

Findings of Fact
Crater High School Parking Lot Improvements
Type II Site Plan Application

Prepared by:

Powell Engineering and Consulting
(Civil Engineer)

Submittal Date:

May 15, 2020

Procedure Type: Type II – Director’s Decision

Project Type: Major Site Plan Application and Architectural Review

Owner: Jackson County School District #6
300 Ash Street
Central Point, OR 97502

Applicant: Same as Owner

Representative: BBT Architects
1140 SW Simpson Avenue, Suite 200
Bend, OR 97702

Civil Engineer: Powell Engineering and Consulting, LLC
221 N. Central Ave., PMB 221
Medford, OR 97501

Property Location: 655 N. 3rd Street, Central Point, OR 97502

Map and Tax Lot: 37-2W-03-DB, Tax Lot 100

Zoning District: Civic

I. PROPOSAL:

The Applicant is seeking to improve the existing gravel parking lot located in the northern portion of the existing Crater High School Campus.

The proposed improvements include approximately 160,000 square feet of new paving for 421 parking spaces. Scope of work includes a new pavement section per geotechnical engineers recommendations (pending), curb and landscape islands as specified, new underground utilities for stormwater management and site lighting, replacement of existing utilities as required for construction, ADA pathways, sidewalks, and pedestrian connectivity, striping, biofiltration swales, planting and irrigation, demolition of the existing batting cages and tennis court, and erosion control.

II. CONFORMANCE WITH APPLICABLE CRITERIA - PROPOSED FINDINGS

**Chapter 17.64
OFF-STREET PARKING AND LOADING**

TABLE 17.64.02B

NON-RESIDENTIAL OFF-STREET PARKING REQUIREMENTS

Use Categories	Minimum and Maximum Vehicle Parking Requirement (fractions rounded down to the closest whole number)
<i>High Schools, Colleges, and Trade Schools (public and private/parochial)</i>	<i>1 space per each 5 students, based on the design capacity of the facility, or 1 space per 4 seats in the main auditorium, gymnasium, or other place available for public assembly, whichever is greater.</i>

A. Calculation of Required Off-Street Parking. Off-street parking facility requirements set forth in Table 17.64.02A, Residential Off-Street Parking Requirements, and Table 17.64.020B, Non-Residential Off-Street Parking Requirements, shall be applied as follows:

- 1. Where the application of the schedule results in a fractional requirement it shall be rounded down to the lowest whole number.*
- 2. For purposes of this chapter, gross floor area shall not include enclosed or covered areas used for off-street parking or loading, or bicycle facilities.*
- 3. Where uses or activities subject to differing requirements are located in the same structure or on the same site, or are intended to be served by a common facility, the total parking requirement shall*

be the sum of the requirements for each use or activity computed separately, except as adjusted through the site plan and architectural review process under the provisions of subsection (B) of this section. The community development director, when issuing a permit(s) for multiple uses on a site, may restrict the hours of operation or place other conditions on the multiple uses so that parking needs do not overlap and may then modify the total parking requirement to be based on the most intense combination of uses at any one time.

4. Where requirements are established on the basis of seats or person capacity, the building regulations provisions applicable at the time of determination shall be used to define capacity.

5. Where residential use is conducted together with or accessory to other permitted uses, applicable residential requirements shall apply in addition to other non-residential requirements.

6. The parking requirements outlined in Table 17.64.02A, Residential Off-Street Parking Standards, and Table 17.64.020B, Non-Residential Off-Street Parking Requirements, include parking for handicapped persons shall be provided pursuant to the requirements of subsection C of this section, Accessible Parking Requirements.

FINDING: There are 1,327 students at the high school as of 4/21/20. The existing Main Gymnasium, an A-4 occupancy with fixed seats, has 1,532 bleacher seats. Using the Assembly-Concentrated occupant load factor (7 net sf/person), the total number increases to 1,669 seats.

One space per five students equals 266 total spaces required. Four spaces per 1669 seats equals 418 spaces required. This new parking lot proposes 421 new spaces. In addition, there are two additional existing parking lots on the school campus with approximately 130 additional spaces. Therefore, we meet the minimum required parking spaces of 418.

B. Adjustments to Non-Residential Off-Street Vehicle Parking. The off-street parking requirements in Table 17.64.02B, Non-Residential Off-Street Parking Requirements, may be reduced, or increased in any commercial (C) or industrial (M) district as follows:

1. Reductions. The maximum off-street parking requirements may be reduced by no more than twenty percent.

2. Increases. The off-street parking requirements may be increased based on a parking demand analysis prepared by the applicant as part of the site plan and architectural review process. The parking demand analysis shall demonstrate and documents justification for the proposed increase.

C. *Accessible Parking Requirements.* Where parking is provided accessory to a building, accessible parking shall be provided, constructed, striped, signed and maintained as required by ORS [447.233](#), and Section 1106 of the latest Oregon Structural Specialty Code as set forth in this section.

1. *The minimum number of accessible parking spaces shall be provided for all uses in accordance with the standards in Oregon Structural Specialty Code, Minimum Number of Accessible Parking Spaces. Accessible parking spaces shall be counted toward meeting off-street parking requirements in Tables 17.64.02A and 17.64.02B, Residential and Non-Residential Off-Street Parking Requirements. The accessible parking requirements are minimum requirements and are not subject to reductions per subsection (B)(1) of this section.*

FINDING: The new parking lot is proposing 421 new parking spaces. Per Oregon Structural Specialty Code, the minimum number of accessible parking spaces for total number of stalls between 401 and 500 is nine (9) accessible parking spaces with a minimum of 2 van aisles. As shown on C3.0, we are proposing 11 new accessible parking spaces with 5 van aisles.

D. *Shared Parking.* Required parking facilities for two or more uses, structures, or parcels of land in any commercial (C) or industrial (M) district may be satisfied by the same parking facilities used jointly, to the extent that the owners or operators show that the need for parking facilities does not materially overlap (e.g., uses primarily of a daytime versus nighttime nature; weekday uses versus weekend uses); and provided, that prior to the issuance of any building permit for the property that the right of joint use is evidenced by a recorded deed, lease, contract, or similar written instrument establishing the joint use. The city may approve owner requests for shared parking through the site plan and architectural review process.

FINDING: No shared parking is proposed as part of this application.

E. *Off-Site Parking.* Except for single-family dwellings, the vehicle parking spaces required by this chapter may be located on another parcel of land, provided the parcel is within three hundred feet of the use it serves and the city has approved the off-site parking through the site plan and architectural review process. The distance from the parking area to the use shall be measured from the nearest parking space to a building entrance, following a sidewalk or other pedestrian route. The right to use the off-site parking must be evidenced by a recorded deed, lease, easement, or similar written instrument in the same manner as set forth in subsection (A)(3) of this section.

FINDING: No parking is proposed off-site as part of this application.

F. Mixed Uses. If more than one type of land use occupies a single structure or parcel of land, the total requirements for off-street automobile parking shall be the sum of the requirements for all uses, unless it can be shown that the peak parking demands are actually less (see subsection D of this section, Shared Parking).

FINDING: No mixed uses or multiple land uses are existing or proposed on the subject property.

G. Compact Car Adjustment.

FINDING: No compact parking spaces are proposed.

H. Change of Use. Prior to the change of use of a building or structure the applicant shall demonstrate that adequate parking spaces are available to accommodate the new use(s) as required in this chapter.

FINDING: There is no change of use as part of this application.

I. Bicycle Parking. Bicycle parking shall be provided in accordance with Table 17.64.04, Bicycle Parking Requirements.

FINDING: As no new structures are proposed as part of this application, Bicycle Parking requirements do not apply.

Chapter 17.65

TOD Districts and Corridors

17.65.040 Land use--TOD district.

Four special zone district categories are applied in the Central Point TOD districts. The characteristics of these zoning districts are summarized in subsections A through D of this section.

C. C--Civic (TOD). Civic uses such as government offices, schools, and community centers are the primary uses intended in this district. These uses can play an important role in the vitality of the TOD district.

FINDING: The subject property is within the TOD district and consists of an existing Public High School that is located within the City's Civic zone.

17.65.050 Zoning regulations--TOD district.

A. Permitted Uses. Permitted uses in Table 1 are shown with a “P.” These uses are allowed if they comply with the applicable provisions of this title. They are subject to the same application and review process as other permitted uses identified in this title.

FINDING: Schools are identified as a Permitted Use in the (C) zone within the TOD, as shown on Table 1 below.

Table 1 TOD District Land Uses							
Use Categories	Zoning Districts						
	LMR	MMR	HMR	EC	GC	C	OS
<u>Civic</u>							
Schools	C	C	C	N	N	P	L2

E. Dimensional Standards. The dimensional standards for lot size, lot dimensions, building setbacks, and building height are specified in Table 2.

FINDING: Table 2 requires a 20’ side yard setback when adjacent to a residential district. This applies to the northern property line, adjacent to the new parking lot. We are providing up to a 39-foot setback in the western half of the parking lot to avoid existing trees, a twenty-foot setback in the eastern half of the parking lot, and retaining but planting the variable width (10’ +/-) existing setback along the existing northern driveway access.

Rear, front, and corner setbacks do not apply due to the placement of the parking lot on the existing school campus. Maximum building heights do not apply as no structures are proposed. The existing school campus is well over the minimum landscaped area of 15%.

F. Development Standards.

3. Parking Standards. The off-street parking and loading requirements in Chapter 17.64 shall apply to the TOD district and TOD corridor, except as modified by the standards in Table 3 of this section.

FINDING: The project complies with the off-street parking and loading requirements in Chapter 17.64 and meets Table 3 of this section which requires 2 parking spaces per classroom. There are 41 classrooms at the High School. Therefore, the minimum required

parking spaces is 82 spaces in the TOC District. This project is proposing 421 new parking spaces.

Chapter 17.72
SITE PLAN AND ARCHITECTURAL REVIEW

17.72.020 Applicability.

No permit required under Title [15](#), Buildings and Construction, shall be issued for a major or minor project, as defined in this section, unless an application for site plan and architectural review is submitted and approved, or approved with conditions, as set forth in this chapter.

B. Major Projects. The following are “major projects” for the purposes of the site plan and architectural review process and are subject to Type 2 procedural requirements as set forth in Chapter [17.05](#), Applications and Types of Review Procedures:

1. New construction, including private and public projects, that:
 - a. Includes a new building or building addition of five thousand square feet or more;
 - b. Includes the construction of a parking lot of ten or more parking spaces; or
 - c. Requires one or more variances or conditional use permits and, in the judgment of the director, will have a significant effect upon the aesthetic character of the city or the surrounding area;

FINDING: The project consists of a new private parking lot with ten or more spaces and falls under the Major Projects classification that is subject to Type 2 procedural requirements set forth in Chapter 17.05

17.72.030 Information required.

Application for site plan and architectural review shall be made to the community development department and shall be accompanied by the application fee prescribed in the city of Central Point planning department fee schedule. The application shall be completed, including all information and submittals listed on the official site plan and architectural review application form.

FINDING: As a part of this application, the Applicant has provided the appropriate application fee along with the required checklist items associated with a Type II Director’s Decision application for a new parking lot without structures.

17.72.040 Site plan and architectural standards.

In approving, conditionally approving, or denying any site plan and architectural review application, the approving authority shall base its decision on compliance with the following standards:

A. Applicable site plan, landscaping, and architectural design standards as set forth in Chapter [17.75](#), Design and Development Standards;

FINDING: Architectural design standards do not apply, as no structures are proposed as part of this application. Landscaping is proposed and meets the criteria set forth in Chapter 17.75.

B. City of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction;

FINDING: The project construction drawings shall utilize the City of Central Point Department of Public Works Department Standard Specifications and Uniform Standard Details for Public Works Construction.

C. Accessibility and sufficiency of fire fighting facilities to such a standard as to provide for the reasonable safety of life, limb and property, including, but not limited to, suitable gates, access roads and fire lanes so that all buildings on the premises are accessible to fire apparatus.

FINDING: A minimum 20' wide non-obstructed fire lane with minimum 28' radius turning movements is proposed through the northern, western, and southern portion of the parking lot. Additionally, two existing fire hydrants will remain that serve the existing gravel parking lot.

**Chapter 17.75
DESIGN AND DEVELOPMENT STANDARDS**

17.75.039 Off-street parking design and development standards.

All off-street vehicular parking spaces shall be improved to the following standards:

A. Connectivity. Parking lots for new development shall be designed to provide vehicular and pedestrian connections to adjacent sites unless as a result of any of the following such connections are not possible:

- 1. Topographic constraints;*
- 2. Existing development patterns on abutting property which preclude a logical connection;*
- 3. Traffic safety concerns; or*

4. Protection of significant natural resources.

FINDING: Pedestrian and vehicle connections to the adjacent residential areas are not proposed for this parking lot due to traffic safety and campus security concerns.

B. Parking Stall Minimum Dimensions. Standard parking spaces shall conform to the following standards and the dimensions in Figure 17.75.03 and Table 17.75.02; provided, that compact parking spaces permitted in accordance with Section [17.64.040\(G\)](#) shall have the following minimum dimensions:

- 1. Width--Shall be as provided in column B in Table 17.75.02;*
- 2. Length--Shall reduce column C in Table 17.75.02 by no more than three feet.*

FINDING: All new parking stalls shall be 9' in width by 19' in length (including a 2' overhang at the bioswale islands), meeting City dimension standards. No compact stalls are proposed at this time.

C. Access. There shall be adequate provision for ingress and egress to all parking spaces.

FINDING: There is an existing northern 20' wide driveway that shall remain for ingress and egress. In addition, there is an existing southern 33' wide driveway that shall remain for ingress and shall be re-stripped for left and right turn egress. See C3.0.

D. Driveways. Driveway width shall be measured at the driveway's narrowest point, including the curb cut. The design and construction of driveways shall be as set forth in the Standard Specifications and Public Works Department Standards and Specifications.

FINDING: There are no new driveways proposed. The existing northern 20' wide driveway shall remain. In addition, the existing southern 33' wide driveway shall remain. The project construction drawings shall identify these existing driveways as Add Alternates #1 and #2 for potential re-paving due to the poor condition of the existing asphalt. If these existing drives are re-paved, the width and location shall remain the same.

E. Improvement of Parking Spaces.

- 1. When a concrete curb is used as a wheel stop, it may be placed within the parking space up to two feet from the front of a space. In such cases, the area between the wheel stop and landscaping need not be paved, provided it is maintained with appropriate ground cover, or walkway. In no event shall the placement of wheel stops reduce the minimum landscape or walkway width requirements.*

FINDING: A concrete curb is utilized as a wheel stop in some locations (bioswales). As such, the parking space in these areas have been reduced to 17' in length from 19' to allow for the 2-foot overhang.

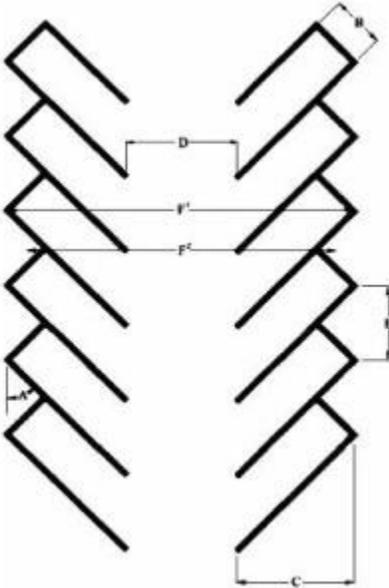


Figure 17.75.03

A = Parking Angle

B = Stall Width

C = Stall to Curb

D = Aisle Width

E = Curb Length per Vehicle

F1 = Maximum Center-to-Center Width of 2-Row Bin

F2 = Nested Center-to-Center Width of 2-Row Bin

TABLE 17.75.02. PARKING DIMENSION SCHEDULE

A	B	C	D	E	F1	F2
0 Degrees (parallel)	8.00	8.00	12.00	23.00	28.00	n.a.
	8.50	8.50	12.00	23.00	29.00	n.a.
	9.00	9.00	12.00	23.00	30.00	n.a.
	9.50	9.50	12.00	23.00	31.00	n.a.
20 Degrees	8.00	14.00	11.00	23.40	39.00	31.50
	8.50	14.50	11.00	24.90	40.00	32.00
	9.00	15.00	11.00	26.30	41.00	32.50
	9.50	15.50	11.00	27.80	42.00	33.10
30 Degrees	8.00	16.50	11.00	16.00	44.00	37.10
	8.50	16.90	11.00	17.00	44.80	37.40
	9.00	17.30	11.00	18.00	45.60	37.80

	9.50	17.80	11.00	19.00	46.60	38.40
40 Degrees	8.00	18.30	13.00	12.40	49.60	43.50
	8.50	18.70	12.00	12.20	49.40	42.90
	9.00	19.10	12.00	14.00	50.20	43.30
	9.50	19.50	12.00	14.80	51.00	43.70
45 Degrees	8.00	19.10**	14.00	11.30	52.20	46.50
	8.50	19.40**	13.50	12.00	52.30	46.30
	9.00	19.80**	13.00	12.70	52.60	46.20
	9.50	20.10**	13.00	13.40	53.20	46.50
50 Degrees	8.00	19.70**	14.00	10.50	53.40	48.30
	8.50	20.00**	12.50	11.10	52.50	47.00
	9.00	20.40**	12.00	11.70	52.80	47.00
	9.50	20.70**	12.00	12.40	53.40	47.30
60 Degrees	8.00	20.40**	19.00	9.20	59.80	55.80
	8.50	20.70**	18.50	9.80	59.90	55.60
	9.00	21.00**	18.00	10.40	60.00	55.50
	9.50	21.20**	18.00	11.00	60.40	55.60
70 Degrees	8.00	20.60**	20.00	8.50	61.20	58.50
	8.50	20.80**	19.50	9.00	61.10	58.20
	9.00	21.00**	19.00	9.60	61.00	57.90
	9.50	21.20**	18.50	10.10	60.90	57.70
80 Degrees	8.00	20.10**	25.00*	8.10	65.20	63.80
	8.50	20.20**	24.00*	8.60	64.40	62.90
	9.00	20.30**	24.00*	9.10	64.30	62.70
	9.50	20.40**	24.00*	9.60	64.40	62.70
90 Degrees	8.00	19.00**	26.00*	8.00	64.00	n.a.

	8.50	19.00**	25.00*	8.50	63.00	n.a.
	9.00	19.00**	24.00*	9.00	62.00	n.a.
	9.50	19.00**	24.00*	9.50	62.00	n.a.
Notes:						
* Two-way circulation						
** Maximum deduction of two feet for overhang when curb serves as wheel stop						

2. All areas utilized for off-street parking, access and maneuvering of vehicles shall be paved and striped to the standards of the city of Central Point for all-weather use and shall be adequately drained, including prevention of the flow of runoff water across sidewalks or other pedestrian areas. Required parking areas shall be designed with painted striping or other approved method of delineating the individual spaces, with the exception of lots containing single-family or two-family dwellings.

FINDING: All parking spaces will be paved and striped, as shown on C3.0. All runoff from the new impervious surfaces will be sheet flow across the new pavement and into curb openings at the Biofiltration Swales or into new catch basins that then convey the runoff to the swales.

3. Parking spaces shall be designed so that no backing movements or other maneuvering within a street or other public right-of-way shall be necessary, except for one- and two-family dwellings with frontage on a local street per the city of Central Point street classification map.

FINDING: As shown on C3.0, no backing movements or other maneuvering will occur within a street or public right of way.

4. Any lighting used to illuminate off-street parking or loading areas shall be so arranged as to direct the light away from adjacent streets or properties.

FINDING: See E3.0 for the location of all proposed new parking lot lights and photometrics. See also attached cut sheet for the proposed luminaire. All light will be directed away from adjacent streets and properties.

5. Service drives shall have a minimum vision clearance area formed by the intersection of the driveway centerline, the street right-of-way line, and a straight line joining the lines through points twenty feet from their intersection.

FINDING: No new service drives are proposed.

6. Parking spaces located along the outer boundaries of a parking lot shall be contained by a curb or a bumper rail so placed to prevent a motor vehicle from extending over an adjacent property line, a public street, public sidewalk, or a required landscaping area.

FINDING: The exterior of the entire parking lot will be contained with a curb. See C3.0.

7. Parking, loading, or vehicle maneuvering areas shall not be located within the front yard area or side yard area of a corner lot abutting a street in any residential (R) district, nor within any portion of a street setback area that is required to be landscaped in any commercial (C) or industrial (M) district.

FINDING: The subject property is in the Civic District. No new parking, loading, or vehicle maneuvering is proposed within a street setback area that is to be landscaped. See C3.0.

8. Except as provided in subsection (E)(3) of this section, all uses, including one- and two-family dwellings on arterial and collector streets, shall provide adequate vehicle turnaround and maneuvering area through the use of aisle extensions and/or turnaround spaces as illustrated in Figure 17.75.04 and 17.75.05. Functionally equivalent turnaround and maneuvering designs may be permitted by the approving authority through the site plan and architectural review process.

FINDING: Vehicle turnaround and maneuvering is met, as shown on C3.0.

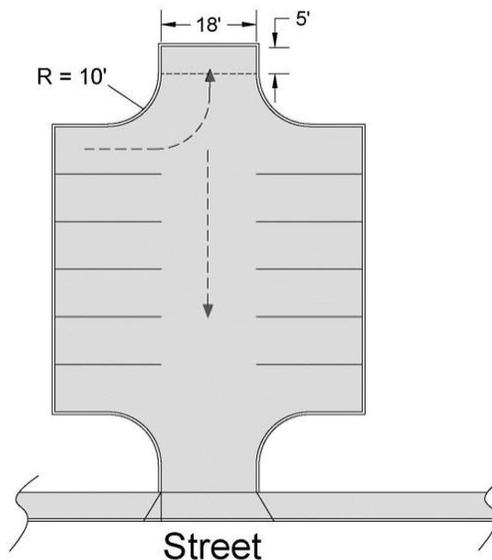


Figure 17.75.04
Parking Lot Minimum Turnaround Standard

F. Limitation on Use of Parking Areas. Required parking areas shall be used exclusively for vehicle parking in conjunction with a permitted use and shall not be reduced or encroached upon in any manner. The parking facilities shall be so designed and maintained as not to constitute a nuisance at any time, and shall be used in such a manner that no hazard to persons or property, or unreasonable impediment to traffic, will result.

FINDING: The proposed parking lot is for exclusive vehicle use in conjunction with the existing permitted use.

G. Parking/Loading Facility Landscaping and Screening. Parking lot landscaping shall be used to reinforce pedestrian and vehicular circulation, including parking lot entries, pedestrian accessways, and parking aisles. To achieve this objective the following minimum standards shall apply; however, additional landscaping may be recommended during the site plan and architectural review process (Chapter [17.72](#)). All parking lots shall be landscaped in accordance with the following standards:

**TABLE 17.75.03
PARKING/LOADING FACILITY PERIMETER AND STREET FRONTAGE LANDSCAPING
STANDARDS**

Street Frontage	Min. Planting Area Width	Plants Required per 100 Lineal Ft. of Street Frontage	
		Trees	Shrubs
Arterial/Collector	15 ft.	4	20
Local	10 ft.	3	15
Perimeter (Abutting) Land Use		Plants Required per 100 Lineal Ft. of Abutting Property	
Residential	20 ft.	4	20
Commercial	10 ft.	3	15
Industrial	5 ft.	2	10

1. Perimeter and Street Frontage Landscaping Requirements. The perimeter and street frontage for all parking facilities shall be landscaped according to the standards set forth in Table 17.75.03.

FINDING: With the exception of the two access driveways, the new parking lot does not have any street frontage.

The proposed parking lot shares its north property line with a residential area and otherwise has no adjacency requiring a buffer. We are providing up to a 39-foot setback in the western half of the parking lot to avoid existing trees, a twenty-foot setback in the eastern half of the parking lot, and retaining but planting the variable width existing setback along the northern driveway access.

There are a number of mature trees already in the buffer zone, some of them straddle the property line. We are counting those trees that are 100% on the schools property as existing trees that would count against the required 4 trees per 100 foot of linear adjacency. The existing trees include three mature Oaks and one smaller deciduous tree.

We are proposing 26 trees along the north buffer zone including European Hornbeams, Chinese Elm, and Thornless Honey Locust. This buffer would require 150 shrubs. We are proposing 152 including emerald Carpet Rubus and Sunset Rockrose.

2. Terminal and Interior Islands. For parking lots in excess of ten spaces all rows of parking spaces must provide terminal a minimum of six feet in width to protect parked vehicles, provide visibility, confine traffic to aisles and driveways, and provide a minimum of five feet of space for landscaping. In addition, when ten or more vehicles would be parked side-by-side in an abutting configuration, interior landscaped islands a minimum of eight feet wide must be located within the parking row. For parking lots greater than fifty parking spaces, the location of interior landscape island shall be allowed to be consolidated for planting of large stands of trees to break up the scale of the parking lot.

The number of trees required in the interior landscape area shall be dependent upon the location of the parking lot in relation to the building and public right-of-way:

- a. Where the parking lot is located between the building and the public right-of-way, one tree for every four spaces;*
- b. Where the parking lot is located to the side of the building and partially abuts the public right-of-way, one tree for every six spaces;*
- c. Where the parking lot is located behind the building and is not visible from the public right-of-way, one tree for every eight spaces.*

FINDING: This lot is not visible from the street frontage of the school and meets the requirement of one tree per 8 spaces. We are providing 57 where 53 are required for the 421 parking spaces proposed in this lot. Trees including varieties chosen from the City of Central Point approved Street Tree list for parking lots.

3. *Bioswales. The use of bioswales within parking lots is encouraged and may be located within landscape areas subject to site plan and architectural review. The tree planting standards may be reduced in areas dedicated to bioswales subject to site plan and architectural review.*

FINDING: The new paved parking lot will be graded in such a manner that all runoff shall be conveyed to one of two Combination Biofiltration Swales and Detention Ponds. These swale/ponds are located at the east and west low points of the development and are shown on C4.0 and are sized per the latest edition of the RVSS Stormwater Management Manual.

END