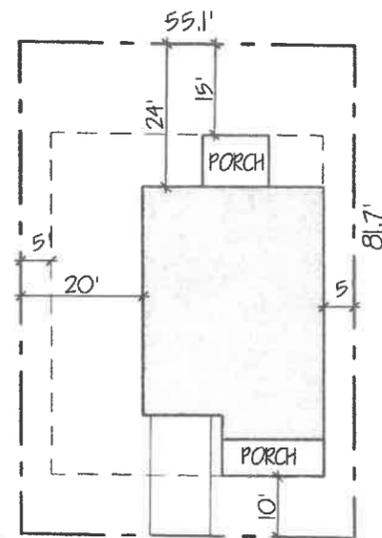
REAR ELEVATION
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RIGHT ELEVATION
1/8" = 1'-0"

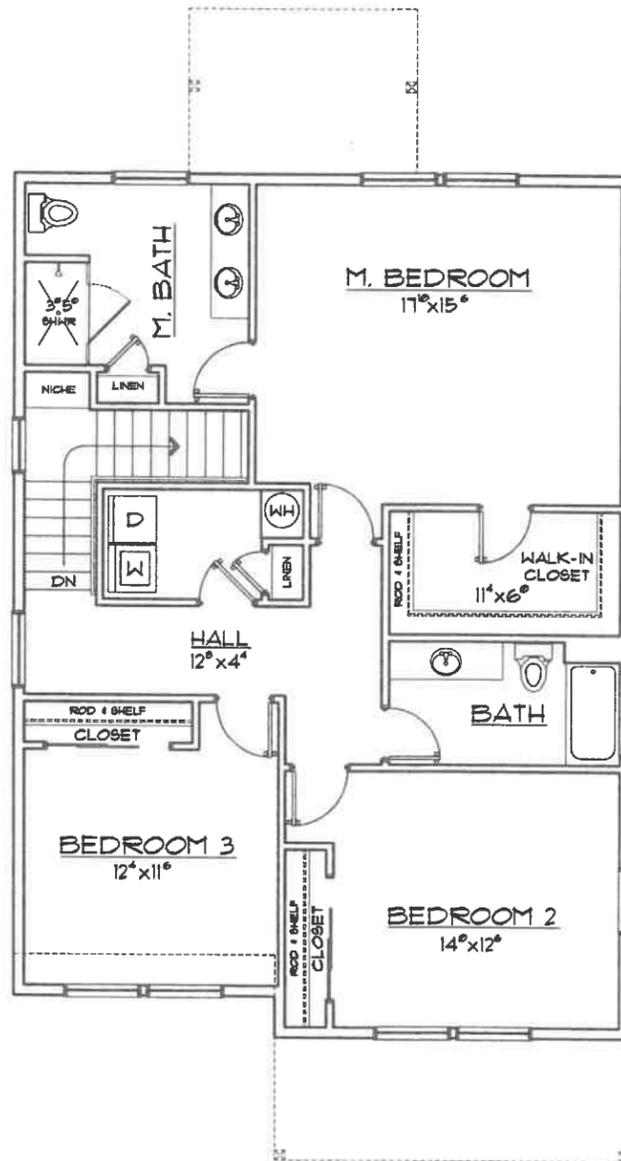


FRONT ELEVATION
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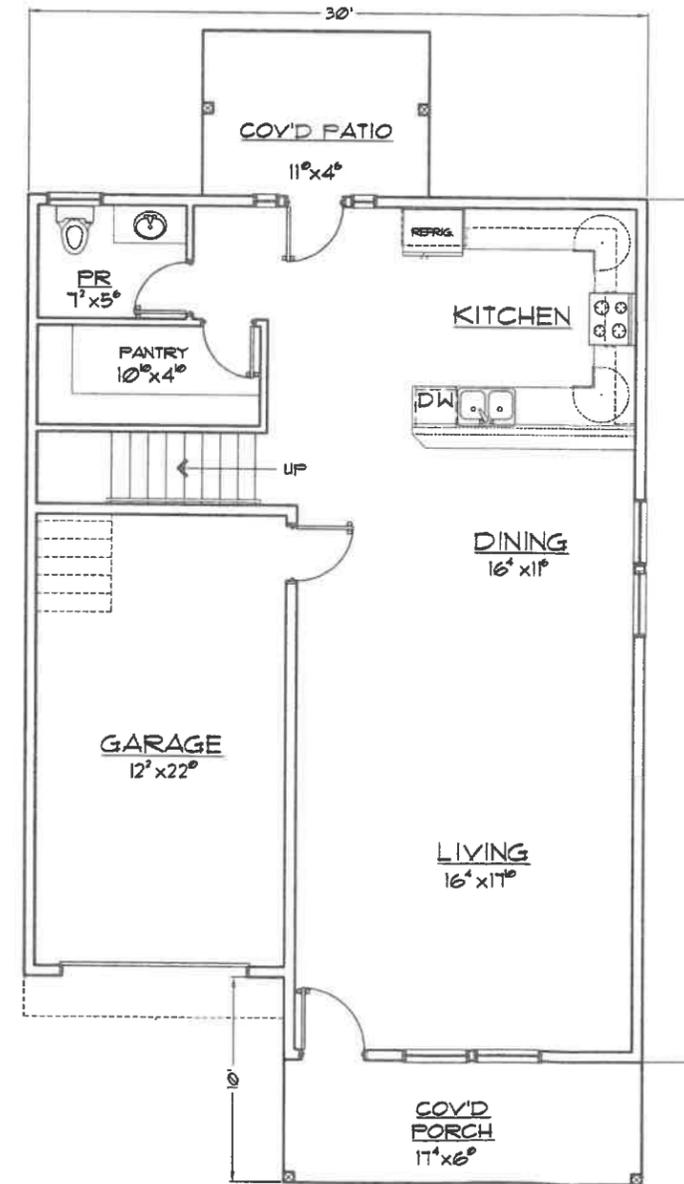
STREET FRONTAGE

PLOT PLAN
NTS



UPPER FLOOR PLAN

1/4" = 1'-0"
LIVING: 1165*



LOWER FLOOR PLAN

1/4" = 1'-0"
LIVING: 924*
GARAGE: 285*
COVERED AREAS: 154*
TOTAL LIVING: 2089*

Model C Concept

NEW 2089# TOD DISTRICT
RESIDENCE FOR:
**BOB FELLOWS
CONSTRUCTION**

CENTRAL POINT, OR

PROJECT NO: 16-133

PRELIMINARY
NOT FOR CONSTRUCTION

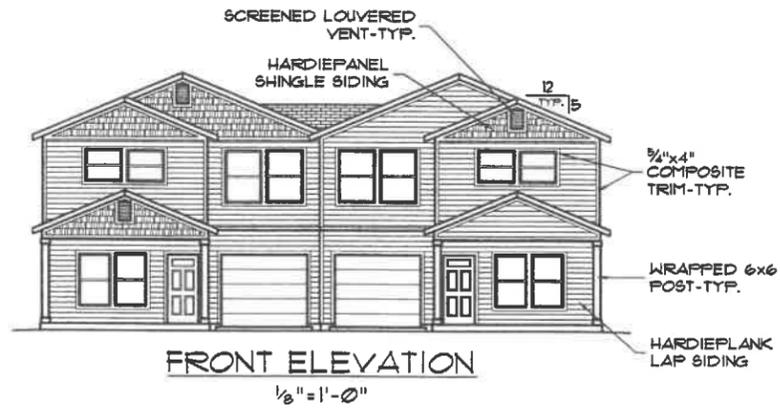


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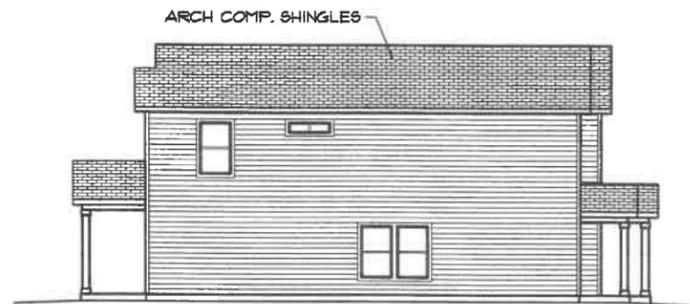
DATE:
22 FEBRUARY 2018

DRAWING NUMBER:
3 OF 5



FRONT ELEVATION

1/8" = 1'-0"



RIGHT ELEVATION

1/8" = 1'-0"



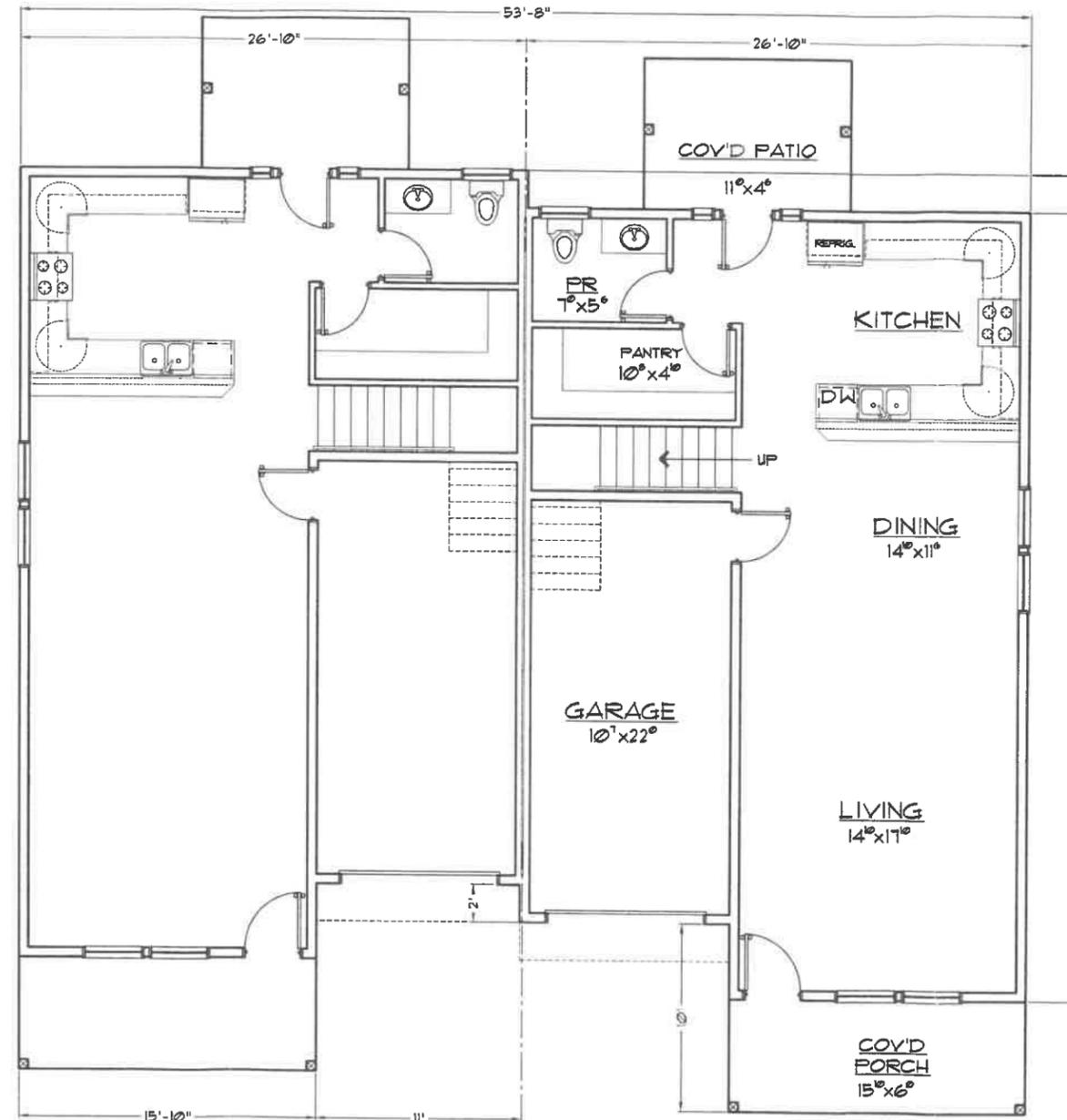
REAR ELEVATION

1/8" = 1'-0"



LEFT ELEVATION

1/8" = 1'-0"



LOWER FLOOR PLAN

1/4" = 1'-0"
 LIVING: 836#
 GARAGE: 248#
 COVERED AREAS: 145#
 TOTAL LIVING: 1084#

Attached Row House Concept

NEW 1,872# TOD DISTRICT RESIDENCE FOR:
BOB FELLOWS CONSTRUCTION

CENTRAL POINT, OR

PROJECT NO: 16-133

PRELIMINARY
 NOT FOR CONSTRUCTION

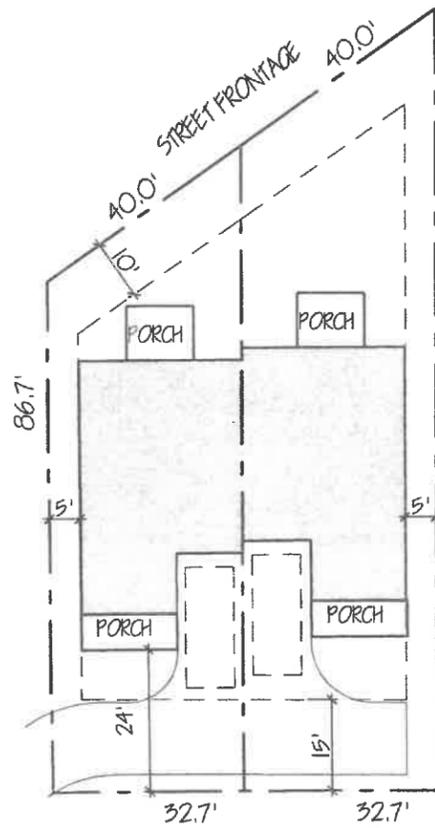


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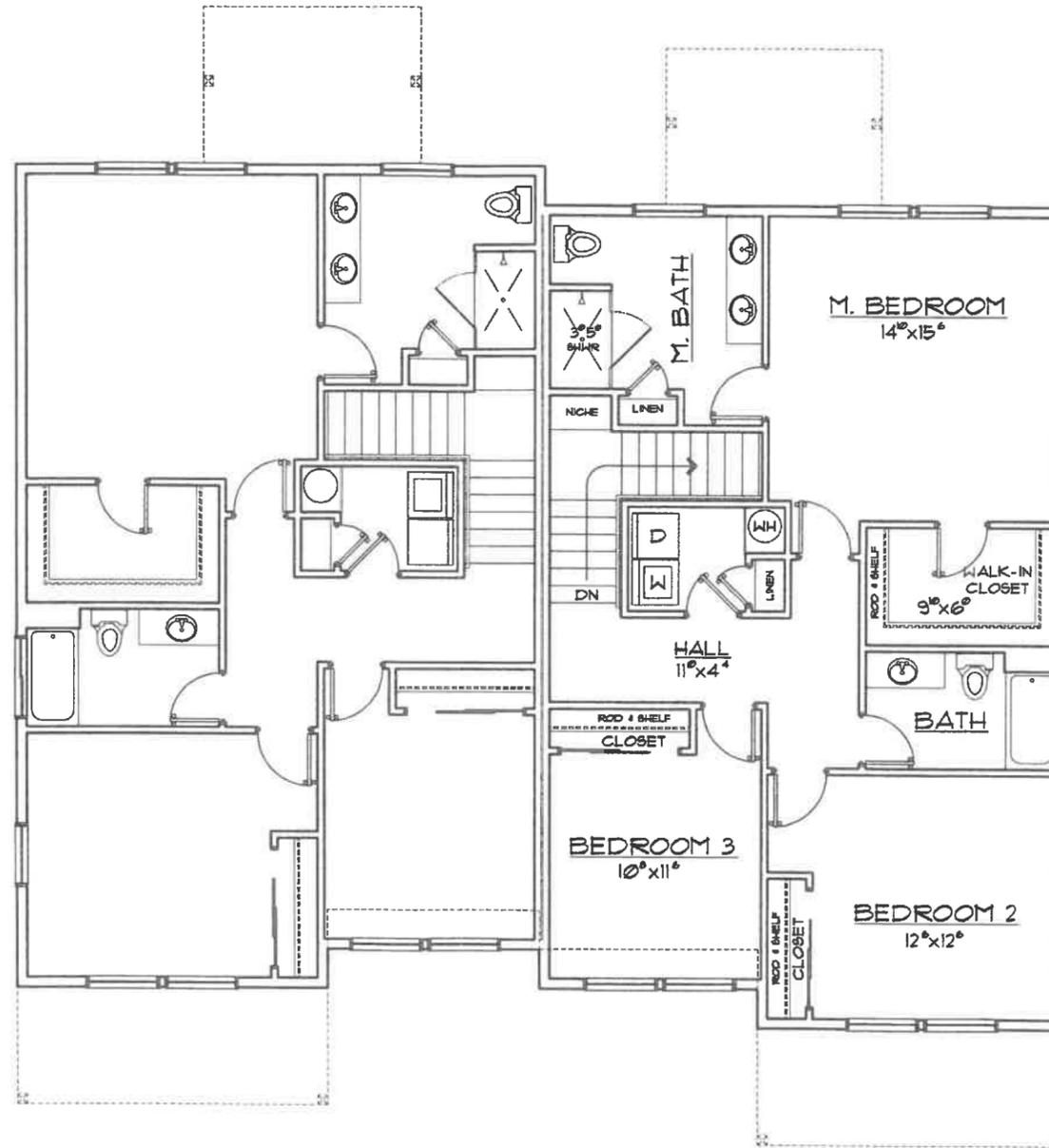
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PLOT PLAN
N15



UPPER FLOOR PLAN

1/4" = 1'-0"
LIVING: 10360

Attached Row House Concept

NEW 1,872# TOD DISTRICT
RESIDENCE FOR:
**BOB FELLOWS
CONSTRUCTION**

CENTRAL POINT, OR

PROJECT NO: 16-133

PRELIMINARY
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