

EXHIBIT "A"
SUPPLEMENTAL FINDINGS
SITE PLAN REVIEW
37-2W-03AB TAX LOT 4300

A. Proposal.

Jackson County School District No. 6 ("Applicant"), is the owner of certain real property, located in Central Point, Oregon, and commonly known as Township 37 South, Range 2 West, Section 03AB, Tax Lot 4300 ("the subject property"). The subject property has been developed with Scenic Middle School since at least 1966, prior to any zoning laws or regulation. Consequently, modifications and upgrades to certain portions of the existing facility are necessary.

B. Schedule of Exhibits.

The following Exhibits have been submitted in support of this Application, which by this reference are incorporated herein and deemed a part of the Application:

EXHIBIT "A":	Supplemental Findings
EXHIBIT "B":	Site Plan / Landscape Plan / Irrigation Plan
EXHIBIT "C":	Assessor's Map
EXHIBIT "D":	Zoning Map
EXHIBIT "E":	SFHA/Stream/Wetland Map
EXHIBIT "F":	Aerial Photograph
EXHIBIT "G":	Site Photographs
EXHIBIT "H":	Building Inventory with square footages
EXHIBIT "I":	School Bus schematics
EXHIBIT "J":	Bicycle Parking Needs Analysis Letter
EXHIBIT "K":	Agent Authorization

C. Background.

The subject property is approximately 13.58 acres in size, is zoned "Civic" and is developed with Scenic Middle School, a pre-existing educational facility comprised of several "stick built" structures attached via covered walkways and four (4) modular buildings, housing two (2) classrooms each. Griffin Creek flows westerly of the subject property and although none of Griffin Creek is within the bounds of the subject property, there is associated mapped floodplain ("AE" zone) which extends across the southwestern corner of the subject property across the western and southern portions of the existing dirt running track and interior lawn of the same. There is existing 5' cyclone (chain-link) fencing, free of privacy slats and potential debris obstruction, in the case of a flood event. A portion of the existing fencing will be replaced by a "rolling gate" panel and the portion of the bus loop which traverses this portion of the property will be paved, requiring a Type 1 floodplain development permit pursuant to CPMC Section 8.24.090. Applicant will file the requisite and obtain the necessary approval prior to any development within the floodplain. A separate "Type C" variance application for a bus loop proposed for location along the southern boundary of the subject property has been submitted for concurrent review. The variance requested

is for adjustment to the required buffer landscaping along the southern boundary of the subject property due to the lack of space between pre-existing buildings and the property boundary which precludes accommodation of both. The landscape buffer(s) required are 10' where adjacent to Aurora Lane and 20' where adjacent to the residentially zoned properties located at either end of the proposed bus loop. Tax Lot 5003 (37-2W-03AB) to the east where the busses will enter the loop on school property and Tax Lot 124 (37-2W-03AC) located to the west and south of the proposed ramp, which will exit the school grounds via Comet Avenue. The Variance application also addresses existing vegetation on the northwestern portion of the property with frontage on Scenic Avenue. The subject property is *not* located within a transit-oriented development district (TOD).

D. Applicable Standards and Criteria.

The standards applicable to this Application are set forth in Central Point Municipal Code ("CPMC") Sections 8.24, 17.13, 17.29, 17.64 17.72 and 17.75, which are set forth as follows:

1. CPMC 8.24 Flood Damage Prevention

8.24.090: Requirement for a floodplain development permit.

A. A floodplain development permit shall be required prior to initiating development activities in any special flood hazard areas as established in Section 8.24.070. The permit shall be for all improvements or structures (including manufactured homes and fences, as set forth in Sections 8.24.050, 8.24.250 and 8.24.260), and for all development including fill and other activities, also set forth in Section 8.24.260. Floodplain development permits shall be subject to the review procedures based on the type of development activity proposed, as set forth below:

1. Section 17.05.200, Type I procedure (administrative), applies to the following floodplain development projects:

- a. Site improvements and construction, including but not limited to new construction, additions, remodels, repairs and renovations located outside a regulatory floodway; [CPMC 8.24.090]**

Applicant's Findings: The Applicant proposes to modify the existing fencing as it abuts Comet Avenue to replace swinging panels for opening and closure, with a sliding gate panel as well as paving of the proposed bus loop. The proposed improvements are not located within the floodway, therefore, a Type 1 floodplain development permit will be required. Applicant agrees to obtain such permit prior to commencement of any grading and paving or replacement of the gate panel.

2. CPMC 17.13 Exceptions to Code Standards. The applicable provisions of CPMC 17.13 are set forth as follows:

17.13.200: Variances - Applicability

A. *Exceptions and Modifications Versus Variances.*

A code standard or approval criterion (“code section”) may be modified without approval of a variance if the applicable code section expressly allows exceptions or modifications. If the code section does not expressly provide for exceptions or modifications, then a variance is required to modify that code section and the provisions of this chapter apply. [CPMC 17.13.200(A)]

Applicant’s Findings: A request for variance to the street frontage landscaping requirements, as applied along the southern and northwestern boundaries of the subject property, has been concurrently submitted for review. CPMC Code Section 17.75.039 “Off-street parking design and development standards” subsection “G” and specifically; Table 17.75.03 “PARKING/LOADING FACILITY PERIMETER AND STREET FRONTAGE LANDSCAPING STANDARDS” does not expressly provide for exceptions or modifications. Therefore, the variance procedure is required.

B. *Combining Variances with Other Approvals; Permit Approvals by Other Agencies.*

Variance requests may be combined with and reviewed concurrently by the city approval body with other land use and development applications (e.g., development review, site design review, subdivision, conditional use, etc.); however, some variances may be subject to approval by other permitting agencies, such as ODOT in the case of state highway access. [CPMC 17.13.200(B)]

Applicant’s Findings: As identified within findings found above, addressing CPMC 17.13.200(A), a request for variance to the street frontage landscaping requirements as applied along the southern and northwestern boundaries of the subject property, has been submitted for concurrent review, in tandem with this site plan review application for parking lot reconfiguration and modification.

C. *Types of Variances.*

As provided in Sections 17.13.300, 17.13.400 and 17.13.500, there are three types of variances (Class A, B, or C). The type of variance required depends on the extent of the variance request and the discretion involved in the decision-making process. (Ord. 1874 §5(part), 2006). [CPMC 17.13.200(C)]

Applicant’s Findings: The proposal exceeds the thresholds of CPMC Section 17.13.300 and there is no applicable option found within Section 17.13.400 to address the need for the variance. Consequently, CPMC Section 17.13.500 is applicable and a separate Type III application for a Class “C” Variance has been submitted for concurrent review with this application.

3. CPMC 17.29 Civic District. The applicable provisions of CPMC 17.29 are set forth as follows:

17.29.040: Civic Use Types

E. *Schools.*

Public and private kindergarten, primary, elementary, middle, junior high, or high schools that provide state mandated basic education and colleges and trade schools.

[CPMC 17.29.040(E)]

Applicant's Findings: The subject property is located within the Civic zoning district. The primary use of the subject property is a public school (Scenic Middle School). The proposed is for modifications to the existing parking facility. Consequently, the pre-existing "school use" and the proposed modification thereof, is a permitted ("P") use within the Civic district pursuant to CPMC table 17.29.050.

4. CPMC 17.64 Off-Street Parking and Loading. The applicable provisions of CPMC 17.64 are set forth as follows:

17.64.020 Applicability

In all districts, in connection with any use whatsoever, there shall be provided at the time any building or structure is erected, enlarged or increased in capacity, or the use is changed or increased in intensity, off-street parking spaces for automobiles, off-street loading, and bicycle parking facilities for the enlarged or increased portion in the case of an addition or for the building, structure or use in other cases, in accordance with the requirements herein. All parking shall be developed and maintained to the standards set forth in Section 17.75.039, Off-street parking design and development standards. [CPMC 17.64.020]

Applicant's Findings: The Applicant proposes a two-phase approach to upgraded development of the school grounds. The current proposal ("Phase 1") does not incorporate the modification of any existing structures nor the addition of any new structures however, a subsequent site plan review ("Phase 2") for the relocation of the existing modulars and addition of a new modular building which will contain six (6) new classrooms will be submitted. The current Phase 1 proposal to upgrade the parking lot incorporates those six (6) classrooms in the "count" (3 seats / classroom) necessary for calculation of adequate parking spaces to serve the continued middle school use pursuant to CPMC Table 17.64.02 B. The dual phase approach for site design and architectural review of the proposed upgrades to the school facility is requested due to construction timelines. The interior layout of the 6 classrooms and location where the new building will be placed and existing modulars moved to, is in the penultimate phase. the Applicant wishes to begin construction of the revised parking lot as soon as possible and reasonably, the bus ramp should be constructed and paved concurrently. The Applicant understands that submission of the subsequent site plan review for the new structural addition, accounting for the six (6) additional classrooms and relocation of the existing modulars and approval of the necessary floodplain development permit for the bus loop and rolling gate may become a condition of this approval, prior to issuance of permits for paving of the new parking area and bus loop.

17.64.030: Off-street loading

A. In all districts for each use for which a building is to be erected or structurally altered to the extent of increasing the floor area to equal the minimum floor area required to provide loading space and which will require the receipt or distribution of materials or merchandise by truck or similar vehicle, there shall be provided off-street loading

space in accordance with the standards set forth in Table 17.64.01, Off-Street Loading Requirements. [CPMC 17.64.030(A)]

Table 17.64.01: Off-Street Loading Requirements

Use Categories	Off-Street Loading Berth Requirement (fractions rounded up to the closest whole number)
OFFICES, HOTELS AND OTHER NONGOODS HANDLING USES	
Sq. Ft. of Floor Area	No. of Loading Berths Required
0-50,000	0
50,001-200,000	1
Over 200,000	2 plus 1 for each 100,000 sq. ft.

Applicant’s Findings: Exhibit “H” identifies current structures existing onsite. The exhibit was prepared using historic Jackson County Assessment field sketches, architectural renderings more recently prepared which incorporate the four (4) existing modular structures and a sheet which accounts for the largest option being considered for the new modular building which will house the six (6) new classrooms that will be reviewed in Phase 2 of the site plan review process. The table below identifies total square footages of both existing and proposed structures:

Building		Square Footage	Building		Square Footage
1.	<i>Cafeteria/Band</i>	11,945	8.	<i>Building “D”</i>	8,555
2.	<i>Library</i>	4,845	9.	<i>Building “E”</i>	6,490
3.	<i>Office</i>	4,750	10.	<i>Modular “1”</i>	1,334
4.	<i>Gym/Locker Room</i>	29,000	11.	<i>Modular “2”</i>	1,715
5.	<i>Building “A”</i>	8,024	12.	<i>Modular “3”</i>	1,715
6.	<i>Building “B”</i>	5,723	13.	<i>Modular “4”</i>	1,715
7.	<i>Building “C”</i>	6,490	14.	<i>New Modular</i>	8,540
			TOTAL		100,841

Pursuant to CPMC Table 17.64.01, one (1) off-street loading space is required. The existing off-street loading space is located west of the cafeteria building.

B. A loading berth shall not be less than ten feet wide, thirty-five feet long and have a height clearance of twelve feet. Where the vehicles generally used for loading and unloading exceed these dimensions, the required length of these berths shall be increased. [CPMC 17.64.030(B)]

Applicant’s Findings: As existing, the off-street loading space located west of the cafeteria building, meets the requirements of this section (*See Exhibit “B”*, attached hereto). This standard is met.

C. If loading space has been provided in connection with an existing use or is added to an existing use, the loading space shall not be eliminated if elimination would result in less space than is required to adequately meet the needs of the use. [CPMC 17.64.030(C)]

Applicant's Findings: The school and the required off-street loading space (west of the existing cafeteria) already exist and will continue to exist, in conformance with requirements of "subsection B" above (See Exhibit "B").

D. Off-street parking areas used to fulfill the requirements of this title shall not be counted as required loading spaces and shall not be used for loading and unloading operations, except during periods of the day when not required to meet parking needs. [CPMC 17.64.030(D)]

Applicant's Findings: The proposed updated parking area, north of the school buildings, will only be utilized for vehicular parking and will not be used for loading or unloading of goods. The one (1) required off-street parking space is not located in the proposed new parking area, it is already located west of the cafeteria (See Exhibit "B"). The standard of this section is met.

E. In no case shall any portion of a street or alley be counted as a part of the required parking or loading space, and such spaces shall be designed and located as to avoid undue interference with the public use of streets or alleys. [CPMC 17.64.030(E)]

Applicant's Findings: No portion of a street or alley was counted as a part of the required parking or loading space. This standard is met.

CPMC 17.64.040: Off-street parking requirements.

All uses shall comply with the number of off-street parking requirements identified in Table 17.64.02A, Residential Off-Street Parking Requirements, and Table 17.64.02B, Non-Residential Off-Street Parking Requirements. For residential uses, the off-street parking requirements are stated in terms of the minimum off-street parking required. For non-residential uses, the off-street parking requirements are presented in terms of both minimum and maximum off-street parking required. The number of off-street parking spaces in Table 17.64.02B, Non-Residential Off-Street Parking, may be reduced in accordance with subsection B of this section, Adjustments to Non-Residential Off-Street Vehicle Parking.

The requirement for any use not specifically listed shall be determined by the community development director on the basis of requirements for similar uses, and on the basis of evidence of actual demand created by similar uses in the city and elsewhere, and such other traffic engineering or planning data as may be available and appropriate to the establishment of a minimum requirement. [CPMC 17.64.040]

CPMC Table 17.64.02 B: Non-residential Off Street Parking Requirements

Use Categories	Minimum and Maximum Vehicle Parking Requirement (fractions rounded down to the closest whole number)
SCHOOLS	
Elementary and Junior High Schools (public and private/parochial)	3 spaces per classroom, or 1 space per 4 seats in the main auditorium, gymnasium, or other place available for public assembly, whichever is greater.

A. Calculation of Required Off-Street Parking.

Off-street parking facility requirements set forth in Table 17.64.02A, Residential Off-Street Parking Requirements, and Table 17.64.020 B, Non-Residential Off-Street Parking Requirements, shall be applied as follows:

- 1. Where the application of the schedule results in a fractional requirement it shall be rounded down to the lowest whole number.**
- 2. For purposes of this chapter, gross floor area shall not include enclosed or covered areas used for off-street parking or loading, or bicycle facilities.**
- 3. Where uses or activities subject to differing requirements are located in the same structure or on the same site, or are intended to be served by a common facility, the total parking requirement shall be the sum of the requirements for each use or activity computed separately, except as adjusted through the site plan and architectural review process under the provisions of subsection (B) of this section. The community development director, when issuing a permit(s) for multiple uses on a site, may restrict the hours of operation or place other conditions on the multiple uses so that parking needs do not overlap and may then modify the total parking requirement to be based on the most intense combination of uses at any one time.**
- 4. Where requirements are established on the basis of seats or person capacity, the building regulations provisions applicable at the time of determination shall be used to define capacity.**
- 5. Where residential use is conducted together with or accessory to other permitted uses, applicable residential requirements shall apply in addition to other non-residential requirements.**
- 6. The parking requirements outlined in Table 17.64.02A, Residential Off-Street Parking Standards, and Table 17.64.02B, Non-Residential Off-Street Parking Requirements, include parking for handicapped persons shall be provided pursuant to the requirements of subsection C of this section, Accessible Parking Requirements. [CPMC 17.64.040(A)]**

Applicant's Findings: The school use is non-residential in nature and the "seat count" is the only mechanism Table 17.64.02(B) for calculating required parking spaces for Elementary and Junior High Schools, there is no calculation by square footage option available.

There are currently 37 classrooms on-site, including those located in the four (4) existing modular structures. The new modular building which will be proposed in the Phase 2 site plan review will add another six (6) classrooms for a total of 43 classrooms and therefore, requiring 129 parking spaces on-site ($43 \times 3 = 129$).

///

B. Adjustments to Non-Residential Off-Street Vehicle Parking.

The off-street parking requirements in Table 17.64.02B, Non-Residential Off-Street Parking Requirements, may be reduced, or increased in any commercial (C) or industrial (M) district as follows:

1. Reductions.

The maximum off-street parking requirements may be reduced by no more than twenty percent. [CPMC 17.64.040(B)]

Applicant's Findings: The Applicant requests a reduction of approximately 5.5% in required parking spaces for a proposed total of 122 parking spaces. The request does not exceed the 20% reduction threshold therefore, this standard is met.

C. Accessible Parking Requirements.

Where parking is provided accessory to a building, accessible parking shall be provided, constructed, striped, signed and maintained as required by ORS 447.233, and Section 1106 of the latest Oregon Structural Specialty Code as set forth in this section.

- 1. The minimum number of accessible parking spaces shall be provided for all uses in accordance with the standards in Oregon Structural Specialty Code, Minimum Number of Accessible Parking Spaces. Accessible parking spaces shall be counted toward meeting off-street parking requirements in Tables 17.64.02A and 17.64.02B, Residential and Non-Residential Off-Street Parking Requirements. The accessible parking requirements are minimum requirements and are not subject to reductions per subsection (B)(1) of this section. [CPMC 17.64.040(C)]**

Applicant's Findings: As identified on the submitted site plan (See Exhibit "B") necessary and required accessible parking will be provided, constructed, striped, signed and maintained as required by ORS 447.233, and Section 1106 of the latest Oregon Structural Specialty Code. The 2019 Oregon Structural Specialty Code (OSSC) Table 1106.1 Accessible Parking Spaces requires a minimum of 5 accessible spaces pursuant to the request for reduction and allowance of 122 total parking spaces:

TOTAL PARKING SPACES PROVIDED IN PARKING FACILITIES	REQUIRED MINIMUM NUMBER OF ACCESSIBLE SPACES
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150 122 spaces proposed →	5
151 to 200	6

As evidenced on the submitted site plan, five (5) accessible spaces (included in the total count of proposed 122 spaces) have been provided adjacent to the northern entry points of the existing school facility and a sixth (6) space has been provided in the smaller parking area in the northwest corner of the property, adjacent to the baseball field and the ingress/egress point from Scenic

Avenue. The standard of this section is exceeded.

D. *Shared Parking.*

Required parking facilities for two or more uses, structures, or parcels of land in any commercial (C) or industrial (M) district may be satisfied by the same parking facilities used jointly, to the extent that the owners or operators show that the need for parking facilities does not materially overlap (e.g., uses primarily of a daytime versus nighttime nature; weekday uses versus weekend uses); and provided, that prior to the issuance of any building permit for the property that the right of joint use is evidenced by a recorded deed, lease, contract, or similar written instrument establishing the joint use. The city may approve owner requests for shared parking through the site plan and architectural review process. [CPMC 17.64.040(D)]

Applicant's Findings: The proposed development is occurring on the grounds of a secure facility and there will be no shared parking. The standards of this section are not applicable.

E. *Off-Site Parking.*

Except for single-family dwellings, the vehicle parking spaces required by this chapter may be located on another parcel of land, provided the parcel is within three hundred feet of the use it serves and the city has approved the off-site parking through the site plan and architectural review process. The distance from the parking area to the use shall be measured from the nearest parking space to a building entrance, following a sidewalk or other pedestrian route. The right to use the off-site parking must be evidenced by a recorded deed, lease, easement, or similar written instrument in the same manner as set forth in subsection (A)(3) of this section. [CPMC 17.64.040(E)]

Applicant's Findings: All 122 proposed parking spaces are located on the subject property. The standards of this section are not applicable.

F. *Mixed Uses.*

If more than one type of land use occupies a single structure or parcel of land, the total requirements for off-street automobile parking shall be the sum of the requirements for all uses, unless it can be shown that the peak parking demands are actually less (see subsection D of this section, Shared Parking). [CPMC 17.64.040(F)]

Applicant's Findings: The proposed development is occurring on the grounds of a secure school facility no additional uses are proposed. The standards of this section are not applicable.

G. *Compact Car Adjustment.*

- 1. Any parking lot or otherwise required public parking area containing ten or more parking spaces shall be eligible for a compact car adjustment, provided all requirements of this chapter are adequately met.**
- 2. Up to, but not exceeding, twenty-five percent of the total number of required parking spaces may be designed and provided for the parking of compact cars.**

- 3. All compact parking spaces must be identified for compact parking only. Compact parking spaces shall be designed in accordance with the minimum standards set forth in Section 17.75.039(B), Parking Stall Minimum Dimensions. [CPMC 17.64.040(G)]**

Applicant's Findings: No adjustments to the size of required parking spaces are proposed for compact vehicles. One hundred sixteen (116) spaces will be standard size parking spaces and six (6) will be Accessible space compliant, as subsequently identified and addressed within findings addressing CPMC Section 17.75 below.

H. Change of Use.

Prior to the change of use of a building or structure the applicant shall demonstrate that adequate parking spaces are available to accommodate the new use(s) as required in this chapter. [CPMC 17.64.040(H)]

Applicant's Findings: There is no proposed change from the current school use of the property. The current proposal is strictly regarding access, circulation and parking to serve the existing school facility. This Phase 1 site plan review application accounts for the six (6) classrooms which will be located in the new structure to be proposed within the Phase 2 site plan review application. The proposal is presented in two (2) phases of development due to the need to begin construction of the parking lot and bus loop in advance of the review and approval for the building addition that will be addressed within the Phase 2 site plan and architectural review.

I. Bicycle Parking.

Bicycle parking shall be provided in accordance with Table 17.64.04, Bicycle Parking Requirements. [CPMC 17.64.040(I)]

Table 17.64.04: Bicycle Parking Requirements

Land Use	Minimum Required
Institutional	
Schools, Junior High/Middle School	4 spaces per classroom

Applicant's Findings: Upon completion of the Phase 2 site plan review and construction addition of the new modular structure, there will be a total of 43 classrooms. Pursuant to CPMC Table 17.64.04, 180 bicycle parking spaces will be required. Currently, there are 75 bicycle parking spaces available for use by students and staff. This required number of spaces (180) far exceeds the number of bicycles the district observes students utilizing. A bicycle parking needs analysis is further addressed below when addressing CPMC Section 17.75.039 (H)(3)(b) and is attached hereto as **Exhibit "J"**.

- 5. CPMC 17.72 Site Plan and Architectural Review.** The applicable provisions of CPMC 17.72 are set forth as follows:

///

17.72.020: Applicability

B. *Major Projects.*

The following are “major projects” for the purposes of the site plan and architectural review process and are subject to Type 2 procedural requirements as set forth in Chapter 17.05, Applications and Types of Review Procedures:

1. New construction, including private and public projects, that:

- a. Includes a new building or building addition of five thousand square feet or more;**
- b. Includes the construction of a parking lot of ten or more parking spaces; or**
- c. Requires one or more variances or conditional use permits and, in the judgement of the director, will have a significant effect upon the aesthetic character of the city of the surrounding area. [CPMC 17.72.020(B)]**

Applicant’s Findings: Although a parking lot containing 77 (more than 10) spaces already exists, the current proposal adds 45 more parking spaces. As well, a subsequent variance request for deviation from the street frontage landscaping buffer requirements for the bus loop at the southern end of the parcel and the street frontage on Scenic Avenue in the northwestern corner of the subject property has been submitted for concurrent review. Thus, subsections (b) and (c) of this section are applicable and the proposal constitutes a “Major Project”. Therefore, the Type 2 procedure pursued through submission of this application is appropriate.

17.72.040: Site Plan and Architectural Standards

The applicable provisions of CPMC 17.72.040 are set forth as follows:

A. Applicable site plan, landscaping, and architectural design standards as set forth in Chapter 17.75, Design and Development Standards; [CPMC 17.72.040(A)]

Applicant’s Findings: A site plans and landscaping plans have been submitted as part of Exhibit “B”. No structural improvements are proposed within this Phase 1 site plan review application.

B. City of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction; [CPMC 17.72.040(B)]

Applicant’s Findings: The proposal shall meet the City of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction.

C. Accessibility and sufficiency of firefighting facilities to such a standard as to provide for the reasonable safety of life, limb, and property, including, but not limited to,

suitable gates, access roads and fire lanes so that all buildings on the premises are accessible to fire apparatus. [CPMC 17.72.040(C)]

Applicant's Findings: No change to existing access is proposed, nor are any new gates. The existing portion of the southern boundary cyclone fence as abutting Comet Avenue will be replaced with rolling gate. Jackson County Fire District No. 3 ("JCFD#3") will be provided the same access it currently has in this location with the existing "swinging" fence panel. Coincidentally, Jackson County Fire District No. 3 has constructed a new (2019) fire station on adjacent Tax Lot 4600 (37-2W-03AB) and has direct access to the school grounds via Rock Way which terminates into the northern boundary of the subject property. Further, the southern boundary of Tax Lot 4600 abuts the subject property and in the event of an emergency, vehicular access over the curb delineating the northern loading area (northern portion of the parking lot on the subject property) would not be an impediment for emergency vehicle access if absolutely necessary. Gates which the fire district already has access through, along the southern fence line, also provide direct access to the southern portion of the school property (*See aerial imagery at Exhibit "F" and photographs of District #3 signage posted on southern fence at Exhibit "G"*).

17.72.050: Conditions on site plan and architectural review approval

The approving authority may attach to any site plan and architectural review approval given under this chapter specific conditions, or restrictions, deemed necessary to protect the public health, safety or welfare including, but not limited to, the following:

- A. Construction and installation of any on-site or off-site improvements, including but not limited to sidewalks, curbs, gutters, streets, bikeways, street signs and streetlights, traffic control signs and signals, water, storm drainage, sanitary sewer, and park and recreation improvements. In requiring off-site improvements, the city shall find that the improvements are reasonably related to the development and would serve a public purpose such as mitigating the negative impact of the proposed development.**

All improvements required under this subsection shall be made at the expense of the applicant and shall conform to the provisions of the City of Central Point Department of Public Works Department Standard Specifications and Uniform Details for Public Works Construction. However, the city, in its discretion, may modify such standards and determine site-specific design, engineering and construction specifications when appropriate in the particular development; [CPMC 17.72.050(A)]

Applicant's Findings: Applicant understands and accepts the financial responsibility for the expense of completion of improvements required under this section. Any proposed improvements will conform to the provisions of the City of Central Point Department of Public Works Department Standard Specifications and Uniform Details for Public Works Construction. Applicant further understands that the city may modify such standards as site-specific and agrees to compliance with said modifications to the standards, provided such measures are reasonable.

- B. An agreement by the owner of the property to waive, on his or her behalf, and on behalf of all future owners of the land, any objection to the formation of a local improvement district which may be formed in the future to provide any of the**

improvements specified in subsection A of this section; [CPMC 17.72.050(B)]

Applicant's Findings: Applicant understands and agrees to the requirements of this section.

- C. An agreement by the owner of the property to enter into a written deferred improvement agreement providing that one or more of the improvements specified in subsection A of this section shall be made by the owner at some future time to be determined by the city; [CPMC 17.72.050(C)]**

Applicant's Findings: Applicant understands and agrees to the requirements of this section.

- D. Any agreement entered into pursuant to subsections B or C of this section shall be recorded in the county recorder's office and shall be intended to thereafter run with the land, so as to bind future owners of the lands affected to the conditions of the agreement. Any and all recording costs shall be the responsibility of the applicant; and [CPMC 17.72.050(D)]**

Applicant's Findings: Applicant understands and agrees to the requirements of this section.

- E. Any other conditions deemed by the city to be reasonable and necessary in the interests of the public health, safety or welfare. [CPMC 17.72.050(E)]**

Applicant's Findings: Applicant understands that the city may impose additional conditions deemed reasonable and necessary in the interest of the public health, safety or welfare. Applicant agrees to compliance with said potential additional measures, provided such measures are reasonable.

- 6. CPMC 17.75 Design and Development Standards.** The applicable provisions of CPMC 17.75 are set forth as follows:

17.75.020: *Applicability* The regulations set forth in this chapter apply to all development within the city of Central Point. [CPMC 17.75.020]

Applicant's Findings: The subject property is within the city limits of the City of Central Point therefore, this section is applicable.

17.75.031 *General connectivity, circulation and access standards* The purpose of this section is to assure that the connectivity and transportation policies of the city's Transportation System Plan are implemented. In achieving the objective of maintaining and enhancing the city's small-town environment it is the city's goal to base its development pattern on a general circulation grid using a walkable block system. Blocks may be comprised of public/private street right-of-way, or accessways.

A. *Streets and Utilities.*

The public street and utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details

for Public Works Construction shall apply to all development within the city. [CPMC 17.75.031(A)]

Applicant's Findings: Applicant acknowledges this provision.

B. Block Standards. [CPMC 17.75.031(B)]

Applicant's Findings: CPMC Section 17.08.010 "Definitions, specific" provides that:

"Block" means an area completely bounded by streets, or a combination of streets, major rights-of-way, waterways, city boundary or other major physical features."

The subject property does not meet the definition above of a "block". The subject property is bounded along a portion of the southern boundary by a street, Aurora Lane, the city of Central Point boundary (city limits) to the west and has approximately 330.80' of street frontage to the north along Scenic Avenue and approximately 35' of frontage (also to the north) where Rock Way dead ends into the school property. The rest of the property to the north and east are surrounded by other parcels. Therefore, the standards of this section are not applicable.

C. Driveway and Property Access Standards.

Vehicular access to properties shall be located and constructed in accordance with the standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 320.10.30, Driveway and Property Access. [CPMC 17.75.031(C)]

Applicant's Findings: The two access points located along the northwestern and central-northern property boundary as well as the proposed bus loop on the southern end of the subject property will conform to the provisions of the City of Central Point Department of Public Works Department Standard Specifications and Uniform Details for Public Works Construction, Section 320.10.30, Driveway and Property Access.

D. Pedestrian Circulation.

Attractive access routes for pedestrian travel shall be provided through the public sidewalk system, and where necessary supplemented through the use of pedestrian accessways as required to accomplish the following:

- 1. Reducing distances between destinations or activity areas such as public sidewalks and building entrances;**
- 2. Bridging across barriers and obstacles such as fragmented pathway systems, wide streets, heavy vehicular traffic, and changes in level by connecting pedestrian pathways with clearly marked crossings and inviting sidewalk design;**
- 3. Integrating signage and lighting system which offers interest and safety for pedestrians;**

4. Connecting parking areas and destinations with retail streets or pedestrian accessways identified through use of distinctive paving materials, pavement striping, grade separation, or landscaping. [CPMC 17.75.031(D)]

Applicant's Findings: The proposal is for upgrades to an existing parking lot and for creation of a bus loop, serving an existing school. There is no use of the public sidewalk system due to the school being a secure site. As evidenced on the submitted site plan (See Exhibit "B") and within the photographs of the existing pedestrian access points at the southern boundary of the school (See Exhibit "G") well identified pedestrian access points and pathways are present and proposed. Once on school grounds, there are multiple covered walkways that provide safe pedestrian circulation throughout school grounds, most of which are in locations where vehicles will never be, due to the clustering of existing structures.

E. Accessways, Pedestrian.

Pedestrian accessways may be used to meet the block requirements of subsection B of this section. When used pedestrian accessways shall be developed as illustrated in Figure 17.75.01. All landscaped areas next to pedestrian accessways shall be maintained, or plant materials chosen, to maintain a clear sight zone between three and eight feet from the ground level. [CPMC 17.75.031(E)]

Applicant's Findings: As identified above, the subject property is not a "block" as defined by CPMC Section 17.08.010 and is a secure facility that is not open to the general public for the purposes of pedestrian traffic to move to and from adjacent sites. Therefore, this section is not applicable.

F. Retail Street.

Retail streets may be used to meet the block requirements of subsection B of this section. When used retail streets shall be developed as illustrated in Figure 17.75.02. [CPMC 17.75.031(F)]

Applicant's Findings: No new streets are proposed, and the development is not for retail use it is a parking lot for an existing educational facility. Further, as identified above, the subject property is not a "block" as defined by CPMC Section 17.08.010. Therefore, this section is not applicable.

17.75.039: Off-street parking design and development standards All off-street vehicular parking spaces shall be improved to the following standards:

A. Connectivity.

Parking lots for new development shall be designed to provide vehicular and pedestrian connections to adjacent sites unless as a result of any of the following such connections are not possible:

- 1. Topographic constraints;**
- 2. Existing development patterns on abutting property which preclude a logical connection;**
- 3. Traffic safety concerns; or**

4. Protection of significant natural resources. [CPMC 17.75.039(A)]

Applicant's Findings: The proposed parking facility will provide access to Scenic Avenue at the northwest corner of the subject property, access to newly developed Rock Way in the central portion of the northern property boundary for general traffic, and ingress from Aurora Lane and egress onto Comet Way (for school busses only). None of this "connectivity" is for general public use as the middle school is maintained as a secure facility, precluding any interconnectivity to surrounding properties for both vehicular and pedestrian traffic.

B. Parking Stall Minimum Dimensions.

Standard parking spaces shall conform to the following standards and the dimensions in Figure 17.75.03 and Table 17.75.02; provided, that compact parking spaces permitted in accordance with Section 17.64.040(G) shall have the following minimum dimensions:

- 1. Width--Shall be as provided in column B in Table 17.75.02;**
- 2. Length--Shall reduce column C in Table 17.75.02 by no more than three feet.**
[CPMC 17.75.039(B)]

Applicant's Findings: No reduction in parking space sizes for compact vehicles are proposed. All parking spaces are 90° (perpendicular) spaces and the "row #3 option" within that sub-group of table 17.75.02, was utilized to prepare the parking stall dimensions identified on the submitted site plan (See Exhibit "B"). Those dimensions are identified and addressed within subsection E(1) below.

C. Access.

There shall be adequate provision for ingress and egress to all parking spaces. [CPMC 17.75.039(C)]

Applicant's Findings: Ingress and egress to the existing parking area, proposed for upgrade within this application, is provided by Scenic Avenue at the northwestern corner of the subject property as well as from newly constructed Rock Way, located in the central portion of the northern property boundary.

D. Driveways.

Driveway width shall be measured at the driveway's narrowest point, including the curb cut. The design and construction of driveways shall be as set forth in the Standard Specifications and Public Works Department Standards and Specifications.
[CPMC 17.75.039(D)]

Applicant's Findings: As evidenced on the submitted site plan (See Exhibit "B"), driveway widths have been measured at the driveway's narrowest point, including the curb cut. The driveways have been designed and will be constructed as set forth in the Standard Specifications and Public Works Department Standards and Specifications.

///

E. Improvement of Parking Spaces.

- 1. When a concrete curb is used as a wheel stop, it may be placed within the parking space up to two feet from the front of a space. In such cases, the area between the wheel stop and landscaping need not be paved, provided it is maintained with appropriate ground cover, or walkway. In no event shall the placement of wheel stops reduce the minimum landscape or walkway width requirements.**

Applicant's Findings: The parking lot has been designed in compliance with the provisions and specifications of this section. The proposed parking spaces are all perpendicular (90° angle) spaces and have been designed in accordance with the provisions of CPMC Table 17.75.02:

Table 17.75.02: Parking Dimension Schedule

A	B	C	D	E	F1	F2
90 Degrees	9.00	19.00**	24.00**	9.00	62.00	n.a.

Per the submitted parking plan (*See Exhibit "B"*, the site plan) the proposed parking stalls and associated dimensional requirements were designed to meet the standards identified in "row 3" of column "B" (column "A" section "90 degrees") in the table above.

- 2. All areas utilized for off-street parking, access and maneuvering of vehicles shall be paved and striped to the standards of the city of Central Point for all-weather use and shall be adequately drained, including prevention of the flow of runoff water across sidewalks or other pedestrian areas. Required parking areas shall be designed with painted striping or other approved method of delineating the individual spaces, with the exception of lots containing single-family or two-family dwellings.**

Applicant's Findings: The parking lot will be paved and striped as identified on the attached site plan and a drainage plan for the parking lot has also been included (*See Exhibit "B"*). As proposed, the standards of this section are feasibly met.

- 3. Parking spaces shall be designed so that no backing movements or other maneuvering within a street or other public right-of-way shall be necessary, except for one- and two-family dwellings with frontage on a local street per the city of Central Point street classification map.**

Applicant's Findings: The proposed parking lot is in the same location in which it has always existed and no maneuvering within a street or other public right of way is necessary to negotiate ingress and egress from the parking area (*See Exhibit "B"*).

- 4. Any lighting used to illuminate off-street parking or loading areas shall be so arranged as to direct the light away from adjacent streets or properties.**

Applicant's Findings: Applicant agrees to the provision of this section.

5. **Service drives shall have a minimum vision clearance area formed by the intersection of the driveway centerline, the street right-of-way line, and a straight line joining the lines through points twenty feet from their intersection.**

Applicant's Findings: Service drives have been designed in compliance with the provision of this section (*See Exhibit "B"*).

6. **Parking spaces located along the outer boundaries of a parking lot shall be contained by a curb or a bumper rail so placed to prevent a motor vehicle from extending over an adjacent property line, a public street, public sidewalk, or a required landscaping area.**

Applicant's Findings: Parking has been designed in compliance with the requirements of this section (*See Exhibit "B"*).

7. **Parking, loading, or vehicle maneuvering areas shall not be located within the front yard area or side yard area of a corner lot abutting a street in any residential (R) district, nor within any portion of a street setback area that is required to be landscaped in any commercial (C) or industrial (M) district.**

Applicant's Findings: The subject property is not a corner lot and not located within a residential (R) district. The subject property is zoned "Civic", and the residential zones located to the northeast, east and south are buffered from the existing parking area proposed for redevelopment by the existing structures that comprise the school facility. Because Rock Way "dead ends" into the subject property at a 90°, perpendicular angle, there is no street "frontage" along Rock Way. The 10 proposed parking spaces located at the northwestern access point from Scenic Avenue are not located within any setback from Scenic way (*See Exhibit "B"*). As proposed, the standards of this section are met.

8. **Except as provided in subsection (E)(3) of this section, all uses, including one- and two-family dwellings on arterial and collector streets, shall provide adequate vehicle turnaround and maneuvering area through the use of aisle extensions and/or turnaround spaces as illustrated in Figure 17.75.04 and 17.75.05. Functionally equivalent turnaround and maneuvering designs may be permitted by the approving authority through the site plan and architectural review process. [CPMC 17.75.039(E)]**

Applicant's Findings: Compliance with the requirements of this section are demonstrated on the submitted site plan (*See Exhibit "B"*).

F. *Limitation on Use of Parking Areas.*

Required parking areas shall be used exclusively for vehicle parking in conjunction with a permitted use and shall not be reduced or encroached upon in any manner. The parking facilities shall be so designed and maintained as not to constitute a nuisance at any time, and shall be used in such a manner that no hazard to persons or property, or unreasonable impediment to traffic, will result. [CPMC 17.75.039(F)]

Applicant's Findings: Applicant agrees to compliance, at all times, with the requirements of this section.

G. *Parking/Loading Facility Landscaping and Screening.*

Parking lot landscaping shall be used to reinforce pedestrian and vehicular circulation, including parking lot entries, pedestrian accessways, and parking aisles. To achieve this objective the following minimum standards shall apply; however, additional landscaping may be recommended during the site plan and architectural review process (Chapter 17.72). All parking lots shall be landscaped in accordance with the following standards:

Table 17.75.03: Parking/Loading facility perimeter and street frontage landscaping standards

Street Frontage	Min. Planting Area Width	Plants Required per 100 Lineal Ft. of Street Frontage	
		Trees	Shrubs
Arterial/Collector	15 ft.	4	20
Local	10 ft.	3	15
Perimeter (Abutting) Land Use		Plants Required per 100 Lineal Ft. of Abutting Property	
Residential	20 ft.	4	20
Commercial	10 ft.	3	15
Industrial	5 ft.	2	10

1. *Perimeter and Street Frontage Landscaping Requirements.*

The perimeter and street frontage for all parking facilities shall be landscaped according to the standards set forth in Table 17.75.03.

Applicant's Findings: The parcel is uniquely shaped and very little of it has street frontage. The northwestern corner abuts Scenic Avenue, classified as a Minor Arterial, for a length of approximately 330.80'. Approximately 60' feet of that segment is devoted to paved accessway and is therefore, removed from the total lineal footage used to calculate the number of required trees and shrubs pursuant to CPMC Table 17.75.03. Approximately 220' of frontage exists west of the accessway therefore, nine (9) trees and forty-five (45) shrubs would be required to be planted at a minimum width of 15' along this segment of street frontage. Approximately 50' of frontage exists to the east of the accessway which would require planting of two (2) trees and ten (10) shrubs at a minimum width of 15' along this segment of street frontage pursuant to CPMC Table 17.75.03 (See Exhibit "B").

The southern boundary of the subject property is approximately 890' in length. Approximately 580' of that southern boundary segment abuts Aurora Lane, classified as a "Local" road. 130' abuts tax lot 124 (37-2W-3AC), approximately 50' abuts the "dead end" of Comet Avenue and the remaining approximately 130', abuts tax lot 132 (37-2W-3AC). The school was constructed near 1966 (per the assessment record) and the southernmost structures are located only 30' from the southern property boundary. The addition of the new building in Phase 2 of the project and reconfiguration and upgrade to the existing parking area proposed within this application are due

to increasing populations over the course of the last 56 years since the school was initially constructed. Due to congestion at existing ingress/egress points during peak times on weekdays (drop off and pick up hours) the Applicant proposes alternative placement of a bus loop, to be located on the southern end of the subject property, between the existing structures and southern property boundary which is bounded by existing cyclone/chain-link fencing with privacy slats (*Please See* photographs at **Exhibit “G”**).

Due to existing structural development on the subject property, there is no alternative location which would accommodate the bus drop-off and pick-up loop without displacing necessary open space; the track and interior lawn, the baseball diamond and outfield and the approximately 550 sq/ft basketball court area adjacent to (east of) the parking lot. Therefore, the proposed location is the only location on the subject property that can accommodate bus loop due. Due to existing structural development in proximity to this southern property boundary, situation of the bus loop in this location precludes the Applicant from meeting the standards of Table 17.75.03 which would require a minimum planting width area of 10’ where the property abuts Aurora Lane and a minimum planting width of 20’ where the bus ramp will wrap around Tax Lot 124 (37-2W-03AC) and exit the subject property via Comet Avenue. There are several types of school busses in varying lengths and widths (*See Exhibit “I”*). The largest bus within the diagram, bus type “D” is approximately 8 feet in width¹. The drive surface of the bus loop requested is 16’ in width, which is necessary to accommodate the school bus, its mirrors and the necessary loading/unloading apparatus for wheelchair bound students. Due to the existing structures and uniquely designed (archaic) stormwater drainage system, it would be impossible to meet the requirements of Table 17.75.03.

As well, the variance application includes a request to rely on existing landscaping as sufficient and ample where there is street frontage located along the western end of the northern property boundary in the area of the access drive from Scenic Avenue. Along the approximately 50’ segment to the east of the access drive from Scenic Avenue, there are more mature trees existing than would be required to be planted within this section (requirement of two (2)) and filling of the open lawn space with shrubbery would remove desired open space. As well, the desire to preserve the outfield of the baseball diamond (to the west of the access drive) is considered for the purposes of the variance. A separate Type III application to request a variance to those standards, has been submitted in conjunction with this application.

2. Terminal and Interior Islands.

For parking lots in excess of ten spaces all rows of parking spaces must provide terminal a minimum of six feet in width to protect parked vehicles, provide visibility, confine traffic to aisles and driveways, and provide a minimum of five feet of space for landscaping. In addition, when ten or more vehicles would be parked side-by-side in an abutting configuration, interior landscaped islands a minimum of eight feet wide must be located within the parking row. For parking lots greater than fifty parking spaces, the location of interior landscape island shall be allowed to be consolidated for planting of large stands of trees to break up the scale of the parking lot.

The number of trees required in the interior landscape area shall be dependent

¹ not accounting for the mirrors or the loading/unloading apparatus for wheelchair bound students

upon the location of the parking lot in relation to the building and public right-of-way:

- a. Where the parking lot is located between the building and the public right-of-way, one tree for every four spaces;**

Applicant's Findings: The proposal is in compliance with the requirements of this section (See Exhibit "B").

- b. Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every six spaces;**

Applicant's Findings: The parking lot is located north of (in "front" of) school buildings and adjacent properties (See Exhibit "B"). This standard is not applicable.

- c. Where the parking lot is located behind the building and is not visible from the public right-of-way, one tree for every eight spaces.**

3. *Bioswales.*

The use of bioswales within parking lots is encouraged and may be located within landscape areas subject to site plan and architectural review. The tree planting standards may be reduced in areas dedicated to bioswales subject to site plan and architectural review. [CPMC 17.75.039(G)]

Applicant's Findings: No bioswales are proposed, this standard is not applicable.

H. *Bicycle Parking.*

The amount of bicycle parking shall be provided in accordance with Section 17.64.040 and constructed in accordance with the following standards:

1. *Location of Bicycle Parking.*

Required bicycle parking facilities shall be located on-site in well lighted, secure locations within fifty feet of well-used entrances and not farther from the entrance than the closest automobile parking space. Bicycle parking shall have direct access to both the public right-of-way and to a main entrance of the principal use. Bicycle parking may also be provided inside a building in suitable, secure and accessible locations. Bicycle parking for multiple uses (such as in a commercial center) may be clustered in one or several locations.

Applicant's Findings: Existing bicycle parking facilities are located adjacent to existing "Modular 1" in a well-lit, secure location, within fifty feet of well-used entrances and not farther from the entrance than the closest automobile parking space. The existing metal bike racks are safely separated from the vehicular parking and maneuvering area by the basketball courts located north of the main office adjacent to "Modular 1". Bicycle parking currently has direct access to both the public right-of-way and to a main entrance of the school.

2. *Bicycle Parking Design Standards.*

All bicycle parking and maneuvering areas shall be constructed to the following minimum design standards:

a. *Surfacing.*

Outdoor bicycle parking facilities shall be surfaced in the same manner as a motor vehicle parking area or with a minimum of a three-inch thickness of hard surfacing (i.e., asphalt, concrete, pavers or similar material). This surface will be maintained in a smooth, durable and well-drained condition.

b. *Parking Space Dimension Standard.*

Bicycle parking spaces shall be at least six feet long and two feet wide with minimum overhead clearance of seven feet.

c. *Lighting.*

Lighting shall be provided in a bicycle parking area so that all facilities are thoroughly illuminated and visible from adjacent sidewalks or motor vehicle parking lots during all hours of use.

d. *Aisles.*

A five-foot aisle for bicycle maneuvering shall be provided and maintained beside or between each row of bicycle parking.

e. *Signs.*

Where bicycle parking facilities are not directly visible from the public rights-of-way, entry and directional signs shall be provided to direct bicycles from the public rights-of-way to the bicycle parking facility.

Applicant's Findings: Please see findings below, addressing CPMC Section 17.75.039(H)(3), requesting an exception to bicycle parking requirements in an effort to avoid unnecessary and costly improvements because the district has identified that the existing bicycle parking facilities are already "under-utilized".

3. *Exceptions to Bicycle Parking.* The approving authority may allow exceptions to the bicycle parking standards as part of the site plan and architectural review process in connection with the following:

a. Temporary uses such as Christmas tree sales; or

b. Uses that do not generate the need for bicycle parking per a bicycle parking demand analysis that demonstrates and documents justification for the proposed reduction. [CPMC 17.75.039(H)]

Applicant's Findings: The proposed improvements do not generate the need for additional bicycle parking than that which already exists onsite (75 spaces). Please see the letter dated March 30, 2022 from the school district, addressing the use or, lack thereof, of existing bicycle parking

facilities (See Exhibit “J”).

17.75.040: Building design standards. *The following building design standards are established to maintain and enhance the small town character of the city: 17.75.041 - Residential building design standards, 17.75.042 - Commercial building design standards and 17.75.043 - Industrial building design standards. [CPMC 17.75.040]*

Applicant’s Findings: No structures are proposed within the current, Phase 1, site plan review application.

17.75.060: Exceptions. *Exceptions to the standards set forth in this subsection shall be processed as a Class “A” variance per Chapter 17.13. (Ord. 1946 (part), 2011). [CPMC 17.75.060]*

Applicant’s Findings: This provision conflicts with the allowances of CPMC Section 17.13.200 which distinguishes “Exceptions and Modifications vs. Variances”. CPMC Section 17.13.200 “Variances – Applicability” subsection “B” states:

“Combining Variances with Other Approvals; Permit Approvals by Other Agencies. Variance requests may be combined with and reviewed concurrently by the city approval body with other land use and development applications (e.g., development review, site design review, subdivision, conditional use, etc.).”


There is no option for exception to the requirements of CPMC Section 17.75.039 (G)(1) and Table 17.75.03 therefore the variance procedure is applicable. Variance Types “A” and “B” do not contain provisions which address the requested variance to the landscaping requirements in the proximity of the proposed bus loop. Therefore, the Class “C”, Type III variance procedure is required and an application for the same has been submitted in conjunction with this application for concurrent review and consideration as part of the total site design review.

E. Conclusion.

Based upon the findings set forth herein and the evidence submitted in conjunction with this Application, the Applicant respectfully contends that the applicable standards and criteria have been met or identified as feasibly met through conditions of approval and the Application should be approved.

Dated this 31st day of March 2022.

O’CONNOR LAW, LLC



Daniel B. O’Connor, OSB No. 950444

C3.1
PARTIAL CIVIL
SITE PLAN

SITE PLAN REVIEW
Project Number: 2024.6
Date: 02/22/2022

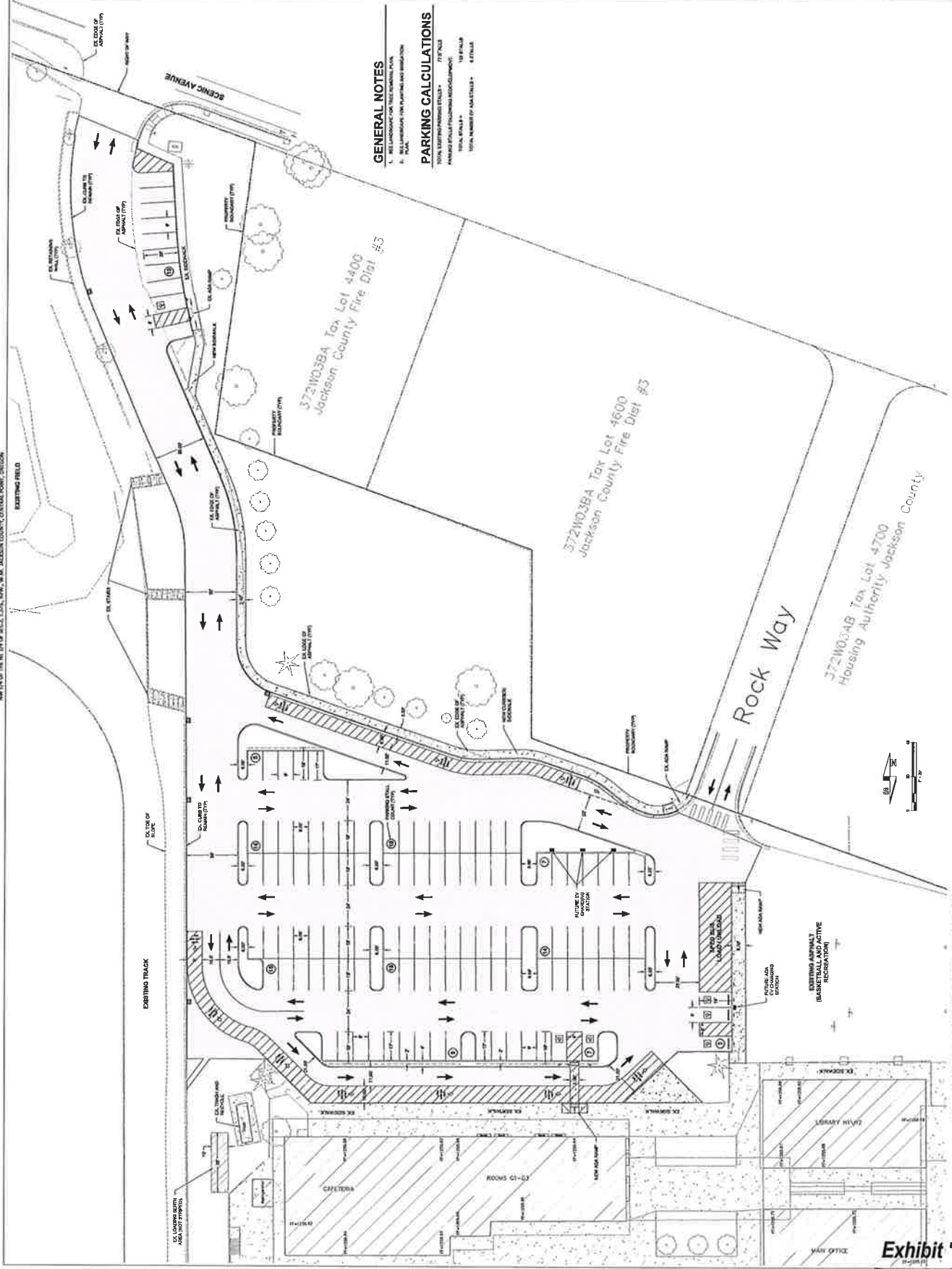
No.	Description	Date

JACKSON COUNTY
DISTRICT #6
SCENIC MIDDLE
SCHOOL PARKING
LOT IMPROVEMENTS
1955 SCENIC AVE.
CENTRAL POINT, OR
97502

GENERAL NOTES
1. SEE LANDSCAPE ARCHITECTURE PLAN FOR PLANTINGS AND IRRIGATION
2. SEE ELECTRICAL PLAN FOR LIGHTING
3. SEE MECHANICAL PLAN FOR HVAC SYSTEMS
4. SEE STRUCTURAL PLAN FOR FOUNDATIONS AND RETAINING WALLS
5. SEE CIVIL PLAN FOR UTILITY LOCATIONS AND DEPTHS
6. SEE TRAFFIC ENGINEERING REPORT FOR TRAFFIC SIGNALS AND SIGNAL PHASING
7. SEE TRAFFIC ENGINEERING REPORT FOR TRAFFIC SIGNALS AND SIGNAL PHASING
8. SEE TRAFFIC ENGINEERING REPORT FOR TRAFFIC SIGNALS AND SIGNAL PHASING
9. SEE TRAFFIC ENGINEERING REPORT FOR TRAFFIC SIGNALS AND SIGNAL PHASING
10. SEE TRAFFIC ENGINEERING REPORT FOR TRAFFIC SIGNALS AND SIGNAL PHASING

POWELL
ARCHITECTS
1955 SCENIC AVE.
CENTRAL POINT, OR 97502
TEL: 531.232.5535
WWW.POWELLARCHITECTS.COM

BBT ARCHITECTS
1140 W. STEVENSON AVE., SUITE 200
BENTON, OREGON 97702
TEL: 531.232.5535 | FAX: 531.238.8033



PLAN VIEW - PARTIAL CIVIL SITE PLAN
SCALE: 1/8" = 1'-0"

Exhibit "B"
Page 3 of 11

C4.0

No.	Description	Date

BBT ARCHITECTS
 1140 W. BENTLEY AVENUE, SUITE 200
 BEND, OREGON 97702
 541.382.5535 | 541.389.8033

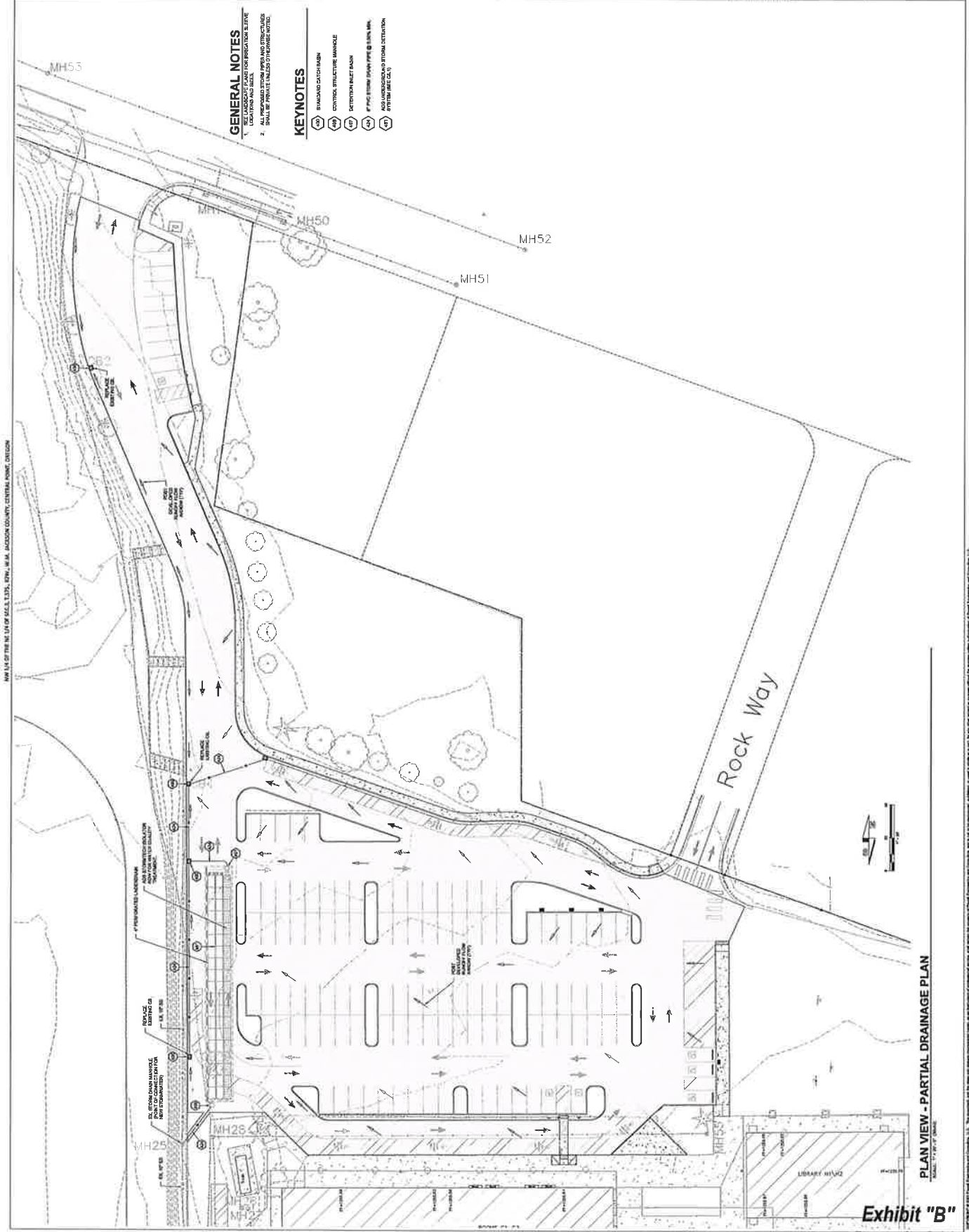


JACKSON COUNTY
 DISTRICT #6
 SCENIC MIDDLE SCHOOL PARKING LOT IMPROVEMENTS

1955 SCENIC AVE.
 CENTRAL POINT, OR 97502

GENERAL NOTES
 1. ALL IMPROVEMENTS SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE OREGON ENGINEERING BOARD'S STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES.
 2. ALL IMPROVEMENTS SHALL BE IN ACCORDANCE WITH THE LATEST EDITIONS OF THE OREGON ENGINEERING BOARD'S STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES.

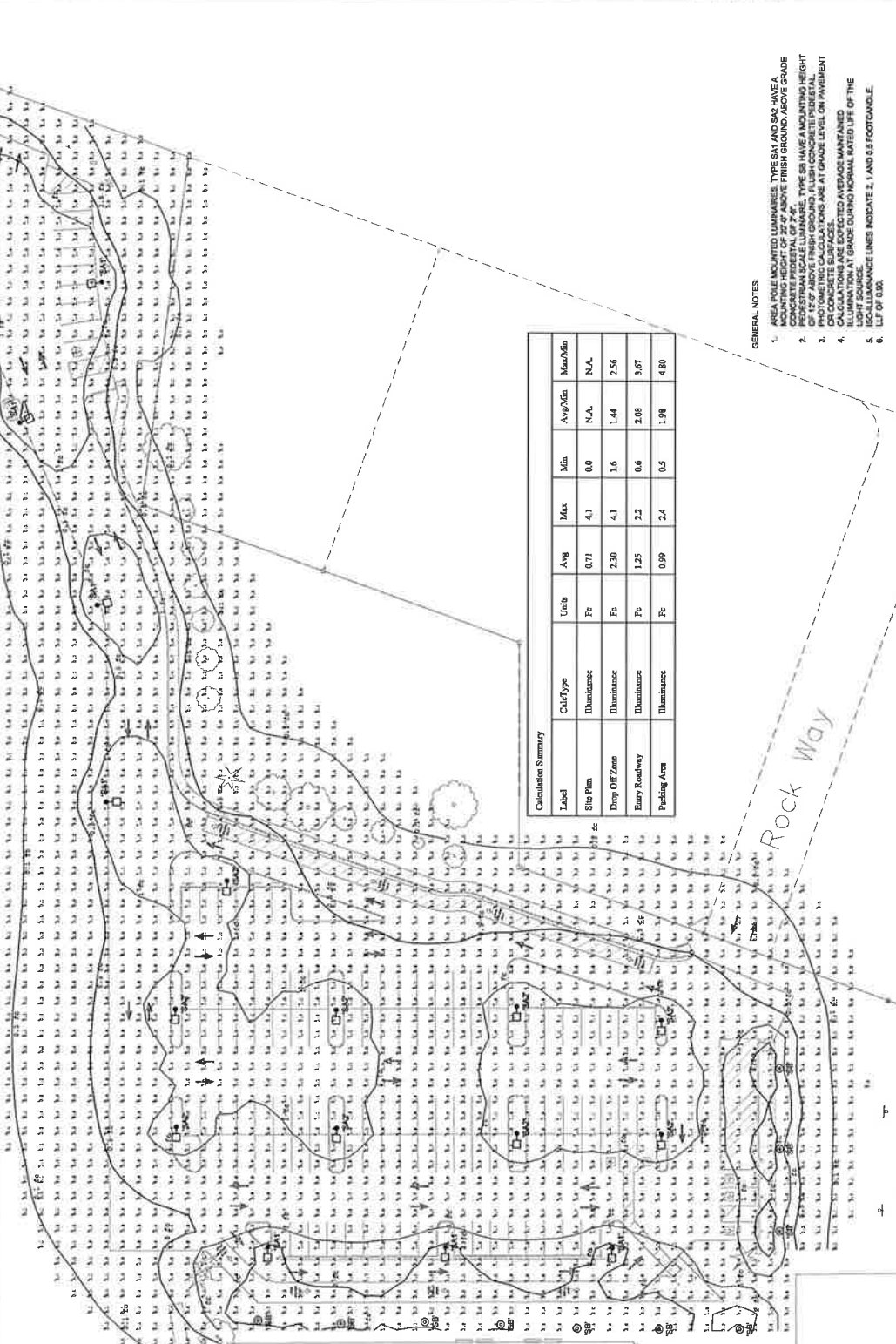
- KEYNOTES**
- ④4 STANDARD CATCH BASIN
 - ④5 CONTROL STRUCTURE MANHOLE
 - ④6 DETENTION INLET BASIN
 - ④7 18" PVC STORM SEWER PIPE @ 2% SLOPE
 - ④8 18" CONCRETE STORM DETENTION PIT WITH 18" DIA. INLET



PLAN VIEW - PARTIAL DRAINAGE PLAN
 SCALE: 1/4" = 1' (SEE SHEET)

LUMINAIRE SCHEDULE

TYPE	DESCRIPTION	HOUSING	SHIELDING	INSTALLATION	DRIVERS	UNIT	MOUNTING HEIGHT	NOTES
1	RECESSED LUMINAIRE POLE MOUNTED	RECESSED ALUMINUM	TYPE 1 SHIELDING	CONCRETE MOUNTING	DRIVERS	100 WATT	15 FT	CONCRETE MOUNTING
2	RECESSED LUMINAIRE POLE MOUNTED	ALUMINUM	TYPE 2 SHIELDING	CONCRETE MOUNTING	DRIVERS	100 WATT	15 FT	CONCRETE MOUNTING
3	RECESSED LUMINAIRE POLE MOUNTED	ALUMINUM	TYPE 3 SHIELDING	CONCRETE MOUNTING	DRIVERS	100 WATT	15 FT	CONCRETE MOUNTING



Calculation Summary

Label	Calc Type	Units	Avg	Max	Min	Avg/Fc	Max/Fc
Site Plan	Illuminance	Fc	0.71	4.1	0.0	N/A	N/A
Drop Off Zone	Illuminance	Fc	2.30	4.1	1.6	1.44	2.56
Entry Roadway	Illuminance	Fc	1.25	2.2	0.6	2.08	3.67
Parking Area	Illuminance	Fc	0.99	2.4	0.5	1.98	4.80

- GENERAL NOTES:
1. ALSO POLE MOUNTED LUMINAIRE, TYPE 311 AND 340 HAVE A MOUNTING HEIGHT OF 20' ABOVE FINISH GROUND, ABOVE GRADE CONCRETE PAVED, OF 7'-6". TYPE 311 HAVE A MOUNTING HEIGHT OF 12'-9" ABOVE FINISH GROUND, FLUSH CONCRETE PAVED, OF 7'-6".
 2. PHOTOMETRIC CALCULATIONS ARE AT GRADE LEVEL ON PAVEMENT.
 3. CALCULATIONS ARE EXPECTED AVERAGE MAINTAINED ILLUMINATION AT GRADE DURING NORMAL WATED LIFE OF THE LUMINAIRE.
 4. ILLUMINATION AT GRADE DURING NORMAL WATED LIFE OF THE LUMINAIRE.
 5. ILLUMINATION LINES INDICATE 2, 1 AND 0.5 FOOTCANDLE.
 6. LLF OF 0.8.

1 PARKING LOT PLAN - PHOTOMETRICS

1140 2nd Street, N.E. - Suite 200
Buck Creek, MO 64601
1-816-223-2333 | Fax: 1-816-223-2333



JACKSON COUNTY
DISTRICT #6
CENTRAL POINT
SCHOOL PARKING
LOT IMPROVEMENTS

1955 SCENIC AVE
CENTRAL POINT, OR
97502

No.	Description	Date

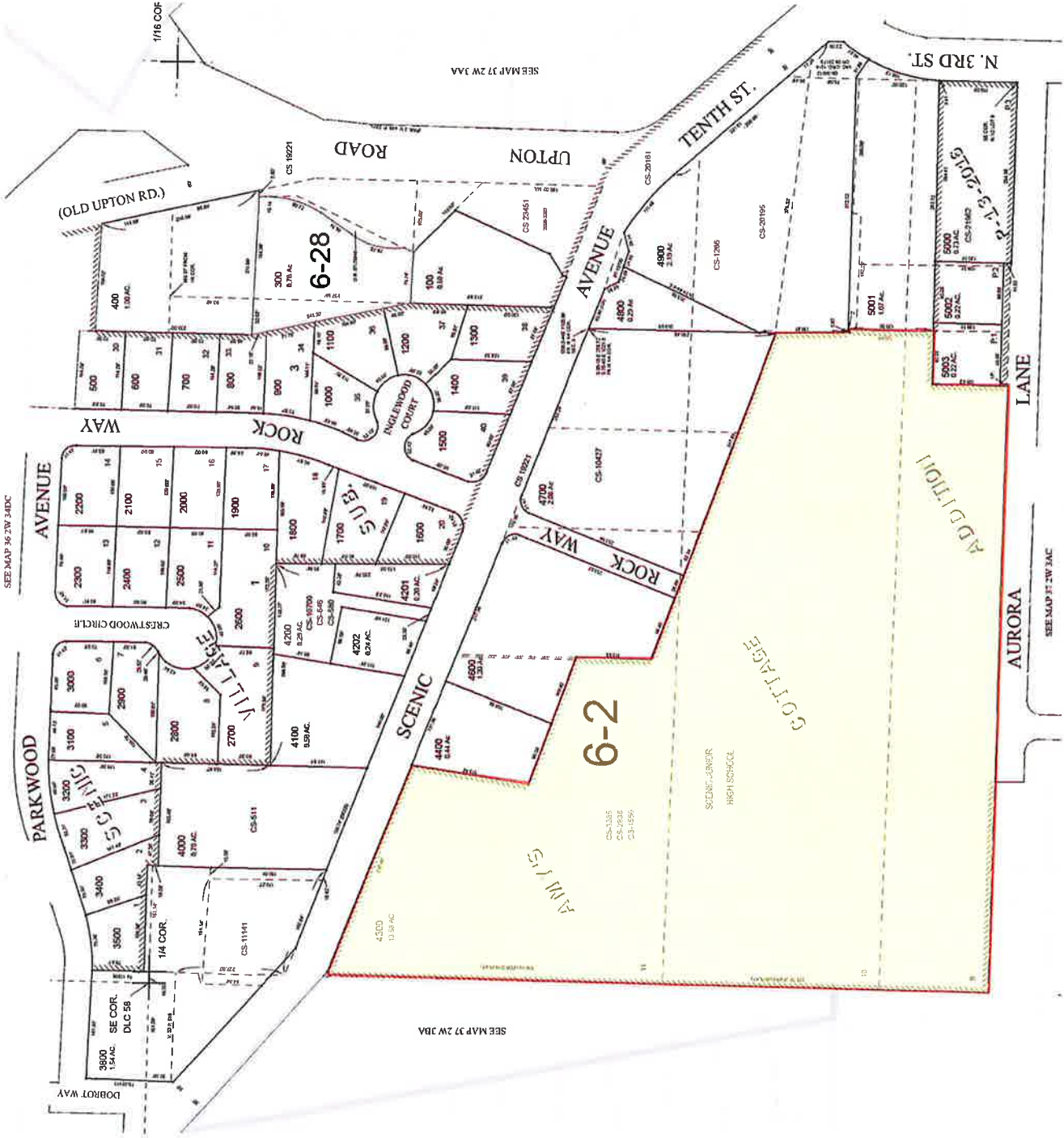
PROJECT NUMBER: 20064
DATE: 09/11/2023

SITE PLAN REVIEW

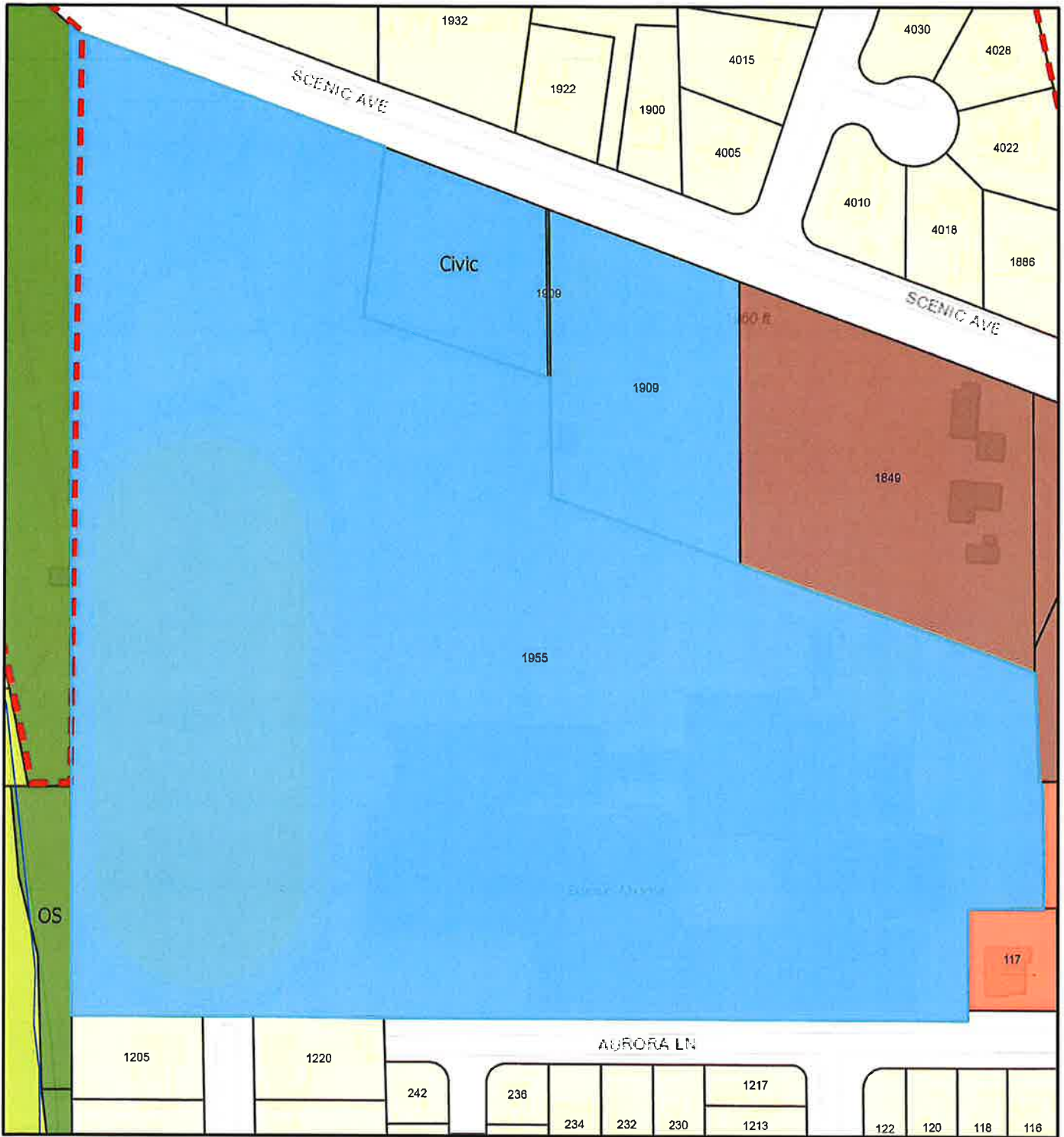
SITE PHOTOMETRICS

E1.1PH

CANCELLED TAX LOT NUMBERS
306 ADDED TO 300
3700 ADDED TO 3600
3790 ADDED TO 3600
3801 ADDED TO 3600
3802 ADDED TO 3600
3901 ADDED TO 3900
3902 ADDED TO 3900
1500 ADDED TO 4600



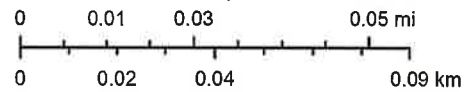
Zoning Map



March 13, 2022

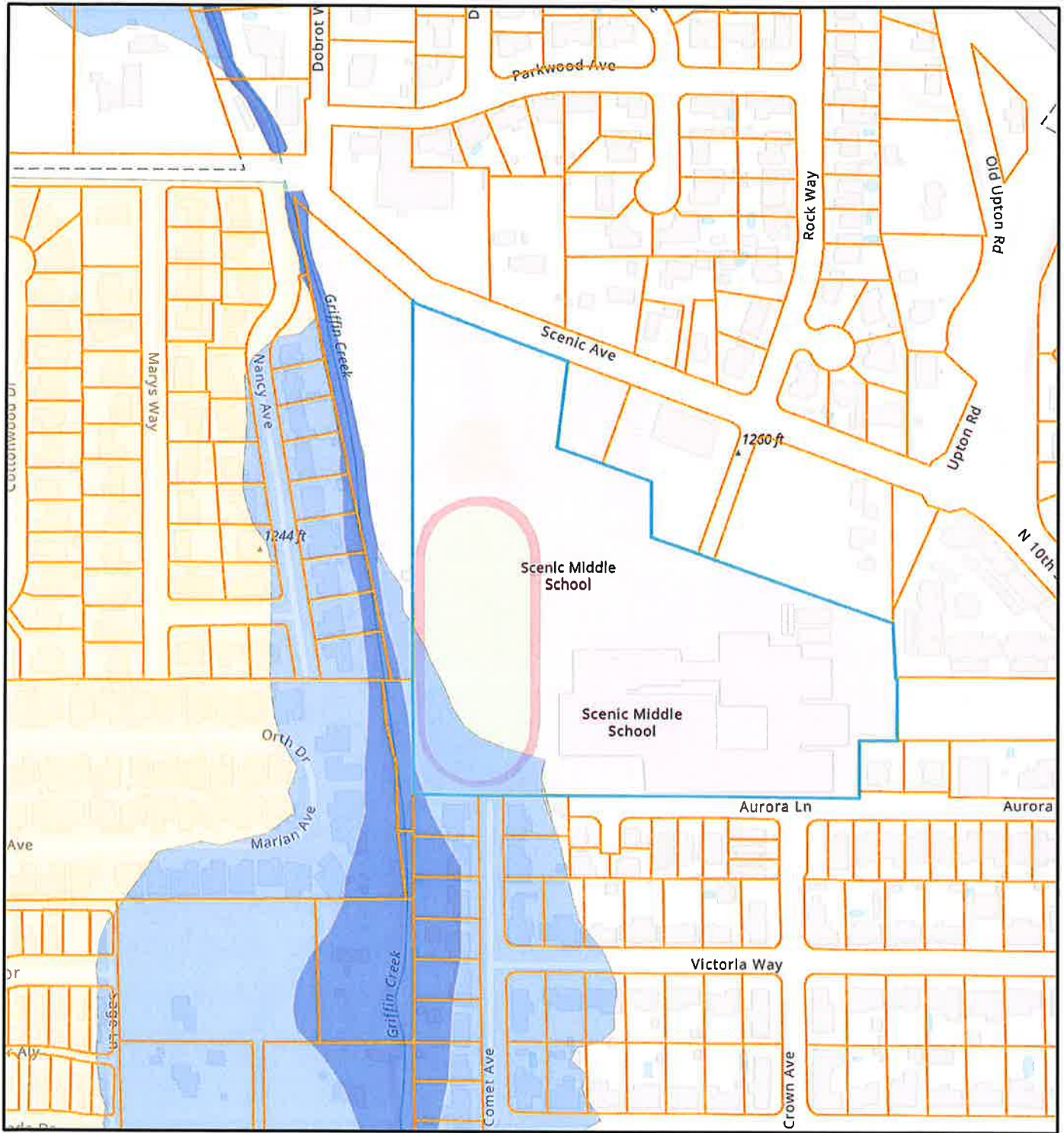
1:2,257





- City Limits
- Streams
- Current Taxlot
- Civic
- OS
- R-3
- R-2
- R-1-6
- R-1-8

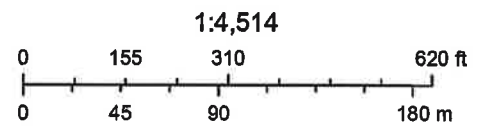


County of Jackson, OR, Bureau of Land Management, State of Oregon, State of Oregon DOT, State of Oregon GEO, Esri Canada, Esri, HERE, Garmin, GeoTechnologies, Inc., USGS, EPA, USDA

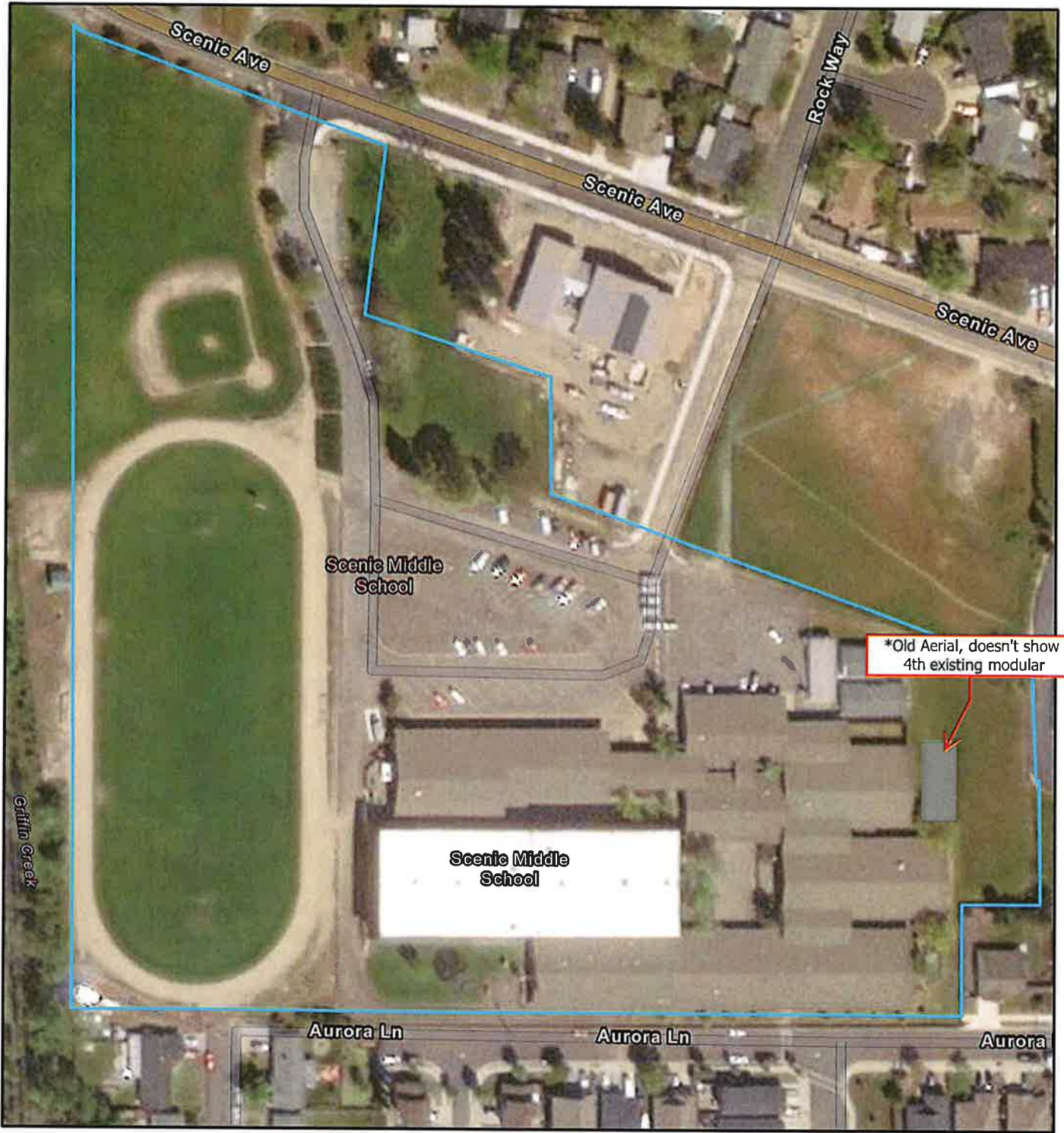
Floodplain



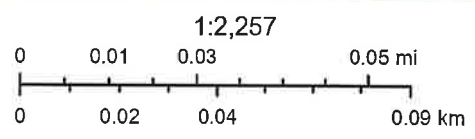
-  Tax Lots
- FEMA Flood Zones**
-  100 Year Determined BFE
-  500 Year Boundary
-  Floodway



Aerial Imagery



March 13, 2022



Maxar, Microsoft, Esri Community Maps Contributors, County of Jackson, OR, State of Oregon GEO, © OpenStreetMap, Microsoft, Esri Canada, Esri, HERE, Garmin, SafeGraph, GeoTechnologies, Inc., METI/NASA, USGS, Bureau of Land Management, EPA, NPS, US Census Bureau, USDA

South facing view of existing landscaping, east of the property entrance located at the northwestern corner of the subject property.



South facing view of existing landscaping, west of the property entrance located at the northwestern corner of the subject property.



View of existing fencing along southern property boundary, abutting Aurora Lane



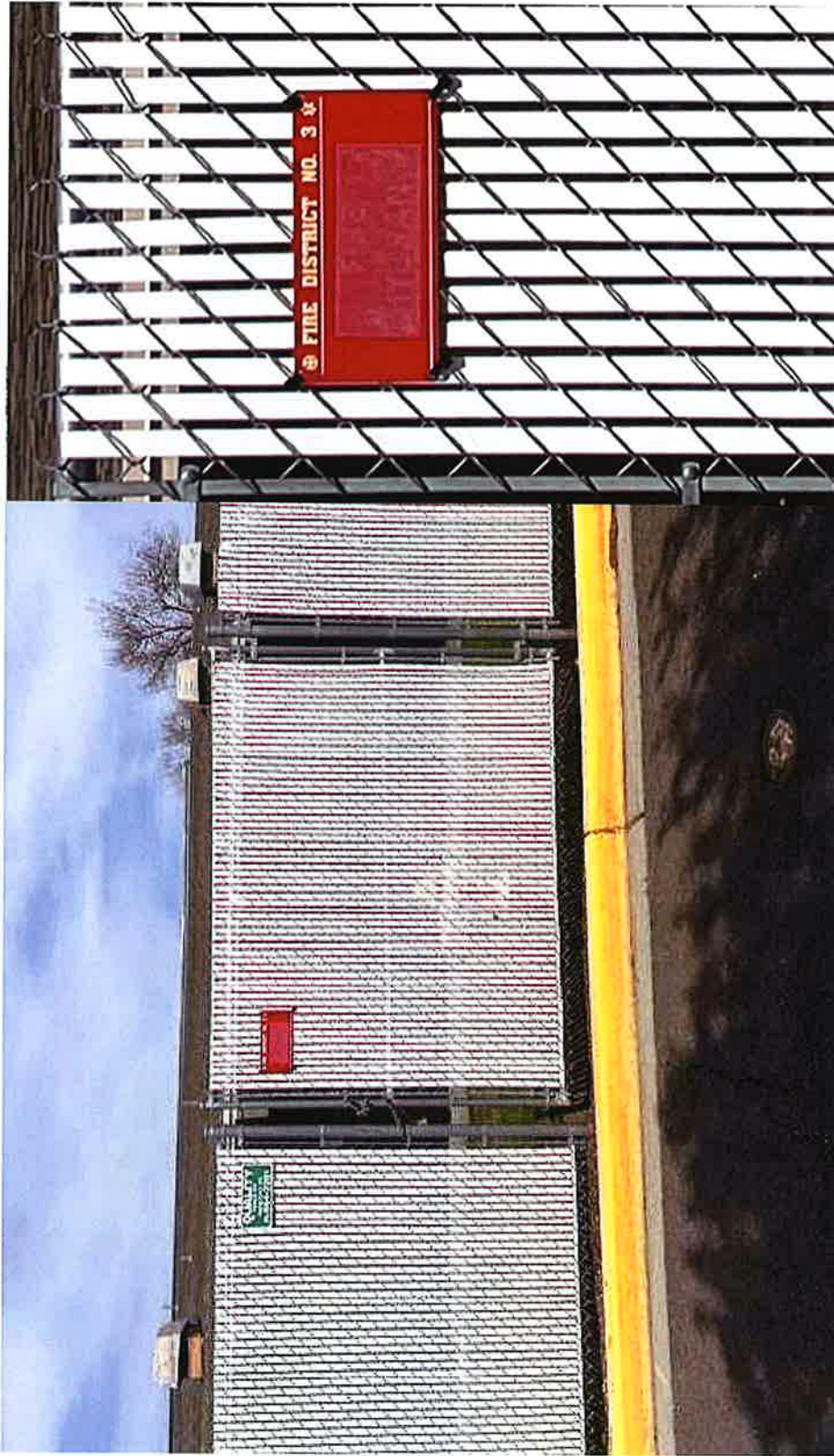
Location bus loop will enter the southeastern corner of the subject property



Pedestrian Crossing and existing fencing along southern property boundary, abutting Aurora Lane



Fire District #3 Signage on fencing along southern boundary (Aurora Lane)



Proposed bus loop egress point on to Comet Avenue



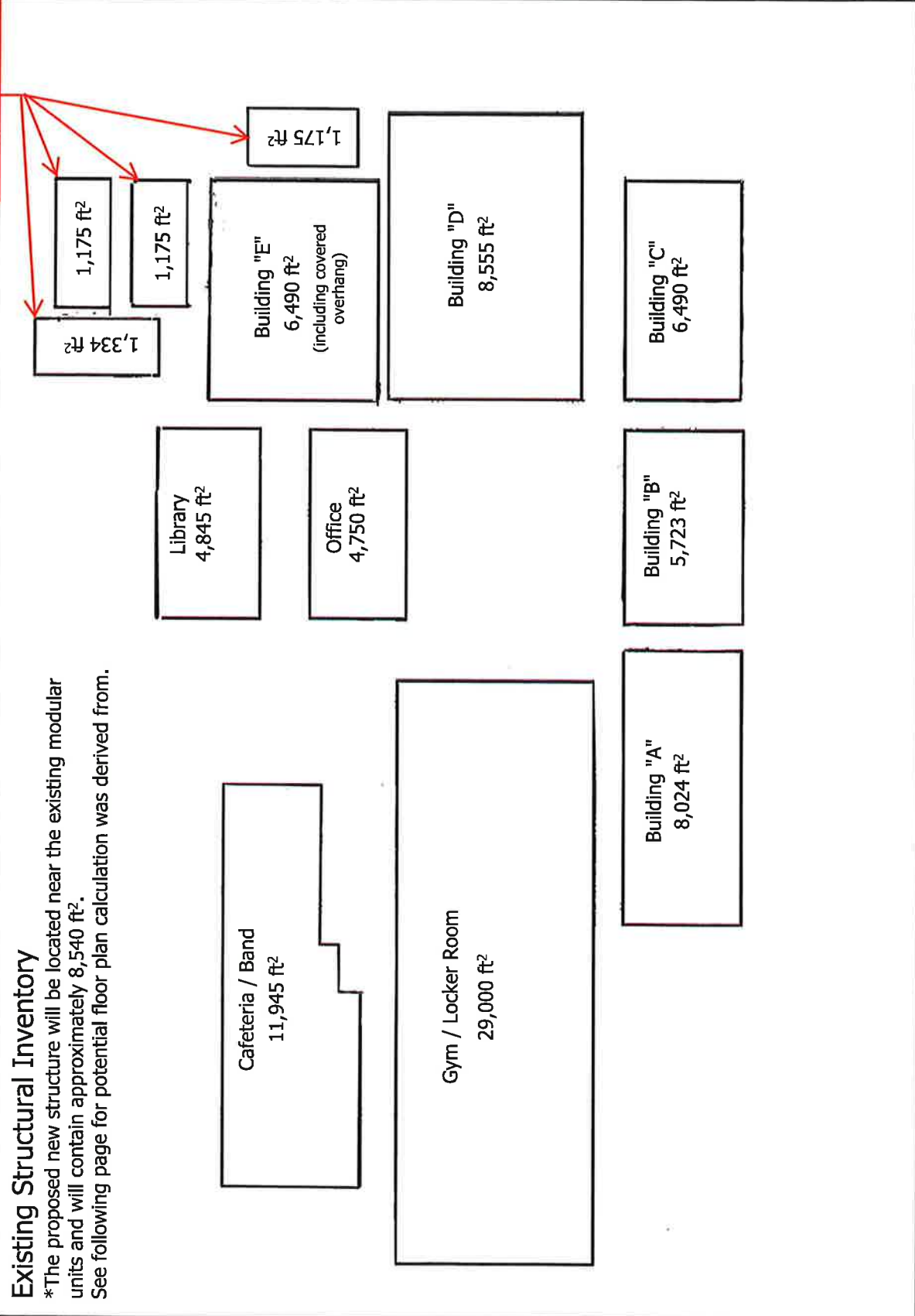
Existing Fencing: Panel with reflective diamond to be replaced with rolling gate.



Existing Structural Inventory

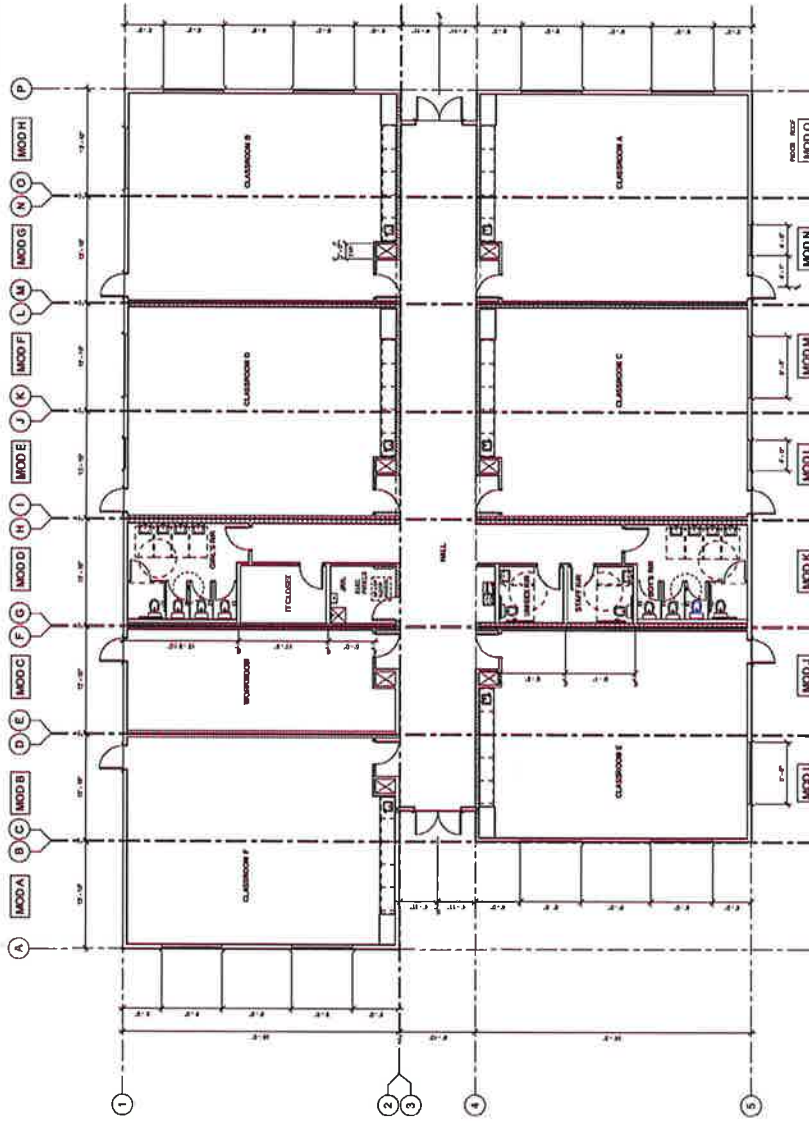
*The proposed new structure will be located near the existing modular units and will contain approximately 8,540 ft². See following page for potential floor plan calculation was derived from.

Existing Modular Units





New Modular Structure to house six (6) new classrooms
 Approximately 8,540 sq/ft







© 2018 ORW ARCHITECTURE, INC.

SCENIC MIDDLE SCHOOL - NEW CLASSROOM ADDITION

2/2/2018 MBM
 10/11/2018

School Bus Size

Bus Type	A 	B 
Weight	Up to 10,000 Pounds	10,000 to 26,500 Pounds
Height	9.1 Feet	10.3 Feet
Width	93 Inches	93 Inches
Wheelbase	138 Inches	138 to 254 Inches
Turning Radius	25 Feet	20.3 to 39.1 Feet
Length	13 to 17.5 Feet	10.8 to 21.7 Feet

Bus Type	C 	D 
Weight	18,000 to 35,000 Pounds	27,800 to 37,000 Pounds
Height	9.9 to 10.4 Feet	9.9 to 10.4 Feet
Width	93 to 96 Inches	93 to 96 Inches (8 Feet)
Wheelbase	149.6 to 276 Inches	136 to 276 Inches
Turning Radius	23 to 38 Feet	21.3 to 34.2 Feet
Length	20.9 to 38.9 Feet	27.3 to 39.11 Feet



WALT DAVENPORT — DISTRICT SUPERINTENDENT

ADMINISTRATION OFFICE
300 ASH STREET
CENTRAL POINT, OREGON 97502
PHONE: (541) 494-6200
FAX: (541) 664-1637
www.district6.org



March 30, 2022

City of Central Point
140 S. 3rd Street
Central Point, OR 97502

To Whom It May Concern:

The Central Point School District has been assessing bicycle parking needs at Scenic Middle School for some time based on staff observations of the existing bicycle parking facilities throughout the school year. There are currently 75 bicycle parking spaces at Scenic Middle School. Although there are seasonal variations in the usage of the existing bicycle parking facilities, it has never been observed to exceed fifty-percent (50%) capacity. Thus, based on our demand analysis, the existing bicycle parking facilities should be more than sufficient to accommodate the future six (6) classrooms at the school. In the unlikely event there is a future need for additional parking facilities, the District will promptly install such facilities in compliance with Section 17.75.039(H)(2) of the Central Point Municipal Code.

Sincerely,



Spencer Davenport
Jackson County School District No. 6 (Central Point)
Chief Projects Officer
Tel. (541) 494-6211
Spencer.davenport@district6.org

.....CHARACTER MATTERS.....

**AGENT AUTHORIZATION
CITY OF CENTRAL POINT, OREGON**

LET IT BE KNOWN that Daniel O'Connor of O'Connor Law, LLC has been retained to Act as Agent regarding a Site Plan Review application and a Variance application for the property identified below, which is owned by the undersigned.


Address or Road: 1955 Scenic Avenue

AS DESCRIBED IN THE RECORDS OF JACKSON COUNTY AS:


TOWNSHIP: 37 **RANGE:** 2W **SECTION:** 03B **TAX LOT:** 4300


This authorization is valid for ONE YEAR.

PROPERTY OWNER: Central Point School District #6

Signature:  _____ **Date:** 3/14/22
Printed Name: Spencer Davenport
Address: 300 Ash Street, Central Point, Oregon 97502-2279
Phone: 541-494-6201

AGENT: Daniel O'Connor or Kate J. McGuire, O'Connor Law, LLC

Signature:  _____ **Date:** 02/28/2022
Printed Name: Daniel O'Connor
Address: 670 G Street, Suite B, Jacksonville, Oregon 97530-6501
Phone: 541-702-5350

Signature:  _____ **Date:** 02/28/2022
Printed Name: Kate J. McGuire
Address: 670 G Street, Suite B, Jacksonville, Oregon 97530-6501
Phone: 541-702-5350 (Ext. #2)