



STAFF REPORT

May 27, 2020

AGENDA ITEM: VII-A (File No. SPAR-20002)

Consideration of a Site Plan and Architectural Review application for the development of a 6,262 square foot automobile care and tire facility, including parking and landscape areas. The 1.0 acre project site is located at 4245 Table Rock Road within the Tourist and Office Professional (C-4) zoning district and is identified on the Jackson County Assessor's map as 37S 2W 01C Tax Lot 700. **Applicant:** Zaremba Group; **Agent:** Amy Gunter; Rogue Planning & Development Services, LLC.

SOURCE

Justin Gindlesperger, Community Planner II

BACKGROUND

The Applicant proposes to construct an approximately 6,262 square foot automobile care and tire facility along Table Rock Road ("Attachment "A-1"). The site plan for development includes parking, landscaping, and street frontage improvements along Table Rock Road and a private retail street.

Access/Circulation:

A private retail street will provide access to the project site from the public roads. Site access from the private street, on-site circulation and frontage improvements will be completed as part of the current application.

Building Design:

The building provides a façade to Table Rock Road on the west and the private retail street to the south. The building design includes an entrance with store-front style windows and metal awnings above the windows. Architecturally, the building complies with the building design standards in CPMC 17.75.042 with changes in vertical and horizontal massing, changes in color, texture and materials (Attachment "A-2").

Landscape Design:

The landscape plan provides landscape areas along all street frontages, the perimeter of the site and interior landscape around the proposed structures and parking lot islands (Attachment "A-3"). A 15-foot landscape area is provided along the Table Rock Road frontage and a 10-foot landscape area along the private retail street with ample street trees. The site is bordered by a 10-foot landscape area between adjacent properties that includes a mix of shrubs and trees to provide a buffer to future adjacent uses.

Parking:

The proposal includes off-street parking to accommodate employees and customers together with interior and perimeter landscape improvements. The number of spaces provided is consistent with the minimum/maximum required spaces for automotive uses in Table 17.64.02B.

ISSUES:

There are three (3) issues relative to the proposal:

1. **Final Plat/Subdivision Improvements.** The subject property is identified as Lot 5 of a Tentative Plan (SUB-20001) for a commercial subdivision (Attachment “A-4”). The Tentative Plan has been approved by the City, but the subdivision improvements have not been completed and the Final Plat for the subdivision has not been recorded.

Comment: It will be necessary to complete the public and private improvements in order to complete the Final Plat process for the proposed subdivision. Staff recommends Condition of Approval No.1(b) requiring a recorded copy of the final plat and complete subdivision improvements prior to building permit issuance.

2. **Traffic Mitigation.** The subdivision proposes private retail streets to provide access into the development from the public streets. The public streets are Jackson County right-of-ways and the Jackson County Roads Department evaluated the effects of the development on the adjacent streets. Per Roads Department comments, a median is required at the intersection of Table Rock Road and the private retail street in order to limit traffic movements at the intersection.

Comment: Prior to construction of the proposed tire facility, a median is required along Table Rock Road that limits turning movements to right-in, right-out, left-in (RIROLI) to and from the development location. Staff recommends Condition of Approval No. 1(e) requiring the project to comply with Jackson County Roads Department requirements, including installation of intersection improvements at Table Rock Road and the private retail street.

3. **Bicycle Parking.** Per Table 17.64.04, CPMC 17.64.040, bicycle parking must be provided for automobile oriented commercial uses and the proposed use requires a minimum of 2 bicycle parking spaces. Bicycle parking is not provided on the plans.

Comment: Despite the auto-centric nature of the use, traffic to the site is generated by customers and employees. Staff recommends Condition of Approval No. 1(c), requiring the applicant to provide a revised site plan demonstrating compliance with the minimum bicycle parking standards in Section 17.64.040(I) before building permit issuance.

FINDINGS OF FACT & CONCLUSIONS OF LAW:

The Firestone Complete Auto Care Site Plan and Architectural Review has been evaluated against the applicable Site Plan and Architectural Review Criteria set forth in CPMC 17.72 and CPMC 17.75 and found to comply as conditioned and as evidenced in the Applicant’s Findings and Supplement Findings (Attachments “B”).

CONDITIONS OF APPROVAL:

1. Prior to building permit issuance, the applicant shall:
 - a. Complete construction of the private retail streets as required per the civil improvement and landscape and irrigation plans approved by the Public Works Department.

- b. Demonstrate that all Final Plat requirements for the 8-lot commercial subdivision (SUB-20001) are completed and provide a recorded copy of the Final Plat to the Community Development Department.
- c. Provide a revised site plan that depicts the location of a minimum of two (2) bicycle parking spaces demonstrating conformance with Section 17.67.040(I).
- d. Demonstrate compliance with the following conditions listed in the Public Works Department Staff Report (Attachment “C”):
 - i. Submit a landscape and irrigation plan for landscape and street tree installation along Table Rock Road and the private retail streets.
 - ii. Submit civil improvement plans to the Public Works Department for the street frontage improvements and street construction. The applicant shall use the 2014 revised Public Works Standards and Specifications for all new construction drawings.
 - iii. Submit a stormwater management plan for the entire tax lot demonstrating compliance with the MS4 Phase II stormwater quality standards.
 - iv. Apply for an erosion and sediment control permit (NPDES 1200-CN) and provide a copy to the Public Works Department.
 - v. Pay all System Development Charges and permit fees.
- e. Demonstrate compliance with the following conditions listed in the Jackson County Roads Department Staff Report (Attachment “D”):
 - i. Obtain a minor road improvement permit for the installation of the intersection median and sidewalks along Table Rock Road.
 - ii. Submit civil improvement plans to Jackson County Roads Department for the intersection median improvements.
 - iii. Obtain utility permits for any utility work in Table Rock Road right-of-way.
 - iv. If Jackson County storm drain facilities are to be used, the applicant’s engineer shall provide a hydraulic analysis and storm drain facilities analysis for review and approval.
- f. Demonstrate compliance with the following conditions listed in the Rogue Valley Sewer Services Staff Report (Attachment “E”):
 - i. Submit construction plans, prepared per RVSS standards, for approval.

- ii. Obtain a sewer service permit from RVSS. This permit will be issued by RVSS upon submittal of appropriate plans and payment of appropriate fees.
 - g. Demonstrate compliance with the conditions listed in the Building Department Staff Report for fire sprinkler requirements with the bulk storage of tires (Attachment “F”).
 - h. Provide a recorded copy of an Avigation, Noise and Hazard Easement to the Community Development Department (Attachment “G”).
2. Prior to Public Works Final Inspection, the applicant shall demonstrate compliance with the following:
- a. Complete Table Rock Road frontage improvements and construction of the private retail streets as required per the civil improvement and landscape and irrigation plans approved by the Public Works Department.
 - b. Complete stormwater management improvements per the Stormwater Management Plan approved by the Public Works Department. The Engineer-of-Record shall certify that the construction of the drainage system was constructed per the approved plans.
 - c. Record an operations and maintenance agreement for all new stormwater quality features.

ATTACHMENTS

Attachment “A-1” – Site Plan

Attachment “A-2” – Architectural Elevations

Attachment “A-3” – Landscape Plan

Attachment “A-4” – Table Rock Crossing Tentative Plan

Attachment “B” – Applicant’s Findings

Attachment “C” – Public Works Department Staff Report, dated 05/05/2020

Attachment “D” – Jackson County Roads Department Staff Report, dated 05/11/2020

Attachment “E” – RVSS Staff Report, dated 05/11/2020

Attachment “F” – Building Department Staff Report, dated 05/15/2020

Attachment “G” – Avigation, Noise and Hazard Easement

ACTION

Consider the proposed Site Plan & Architectural Review application and 1) approve; 2) approve with revisions; or 3) deny the application.

RECOMMENDATION

Approve the Site Plan & Architectural Review application for the Firestone Complete Auto Care Facility per the Staff Report dated May 27, 2020 including all attachments thereto herein incorporated by reference.

PRELIMINARY SITE PLAN

CITY, STATE - STREET:
Central Point, OR 4245 Tablerock Road @ Biddle Road

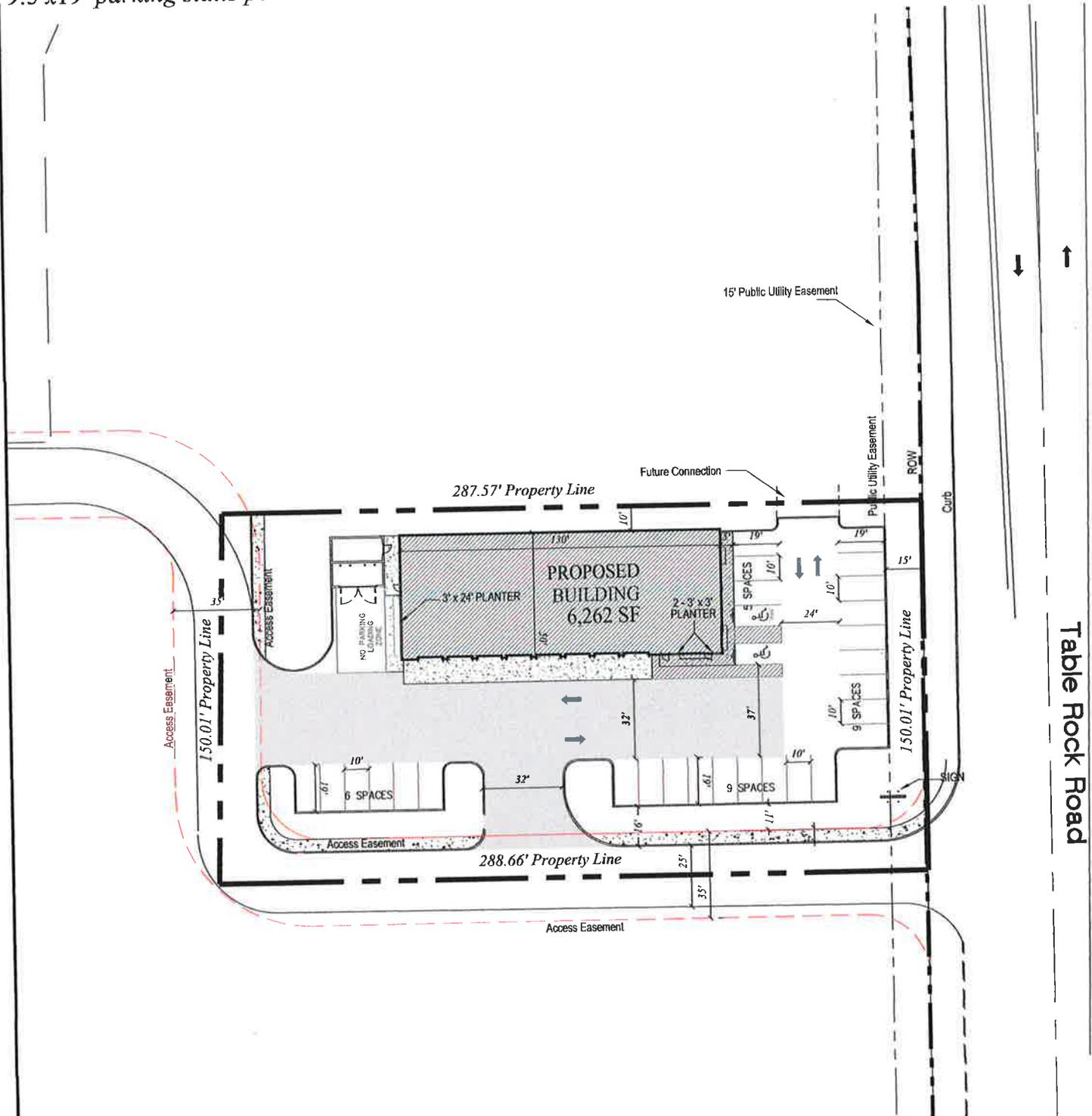


PROTOTYPE: SMALL ER (8 BAY)	DEVELOPER		DESIGNER	DATE:
BLDG SF: 6,262	COMPANY: Zaremba Group	NAME: Dale Stahlberg		02.17.20
LOT ACREAGE: 9.04 Total 1.03 used	NAME: Greg Potts	PHONE: 216-221-6600		
CURRENT ZONING: C-4	PARKING SPACES REQUIRED: 21	PARKING SPACES PROVIDED: 29		

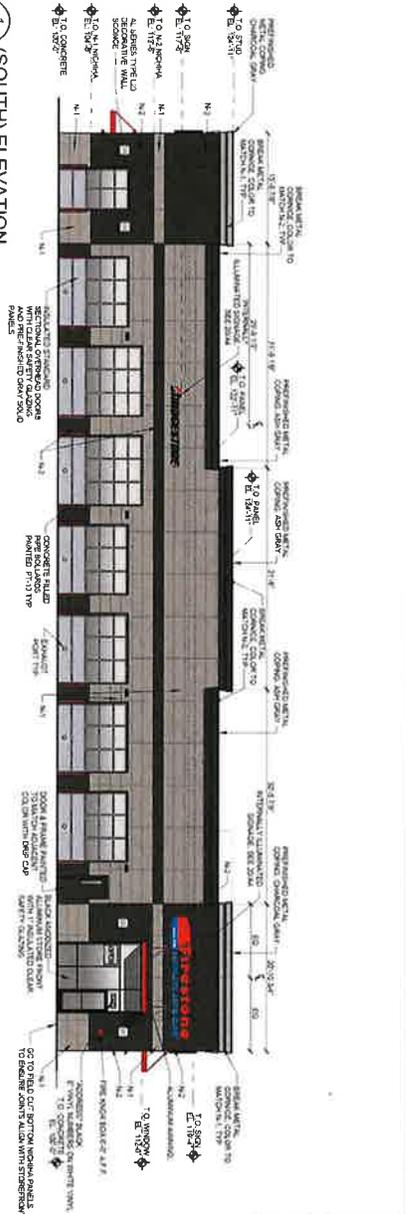
Unusual Conditions:

Site plan is part of a proposed shopping center.
This property is part of a larger tract of land.
Current zoning is C-4 which is the correct zoning for this use.
9.5'x19' parking stalls per code.

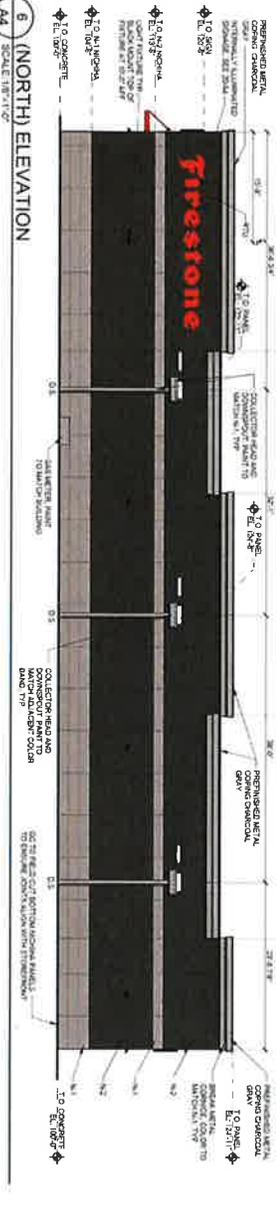
Attachment "A-1"



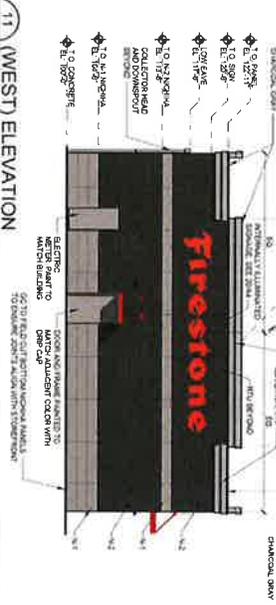
SCALE 1" = NTS



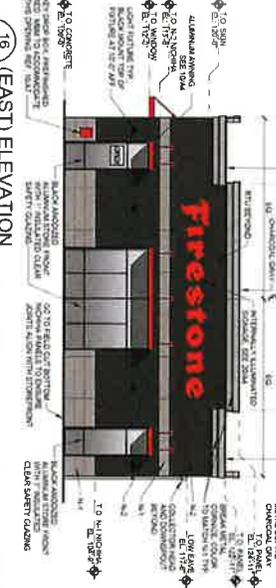
1 (SOUTH) ELEVATION
SCALE: 1/8"=1'-0"



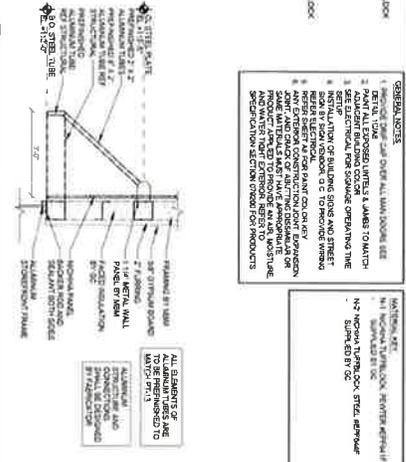
6 (NORTH) ELEVATION
SCALE: 1/8"=1'-0"



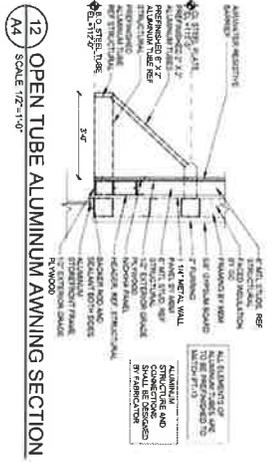
11 (WEST) ELEVATION
SCALE: 1/8"=1'-0"



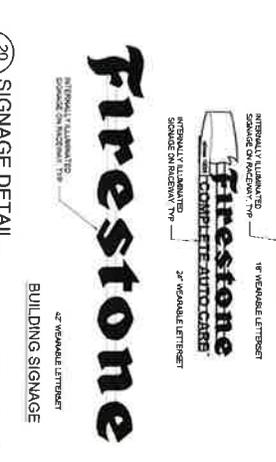
16 (EAST) ELEVATION
SCALE: 1/8"=1'-0"



10 OPEN TUBE ALUMINUM AWNING SECTION
SCALE: 1/2"=1'-0"



12 OPEN TUBE ALUMINUM AWNING SECTION
SCALE: 1/2"=1'-0"



20 SIGNAGE DETAIL
SCALE: 1/4"=1'-0"

CONSTRUCTION DOCUMENTS WILL BE BASED ON THE LATEST RELEASE OF THE PROTOTYPICAL DOCUMENTS ON LUCREREX AT THE TIME OF CONSTRUCTION DOCUMENT AUTHORIZATION.

NOTE: THIS DRAWING IS FOR CONCEPTUAL PURPOSES ONLY. SITE INFORMATION WAS OBTAINED FROM THE CLIENT'S DOCUMENTS; DESIGN MAY VARY.

PROJECT CONTACTS	PHONE #	PROJECT CONTACTS	PHONE #
NS DEV MGR: DOB EHLERS	(616) 721-0883	ARCHITECT: DAN RICE	(616) 597-8600
DESIGN MGR: BRANT HEFLIN	(616) 937-2945	SCA DESIGN GROUP, P.C.	
CONST MGR: JASON AVERETT	(616) 720-4519		
MGR CONST SUPPORT: CHRIS WAGY	(616) 937-8225		
ATTORNEY: NICK MANN	(616) 937-6274	PROTOTYPE: 2020 ER	

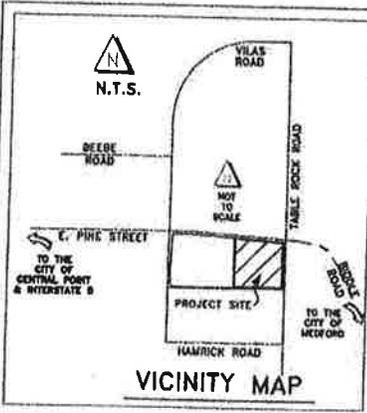
ZONE APPROVAL	(BY/DATE)
VP	
CONT.	
RM	
CM	

4245 TABLE ROCK ROAD
CENTRAL POINT, OR

SSA Architecture, P.C.
1425 North Lincoln, Suite 100
Tualatin, Oregon 97146
www.ssaarchitect.com

EXTERIOR ELEVATIONS & DETAILS

PA4



TENTATIVE PLAT
TABLE ROCK CROSSING SUBDIVISION

LOCATED IN:
PARCEL 2 OF LAND PARTITION P-08-2020
IN THE SW 1/4 OF SECTION 1, T37S, R2W, WM,
CITY OF CENTRAL POINT, JACKSON COUNTY, OR.

BY: GABRIEL L. HUCK	DATE: 03-20-2021
PROJECT NO:	SOUTH SALEM LLC
PLAT NO.:	14-020
PREPARED BY:	BOB LOTT, L.S., T.O.S.
DATE OF PREPARATION:	MARCH 2, 2020
REVISION NO.:	
PROJECT TYPE:	
DATE OF RECORDING:	SURVEY NO. 25578
BOOK AND PAGE:	14-020
EXAMINER'S DATUM:	NAD 83
SCALE:	AS SHOWN
REVISION BY:	

APPLICANT/OWNER
SOUTH SALEM LLC
P.O. BOX 4480
MEDFORD, OREGON 97521
(541) 773-8325

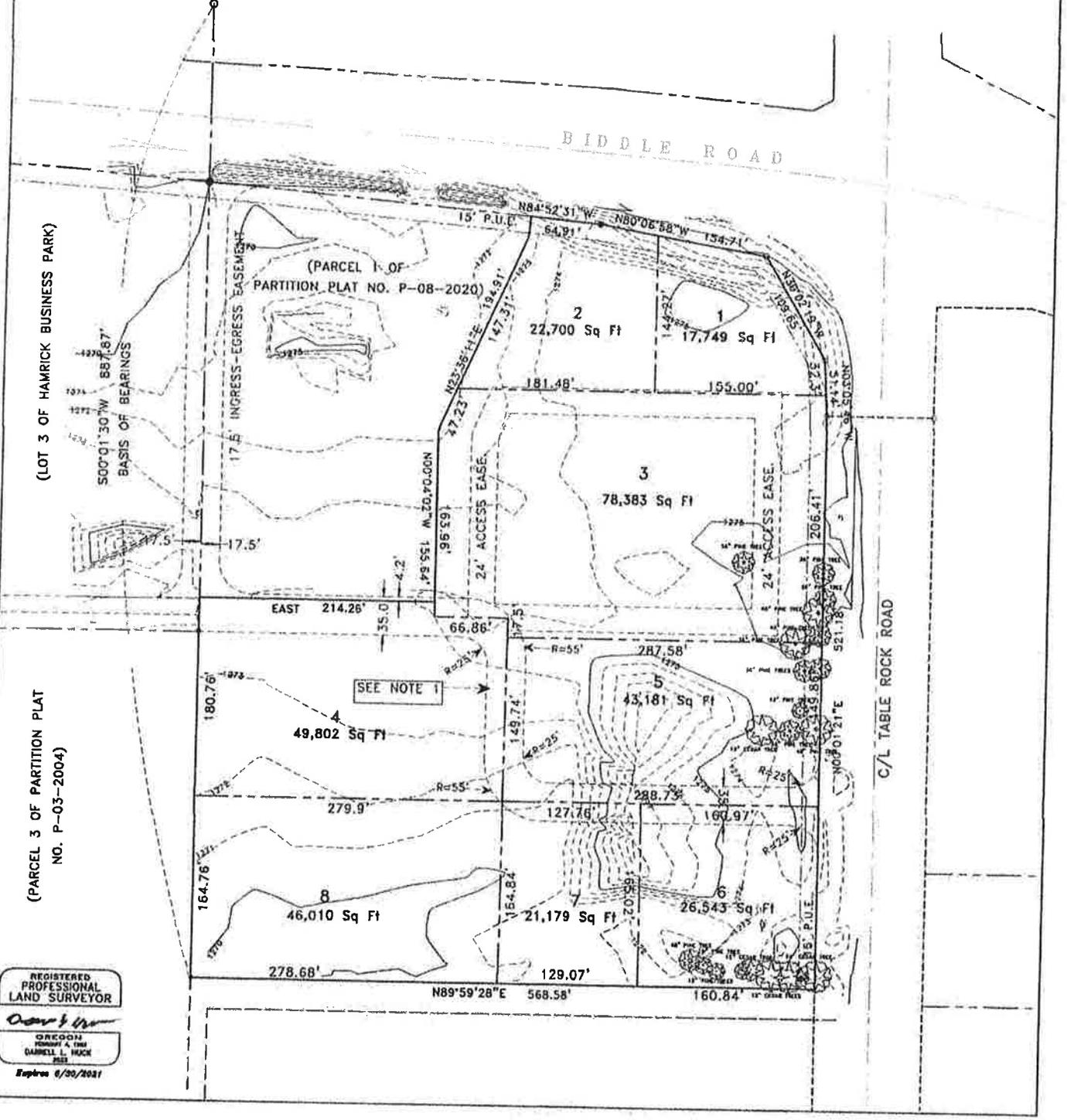
AGENT
ROGUE PLANNING & DEVELOPMENT
33 N. CENTRAL #213
MEDFORD, OREGON 97501
(541) 951-4020

SURVEYOR
HOFFBUHR & ASSOCIATES, INC.
880 GOLF VIEW DRIVE, SUITE 201
MEDFORD, OREGON 97504
(541) 779-4641

CONTOUR INTERVAL = 1'
INDEX CONTOURS = 5'

NOTES:
1. INGRESS-EGRESS EASEMENT PER PARTITION PLAT P-08-2020 IS BEING DEDICATED AS P.U.E.

SCHOOL ZONE 549C
IRRIGATION DISTRICT R.R.V.I.D.
SANITATION DISTRICT R.V.S.S.
AREA 9.07 ACRES
ZONING C4
EXISTING USE - VACANT
PROPOSED USE - COMMERCIAL



REGISTERED PROFESSIONAL LAND SURVEYOR

GABRIEL L. HUCK

Expire 6/30/2021

ZAREMBA GROUP



Site Plan and Architectural Review of new commercial structure



April 21, 2020

**Site Plan and Architectural Review
for the development of the property located at
4245 Table Rock Road (37 2W 01C: TL#700).**

Property Owner:

South Salem LLC
PO BOX 4460
Medford, OR 97501

Applicant:

Zaremba Group
14600 Detroit Avenue
Cleveland, OH 44107

Agent:

Rogue Planning & Development Services, LLC
Amy Gunter
33 N Central Avenue, Suite 213
Medford, OR 97501

Land Surveyor:

Hoffbuhr and Associates
Darrell Huck
880 Golf View Drive, Suite 201
Medford, OR 97504

Civil Engineering:

CEC Engineering
132 W Main Street, Suite 201
Medford, OR 97501

Landscape Design:

Madera Design Inc.
2994 Wells Fargo Way
Central Point, OR 97502

Request:

The request is for Site Plan and Architectural Review to allow for the development of a Firestone Complete Auto Care and Tire facility. Bridgestone/Firestone is a national tire and car service retail facility, they propose to construct a 6,262 square foot commercial structure and associated site improvements.

The area of the property proposed for development is part of a larger commercial subdivision and community shopping center proposal for the 7.01-acre property. Phase 2 includes the private retail street and the development of the Firestone Complete Auto Care facility. Phase 1 is a lot partition and site plan and architectural review, conditional use permit for an oil change and carwash facility that is approved for the property (PAR-19002 and SPAR-19002).

The proposed development is for a 1.03-acre portion of the 7.01-acre commercial subdivision area. The proposed lot area is at the northwest intersection of the proposed private retail street and Table Rock Road. The lot extends 150-foot north along Table Rock Road, and 287.57 feet west of Table Rock Road along the private retail street. The private retail street abuts the west property line as it jogs north, then west to complete the connection through the property.

The Firestone Complete Auto Care and Tire structure is proposed as a steel-framed, metal building. The exterior materials include architectural fiber cement wall panels, anodized aluminum storefront windows and doors. The proposed structure is 50-feet X 130-feet, 6,262 square feet in area. The proposed structure will have a showroom and customer waiting area, and an eight-bay, tire facility.

Additional site improvements consist of the drive isles, parking areas, landscaping and irrigation, screening plantings, and trash /recycling areas.

Architecture:

The proposed building is consistent with the conceptual elevations provided within the Central Point commercial building design standards. The building is oriented towards both Table Rock Road and the private retail street. The pedestrian entry door is clearly oriented towards the private retail street. The entry is recessed with column-like features of the wall panels on either side of the black, anodized aluminum storefront door. There are two large sidelight windows, a transom window, and distinctive entryway lighting. The façade facing Table Rock Road has large storefront style windows with metal, awnings above the windows. The buildings' architecture provides changes in horizontal and vertical massing, changes in color, texture, and materials. The structure has a distinct profile with a parapet topped with a cornice that provides screening for

the rooftop equipment. The cornice has changes in height and a cap is provided on the cornice. This adds interest to the mass of the structure.

Access:

Access to the site is provided from the proposed private retail street. The private retail street will extend from Table Rock Road through the property to the west to connect with the terminus of the retail street on the adjacent property to the west.

As required in the Land Development ordinance, a Traffic Impact Analysis is required by the City of Central Point and Jackson County. Southern Oregon Transportation Engineers performed a traffic impact analysis for the community shopping center. The tire store is included in the community shopping center trip generation details. It can be found that the proposal complies with the standards required by the City of Central Point and Jackson County Roads Department.

Based on the results of the previous Traffic Impact Analysis, Jackson County Roads has indicated the proposed private retail street from Table Rock Road will be right in, right out and left in. This will be evidenced by the Civil improvement plans for the subdivision. The Transportation Impact Analysis did not trigger any necessary alterations to the Biddle Road and private retail street intersection at this time. As noted in previous decisions on the property, Jackson County Roads reserves the right to restrict access from Biddle Road to right in and right out only and the property owner and applicant are aware.

To provide internal circulation, a private retail street is proposed to be extended through the property. The north/south portion of the street will be installed with the Phase 1 portion of site improvements. This will connect to the intersection of the retail street along the east property line.

Adequate transportation facilities are provided within the public rights-of-way and via the private retail street. Block perimeter lengths of 2,000 feet is slightly in excess due to the inability to provide new intersections closer to the Biddle Road and Table Rock Road intersection. There are pedestrian and bicycle connections through the parking areas and drive aisles. Vehicular access is restricted to the proposed private retail street locations provided on the site plan.

Parking:

The proposed Site Plan and Architectural details for the Firestone Complete Auto and Tire facility provide adequate drive aisles and parking areas located in a convenient and safe location for the development of the site.

There are 29 parking spaces proposed on-site. This is consistent with the standards of 17.64.040.B.D and F.

All parking areas are proposed to be paved. The parking spaces are all proposed to be at least 9-feet by 19-feet in depth. The parking area is proposed to be buffered with a 10-foot landscape strip that is a dual purpose as a sidewalk buffer and parking area buffer.

The parking areas, stormwater collection facilities, and construction requirements will be designed per the Rogue Valley Sanitary Sewer Services (RVSS) Regional Stormwater Quality Design Manual.

Landscaping:

The landscape plan has been designed following CP 17.75.039.G. Along the east property line adjacent to Table Rock Road, a 15-foot wide landscape buffer is proposed. A ten-foot-wide landscape buffer is proposed along the south and north sides of the site. An oversized landscape buffer is proposed along the west property line.

Conclusion:

The project team finds that the purpose and intent of the C-4 Zoning District is to provide for the development of concentrated tourist commercial and entertainment facilities to serve both residents and traveling public at locations that will maximize ease of access and visibility from the Interstate 5 freeway and major arterial streets and to be convenient to the users of Expo Park.

The project team believes that the proposed structure complies with the city of Central Point Standards for the development of a commercially zoned property at the boundary of the City limits and UGB, separated a substantial distance from the historic downtown and city center.

It can be found the proposed structure will provide an architecturally interesting design template that reflects the architectural character of the City of Central Point and demonstrates compliance with the standards and that the design is consistent with other auto-oriented design in the vicinity that serves the traveling public.

As proposed, Phase 2 development of the site includes the proposed site plans, architectural details, landscape plans and conceptual utility plans that promote the orderly and harmonious development of an underdeveloped commercial area in the city.

The proposed development will not have a negative impact on the stability of land values and investments, and the general welfare, including aesthetic considerations for the adjacent neighboring properties. The proposed building will enhance the neighborhood while providing commercial use and creation of jobs.

Attachments:

Title Report
Firestone Preliminary Site Plan
Firestone Preliminary Site Plan (aerial)
Conceptual Commercial Subdivision
Alta Survey
Transportation Impact Analysis
Conceptual Utility Plan
Conceptual Grading Plan
Landscape and Irrigation Plan

FINDINGS OF FACT

City of Central Point Municipal Code Chapter 17: Zoning Ordinance

C-4 Tourist and Office-Professional District

17.44.010 Purpose.

The C-4 district is intended to provide for the development of concentrated tourist commercial and entertainment facilities to serve both residents and the traveling public, and also for the development of compatible professional office facilities. C-4 development should occur at locations that will maximize ease of access and visibility from the Interstate 5 freeway and major arterial streets and to be convenient to the users of Expo Park, the airport, and downtown.

Finding:

The proposed development is for Bridgestone/Firestone Complete Auto Care facility. The structure and site improvements will complement the approved site review, conditional use permit and lot partition of the Premier Oil Change and Car Wash facility that is approved for the property (PAR-19002 and SPAR-19002). These uses serve the residents and the traveling public.

There are three points of private retail street ingress and egress to the Commercial Shopping Center that provide access to the proposed tire store. Two of those driveways are from major arterial streets, Biddle Road and Table Rock Road.

The tire store is proposed to be located near Table Rock Road, northwest of the private retail street intersection and Table Rock Road. Driveway access is provided to the proposed parking area from the retail street.

17.44.020 Permitted Uses.

B. Tourist and entertainment-related facilities, including but not limited to:

3. Automobile service station, automobile, and recreational vehicle parts sales and repairs, and truck rentals;

Finding:

The proposal is for an automobile service facility. The site is proposed to be developed with a Firestone Complete Auto Care facility in the form of a retail tire sales and installation, car care facility. The CPMC allows this use as an outright permitted use.

17.44.040 Site plan and architectural development standards.

Development within the C-4 District shall be subject to the site and architectural standards outlined in Chapter 17.75, Design and Development Standards.

Finding:

The proposed development can be found to comply with the site and architectural standards outlined in CPMC 17.75. See additional findings.

17.44.050 General use requirements.

A. Uses that are normally permitted in the C-4 district but that is referred to the planning commission for further review, per Section 17.44.030(A)(19), Conditional uses, will be processed according to application procedures for conditional use permits. No use shall be permitted and no process, equipment or materials shall be used which are found by the planning commission to be harmful to persons living or working in the vicinity by reason of odor, fumes, dust, smoke, cinders, dirt, refuse, water-carried waste, noise, vibration, illumination or glare, or are found to involve any hazard of fire or explosion.

Finding:

The proposal is for a commercial structure. The intended use is a retail tire sales and installation facility, a permitted use in the zone. The use, process, equipment, and materials used in conjunction with the site development are not harmful to persons working in the vicinity of the proposed development. The proposed development is setback more than 200-feet from the nearest City of Central Point residentially zoned properties.

The proposed use is a traveler and resident oriented business that complies with the purpose and intent of the Commercial (C-4) zone to meet the needs of the tourist-oriented business. There is a public, Jackson County RV park not far from the property, highway travelers that attend the tourist-oriented venues of the Family Fun Center and the Jackson County Expo. The proposed tire facility is part of the larger commercial shopping center.

The tire facility will not cause more noise, dust, odor glare, vibration, illumination or glare beyond what is reasonably accepted in a Commercial zone along two major arterial streets.

B. All businesses, services and processes shall be conducted entirely within a completely enclosed structure, with the exception of off-street parking and loading areas, outdoor eating areas, service stations, outdoor recreational facilities, recreational vehicle overnight facilities, and other compatible activities, as approved by the planning commission.

Finding:

The primary functions of the site are automobile-oriented. These functions will occur within an enclosed structure. The building is proposed to include an eight-bay garage structure on the south side of the building. A retail showroom and customer waiting area is proposed on the east side of the building. Parking and loading areas are proposed on the east and south sides of the proposed structure. The loading area and refuse area are to the west of the structure in a screened enclosure.

C. Open storage of materials related to a permitted use shall be permitted only within an area surrounded or screened by a solid wall or fence having a height of six feet; provided that no materials or equipment shall be stored to a height greater than that of the wall.

Finding:

No materials are proposed to be stored outside of the structure.

17.44.060 Signage standards.

Signs in the C-4 district shall be permitted and designed according to provisions of Section 17.75.050, Signage standards, and Chapter 15.24, Sign Code.

Finding:

The signs for Firestone Complete Auto Care will comply with the Sign Code of CPMC section 15.24 and Section 17.75.050.

A separate sign permit application demonstrating compliance will be obtained at the time of the construction and permitting phase.

17.44.070 Off-street parking.

Off-street parking and loading spaces shall be provided as required in Chapter 17.64, Off-Street Parking and Loading, and developed to the standards set forth in Section 17.75.039, Off-street parking design and development standards.

Finding:

The proposed off-street parking and loading spaces are proposed following CPMC 17.64, Off-Street Parking and Loading standards.

Parking is generally shown to be met throughout the entirety of the subdivision for the potential uses envisioned in the Community Shopping Center conceptual layout. Each individual Site Plan and Architectural Review will demonstrate compliance with the parking standards for the proposed use.

As for the proposed Site Plan and Architectural Review of Lot #5, the proposed Site Plan and Architectural Review for Firestone, the site plan for the development of the site provides for 29 parking spaces.

The number of parking spaces proposed is based on the Central Point Chapter 17.64.02.B. The use category for tire sales, service and repair is most similar to the automotive, boat, RV sales and repair. One (1) space for each employee on the major shift; plus 2 spaces for each service bay; plus 1 space per each 300 square feet of showroom area; plus 1 space per each 2,000 square feet of used or new vehicle sales area, or other outdoor sales area is the required number of spaces.

One parking space for each employee during the most major shift is ten (10) employees.. There are eight (8) service bays, the eight service bays require 16 parking spaces. There is approximately 760 square feet of show room area, including the waiting area divided by a partition wall, this requires 2.53 parking spaces. A total of 28.53 parking spaces are required and 29 parking spaces are proposed.

All parking areas are proposed to be paved. The parking spaces are all proposed to be at least 9-foot by 19-foot in depth. The parking area is proposed to be buffered with a 10-foot landscape strip that is a dual purpose as a sidewalk buffer and parking area buffer.

The parking areas, stormwater collection facilities, and construction requirements will be designed per the Rogue Valley Sanitary Sewer Services (RVSS) Regional Stormwater Quality Design Manual.

Accessible parking spaces are proposed per the standards for proximity to entry, slope, striping, and access.

Design and Development Standards

17.75.031 General connectivity, circulation and access standards.

The purpose of this section is to assure that the connectivity and transportation policies of the city's Transportation System Plan are implemented. In achieving the objective of maintaining and enhancing the city's small town environment it is the city's goal to base its development pattern on a general circulation grid using a walkable block system. Blocks may be comprised of public/private street right-of-way, or accessways.

Finding:

The street frontage of the property was recently improved along Table Rock Road. Throughout the development, there is a primary east to the west connection provided via the private retail street. The private retail street will have street-like improvements installed along the frontage of the subject property. The retail street is proposed to have a five-foot curbside sidewalk and a ten-foot landscape buffer to the parking areas. Along the west property line of the project site, a five-foot curbside sidewalk and a large landscape buffer are proposed between the sidewalk and the loading zone.

A. Streets and Utilities. The public street and utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction shall apply to all development within the city.

Finding:

Table Rock Road along the frontage of the property has been improved with curb, gutter, sidewalk, utility installations, etc.

The private retail street is proposed to be connected to the existing private retail street that exists as an east/west connection from Hamrick Road (where parallel to Table Rock Road) as part of a pending commercial shopping center proposal for the 7.01-acre parcel. This private retail street will intersect with a north/south, private retail street connection (approved access for Premier Oil).

Landscape medians and buffering and an interconnected five-foot-wide sidewalk system are provided through the development to provide pedestrian connectivity to the public street system. (Note: the conceptual subdivision plan demonstrates a six-foot-wide sidewalk through the development, this is in the event of anticipated code changes. A five-foot sidewalk is presently proposed per the Firestone site plan.

The proposed development will comply with all utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction.

B. Block Standards. The following block standards apply to all development:

1. Block perimeters shall not exceed two thousand feet measured along the public street right-of-way, or outside edges of accessways, or other acknowledged block boundary as described in subsection (B)(4) of this section.

Finding:

The proposed private retail street connections through the community shopping center slightly exceed the block perimeters with 2,200-feet. This is due to the shifting of the private retail street intersection south of the Biddle Road and Table Rock Road intersections. The jog in the retail street adds additional distance.

2. Block lengths shall not exceed six hundred feet between through streets or pedestrian accessways, measured along street right-of-way, or the pedestrian accessway. Block dimensions are measured from right-of-way to right-of-way along street frontages.

Finding:

The proposed block lengths through the development exceed 600-feet.

The intersection of the retail street that traverses Parcel 2, will have a more than 600-foot block length to Table Rock and Biddle Road. This is due to the presence of the major arterial streets, and intersection alignment issues, separation from the intersection to the street, and sight-lines on Table Rock Road and Biddle Road.

3. Accessways or private/retail streets may be used to meet the block length or perimeter standards of this section, provided they are designed in accordance with this section and are open to the public at all times.

Finding:

A private retail street system is used to attempt to meet the block length and perimeter standards. The retail street has been designed per the requirements of this code.

4. The standards for block perimeters and lengths may be modified to the minimum extent necessary based on written findings that compliance with the standards are not reasonably practicable or appropriate due to:

Finding:

The proposed block lengths through the development exceed the 600-foot block length rule. The intersection of the private retail street that traverses Parcel 2, the 7.01-acre area of the commercial subdivision and community shopping center, will have a more than 600-foot block length to Table Rock and Biddle Road. Additionally, the block perimeter exceeds 2,000 feet. This is due to the separation standards from the intersection to the street and the sight-lines on Table Rock Road. The private retail street and connected parking lot system allow for pedestrians and bicycles to cross the site without following the block boundaries, only automobiles will be restricted to the private retail street and arterial street intersections.

C. Driveway and Property Access Standards. Vehicular access to properties shall be located and constructed in accordance with the standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction, Section 320.10.30, Driveway and Property Access.

Finding:

The proposed civil improvements throughout the development will comply with all utility standards set forth in the City of Central Point Department of Public Works Standard Specifications and Uniform Standard Details for Public Works Construction. The Traffic Impact Analysis will be reviewed by Jackson County Roads. The driveway access reservation will be made with Jackson County Roads upon approval of the development.

D. Pedestrian Circulation. Attractive access routes for pedestrian travel shall be provided through the public sidewalk system, and where necessary supplemented through the use of pedestrian accessways as required to accomplish the following:

Finding:

There will be a complete sidewalk system along both public street frontages of the property. Through the development, there are private retail streets and an interconnected parking lot

system proposed. These streets are proposed to have landscape buffers and sidewalk systems with sidewalk and parking lot crossings that are distinguished from the driving surface to provide pedestrian connectivity through the property.

Pedestrian scale streetlights and directional signage will provide interest and safety for pedestrians.

E. Accessways, Pedestrian. Pedestrian accessways may be used to meet the block requirements of subsection B of this section. When used pedestrian accessways shall be developed as illustrated in Figure 17.75.01. All landscaped areas next to pedestrian accessways shall be maintained, or plant materials chosen, to maintain a clear sight zone between three and eight feet from the ground level.

Finding:

The pedestrian accessways provided through the development in the parking areas and along the Private Retail Street will reduce the block length as a pedestrian can bisect the development vs. an automobile which must stay on the driving surfaces.

All landscape areas will be professionally designed, installed and maintained. The plant materials sections provide for clear sight zones and to provide safety and security throughout the sight. Vision clearance triangles will be maintained at the intersections.

F. Retail Street. Retail streets may be used to meet the block requirements of subsection B of this section. When used retail streets shall be developed as illustrated in Figure 17.75.02.

Finding:

The private retail street is proposed to attempt to meet the block requirements of subsection B. The retail street is proposed to connect to existing retail streets developed on the adjacent properties. Utility, access and maintenance easements for the private retail street that is proposed through the community shopping center will be provided with the survey plat map.

17.75.039 Off-street parking design and development standards.

All off-street vehicular parking spaces shall be improved to the following standards:

A. Connectivity. Parking lots for new development shall be designed to provide vehicular and pedestrian connections to adjacent sites unless as a result of any of the following such connections are not possible:

Finding:

The parking lots throughout all phases of the development will be designed in a manner that provides vehicular and pedestrian connections to the adjacent properties and public right-of-way.

B. Parking Stall Minimum Dimensions. Standard parking spaces shall conform to the following standards and the dimensions in Figure 17.75.03 and Table 17.75.02; provided, that compact parking spaces permitted in accordance with Section 17.64.040(G) shall have the following minimum dimensions:

Finding:

The proposed parking space width, length, access, drive isles, and accessibility standards are met with the proposal.

C. Access. There shall be adequate provision for ingress and egress to all parking spaces.

Finding:

The driveways, driving aisles and access thorough the development provide adequate provisions for ingress and egress to all parking spaces. The driveways and drive isles are all 24-feet or greater.

D. Driveways. Driveway width shall be measured at the driveway's narrowest point, including the curb cut. The design and construction of driveways shall be as set forth in the Standard Specifications and Public Works Department Standards and Specifications.

Finding:

The driveway and access point design and construction will comply with the standards and specifications of the public works department. The driveway intersection with the private retail street is more than 100-feet to the west, which complies with the standards. The driveway widths provide adequate dimensions to meet turning movement and access standards.

E. Improvement of Parking Spaces.

Finding:

The proposed parking lots are proposed to be designed and installed to the standards of the city of Central Point.

All paving and parking space delineation, including curbing and directional arrows painted on the drive aisles to facilitate on-site traffic, is proposed.

The parking area will be paved and striped in accordance with the standards of the city of Central Point.

Per the Conceptual Grading and Drainage Plan (Exhibit C1), there is adequate capacity through the proposed measures of below-grade detention and then release into the system within the private retail street system. The final construction details of the Firestone Complete Auto Care and Tire facility site development and structure will address stormwater needs as required by the RVSS Standards in effect at the time and in general accordance with Exhibit C1, the Conceptual Grading, and Drainage Plan.

No parking spaces are designed with backing movements or other maneuvering within a street or other public right-of-way.

All lighting used to illuminate the off-street parking and loading areas will be arranged to direct the light away from the streets and adjacent properties.

All drives and streets will have a minimum vision clearance area met with the landscape plantings and signage. No vision clearance problems will be created by the proposed drive isles.

Curbing is proposed for all parking spaces and drive aisles at the outer boundaries of the parking lot to prevent motor vehicles from extending over property lines, public streets, and landscape areas.

Parking, loading and vehicle maneuvering areas are not located within any portion of the street setback area that is required to be landscaped in the commercial district.

All vehicle parking areas provide adequate vehicle turnaround and maneuvering areas through the use of drive-aisle and turnaround spaces and with an interconnected driveway system. The proposed layout appears to be consistent with the figures from 17.75.04 and 17.75.05.

F. Limitation on Use of Parking Areas. Required parking areas shall be used exclusively for vehicle parking in conjunction with a permitted use and shall not be reduced or encroached upon in any manner. The parking facilities shall be so designed and maintained as not to constitute a nuisance at any time, and shall be used in such a manner that no hazard to persons or property, or unreasonable impediment to traffic, will result.

Finding:

The parking areas will be used exclusively for vehicle parking associated with permitted uses on the property.

G. Parking/Loading Facility Landscaping and Screening. Parking lot landscaping shall be used to reinforce pedestrian and vehicular circulation, including parking lot entries, pedestrian accessways, and parking aisles. To achieve this objective the following minimum standards shall apply; however, additional landscaping may be recommended during the site plan and architectural

review process (Chapter [17.72](#)). All parking lots shall be landscaped in accordance with the following standards:

Finding:

The parking lot landscaping is professionally designed. The landscaping is meant to enhance the pedestrian environment, improve the screening of vehicles from the adjacent properties and the public right-of-way. The proposed landscaping site plan achieves the minimum standards from the table found in 17.75.03 with a 15-foot buffer along Table Rock Road and a 10-foot buffer adjacent to the north property line where future site development will likely locate driveway access to the adjacent lot to the north.

H. Bicycle Parking. The amount of bicycle parking shall be provided in accordance with Section [17.64.040](#) and constructed in accordance with the following standards:

Finding:

A bicycle parking structure that allows for locking of the frame will be provided with the building permit submittals.

17.75.042 Commercial building design standards.

The following design standards are applicable to development in all commercial zoning districts, and are intended to assure pedestrian scale commercial development that supports and enhances the small town character of the community. All publicly visible buildings shall comply with the standards set forth in this section.

Finding:

The proposed structure is in the commercial zoning district, at the boundary of the city limits and urban growth boundary. The proposed development of the site with a Firestone Complete Auto Care facility is proposed as a vehicle-oriented use. The scale of the structure, the setbacks, landscape buffers and installation of sidewalks provide for a pedestrian scale development in an auto-centric area of town.

A. Massing, Articulation, Transparency, and Entrances.

1. Building Massing. The top of the building shall emphasize a distinct profile or outline with elements such as a projecting parapet, cornice, upper level setback, or pitched roofline.

Finding:

The proposed building has modern elements. The parapet and cornice emphasize a distinct profile and provides architectural symmetry.

2. Facade Articulation. Facades longer than forty feet and fronts on a street, sidewalk, accessway or residential area shall be divided into small units through the use of articulation, which may include offsets, recesses, staggered walls, stepped walls, pitched or stepped rooflines, overhangs, or other elements of the building's mass.

For purposes of complying with the requirements in this subsection "facade articulation" shall consist of a combination of two of the following design features:

- a. Changes in plane with a depth of at least twenty-four inches, either horizontally or vertically, at intervals of not less than twenty feet and not more than forty feet; or
- b. Changes of color, texture, or material, either horizontally or vertically, at intervals of not less than twenty feet and not more than one hundred feet; or
- c. A repeating pattern of wall recesses and projections, such as bays, offsets, reveals or projecting ribs, that has a relief of at least eight inches.

Finding:

The structure is proposed use Nichiha, Engineered Architectural Panels on the exterior façade. The TuffBlock panels are textured, cement board siding with a rugged finish akin to concrete block. There are two colors proposed, Steel (dark gray) and Pewter (light gray). The structure is proposed to have a parapet above that is topped with a cornice and coping.

The Table Rock Road façade (East) is 50-foot, 11-inches wide. This façade is divided into three 'masses' through the has substantial articulation in stepped rooflines with a detailed cornice atop the parapet. The width of the structures mass has a horizontal plane, the cornice steps up 24". The middle mass is 32' 2 ¾" wide, stepping down 24" to the third horizontal plane of 9' 4 ¼". The cornice is topped with a charcoal gray coping. The 24-inch change is provided horizontally along the top of the parapet providing visual changes in the horizontal roof plane. This is consistent with standard 17.75.042.A.2.a.

The Architectural panels are proposed to be oriented horizontally. A 4' – 9" course of the darker gray "Steel" colored panel is proposed as the 'base'. Above that there is 8.9' of the lighter gray panel "Pewter", a course of Steel is proposed another approximately 6' of the Pewter panel is provided. This complies with 17.75.042.A.2.b and a vertical change in color at intervals of not less than twenty feet and not more than one hundred feet is provided.

Three large windows, two more narrow and a third wider window are proposed within the three distinct masses, there are metal awnings are proposed above the windows.

The south façade, facing the public street, has substantial articulation in the façade with a prominent business entrance. The entry area proposed to have a taller wall to highlight

the business entrance. There are column type features created on either side of the recessed entrance door with windows to the side and above the entry door. The structure is proposed to have the Steel color to create a base of 4' – 9" in height, the Pewter color, the Steel band and the light Pewter. The building entrance segment is the same vertical, material treatment same as the Table Rock Road façade.

The horizontal plane of south side facade section of the structure is divided into five distinct rooflines with 24' steps in the cornice, providing the divisions in the horizontal plan consistent with 17.75.042.A.2.a.

This façade along the private street has eight bays for the tire and vehicle service. The garage bays have overhead doors that include a solid panel at the base similar to the gray base panels. The remainder of the overhead doors is clear glazing. The majority of this façade plane is proposed as the dark Steel color. The west building massing section provides adequate changes of color, texture, or material, either horizontally and vertically along the facade.

The rear, west side of the structures faces a portion of the private retail street, this portion of the structure has a service entry door. Mimicking the east façade, an articulated roofline is proposed. There is a screening wall for the loading zone and the trash recycle enclosure area and a wide landscape buffer between the private retail street and the structure.

The north wall of the structure abuts a property line with the proposed future commercial development. The development of the lot to the north is conceptual and may include a driveway in the future. The proposed façade of the Firestone structure provides adequate articulation with a stepped parapet with a 24" variation in the horizontal plan. The horizontal plan is divided into smaller masses through the stepping of the parapet consistent with 17.75.042.A.2.a. The vertical plane of the wall is divided into texturized metal panels including a 4' – 9" dark, Steel color base panel. The Architectural panels are proposed to be oriented horizontally. A 4' – 9" course of the darker gray "Steel" colored panel is proposed as the 'base'. Above that there is 8.9' of the lighter gray panel "Pewter", a course of Steel is proposed another approximately 6' of the Pewter panel is provided. This complies with 17.75.042.A.2.b and a vertical change in colors. The roof drains are attached to exterior scuppers that add additional dimension along the facade.

3. Pedestrian Entrances. For buildings facing a street, a primary pedestrian entrance shall be provided that is easily visible, or easily accessible, from the street right-of-way, or a pedestrian accessway. To ensure that building entrances are clearly visible and identifiable to pedestrians the principal entry to the building shall be made prominent with canopies or overhangs.

To achieve the objectives of this subsection the design of a primary entrance should incorporate at least three of the following design criteria:

- a. For building facades over two hundred feet in length facing a street or accessway provide two or more public building entrances off the street;
- b. Architectural details such as arches, friezes, tile work, murals, or moldings;
- c. Integral planters or wing walls that incorporate landscape or seating;
- d. Enhanced exterior light fixtures such as wall sconces, light coves with concealed light sources, ground-mounted accent lights, or decorative pedestal lights;
- e. Prominent three-dimensional features, such as belfries, chimneys, clock towers, domes, spires, steeples, towers, or turrets; and
- f. A repeating pattern of pilasters projecting from the facade wall by a minimum of eight inches or architectural or decorative columns.

Finding:

The proposed building's primary frontage is oriented towards the private retail street. A clear sense of entry is proposed with a black, anodized aluminum storefront door and large windows. The clearly visible entry is recessed within the prominent, column-like features created by the architectural panels and projecting black metal awning.

Enhanced exterior light fixtures are provided on both sides of the entry door.

There are planter areas proposed on either side of the entry and along the Table Rock Road side of the structure.

4. Transparency. Transparency (glazing) provides interest for the pedestrian, connects the building exterior and interior, puts eyes on the street/parking, promotes reusability, and provides a human-scale element on building facades. The transparency standard applies to a building's principal facade. Projects subject to this section shall meet the following glazing requirements:

Finding:

There is a substantial amount of glazing provided on the principal facades of the structure. The principal facades has large, black, anodized aluminum windows are facing Table Rock Road frontage. Along the south side of the structure facing the private retail street, a clearly delineated pedestrian entrance is provided that has black aluminum, commercial-style storefront doors. There are large windows including a transom window over the entrance door and windows, adding emphasis to the pedestrian entrance. There are clear glazed windows in each roll-up garage door providing additional transparency.

5. Wall Faces. As used in this section there are three types of wall faces. To ensure that buildings do not display unembellished walls visible from a public street or residential area the following standards are imposed:

Finding:

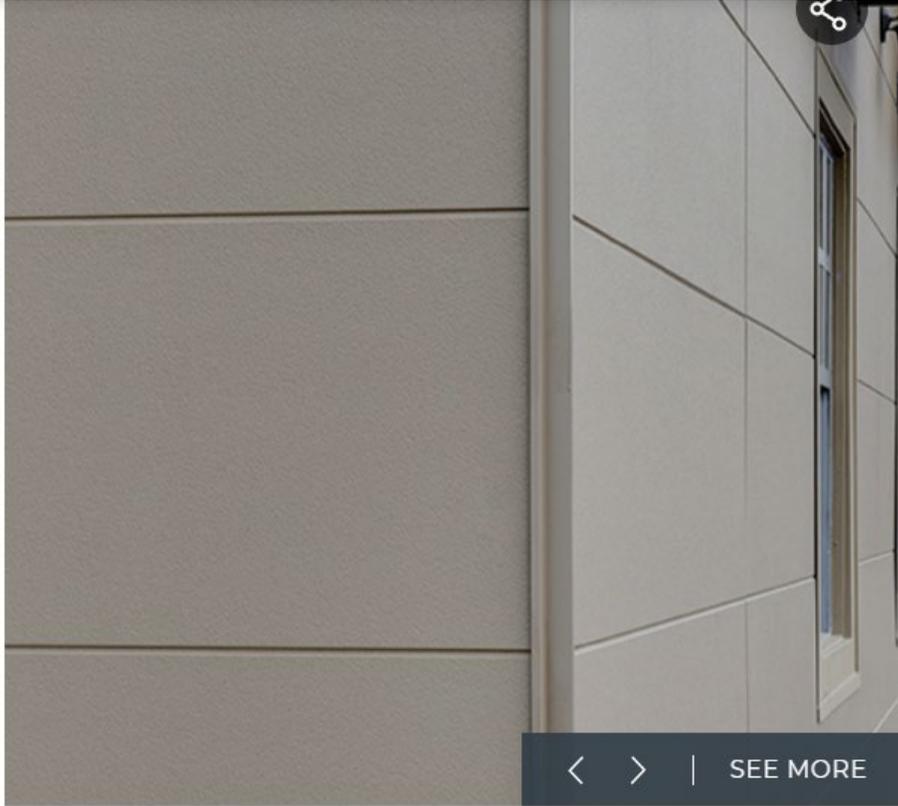
All wall faces visible from a public street are embellished with exterior materials that provide for changes in color, heights, large windows and an enhanced public business entrance.

The property is more than 200-feet from the residential properties to the northwest, across Biddle Road. This distance, future site development and the driving aisles all provide a substantial distance that reduces, if not eliminates the visibility of the back wall (north wall) of the Firestone structure.

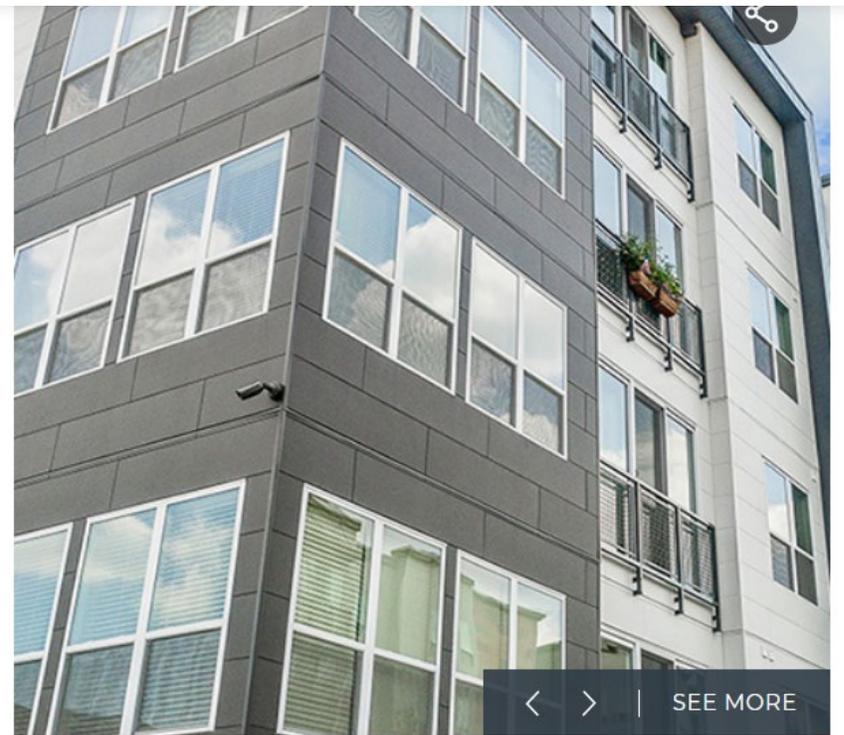
6. Screening of Service Areas and Rooftop Equipment. Publicly visible service areas, loading zones, waste disposal, storage areas, and rooftop equipment (mechanical and communications) shall be fully screened from the ground level of nearby streets and residential areas within two hundred feet; the following standards apply:

Finding:

The loading zone, waste disposal areas will be fully screened with a masonry and cedar fence panel screening. The rooftop mechanical equipment is screened by the building parapet.



TuffBlock – Architectural Fiber Cement – Pewter



TuffBlock – Architectural Fiber Cement - Steel



PUBLIC WORKS STAFF REPORT

May 5, 2020

AGENDA ITEM:

SPAR-20002 – Bridgestone/Firestone Tire Center

Traffic:

The applicant is proposing a 6,242 sq. ft. tire center. The City uses the Institute of Transportation Engineers (ITE) Volume 10 for scoping Traffic Impact Analysis (TIA). A TIA was completed by Southern Oregon Transportation for this use, within the TIA for the commercial subdivision. The restrictions include no left out movement from either access on Table Rock or Biddle Roads. There is no corresponding effect on any City related intersections.

Existing Infrastructure:

Water: There is 8 inch waterline in the private drive to the west of the development
 Streets: Biddle Road is a major arterial street owned and maintained by Jackson County. The private drive to the west is a private retail street..
 Storm Water: There is 24” Storm Drain line in the private drive that has capacity. Jackson County also has a line on Biddle that may be accessible.

Issues:

There is only one major issue:

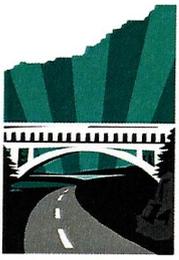
1. **Subdivision Improvements** - The approved commercial subdivision with its associated public and private improvements are yet to be completed. The plans have not been submitted or approved for construction at the date of this staff report. All improvements will need to be completed prior to the building approval.

Conditions of Approval:

1. PW Standards and Specifications – Applicant shall comply with the public works standards and specifications for construction within the right of way.
2. Commercial Subdivision Improvements - The proposed building cannot be issued a final approval until the private retail street, utilities, and storm water requirements associated

with the commercial subdivision are complete.

3. Storm Water Quality - The project is within the Phase 2 stormwater quality area and will require a stormwater management plan that is in accordance with the Rogue Valley Stormwater Quality Design Manual (RVSQDM). An operations and maintenance agreement for all new stormwater quality features is required. Construction on site must be sequenced so that the permanent stormwater quality features are installed and operational when stormwater runoff enters.
4. Erosion Control. – The proposed development will disturb more than one acre and will require an erosion and sediment control permit (NPDES 1200-C) from the Department of Environmental Quality (DEQ).



JACKSON COUNTY

Roads

Roads Engineering

Chuck DeJanvier
Construction Engineer

200 Antelope Road
White City, OR 97503
Phone: (541) 774-6255
Fax: (541) 774-6295
dejanvca@jacksoncounty.org

www.jacksoncounty.org

May 11, 2020

Attention: Justin Gindlesperger
City of Central Point Planning
140 south Third Street
Central Point, OR 97502

RE: Site Plan and Architectural Review of a lot on
Table Rock Road at Biddle Road– county-maintained roads.
Planning File: SPAR-20-002; 37-2W-01C Tax Lot 700

Dear Justin:

Thank you for the opportunity to comment on a Site Plan & Architectural Review for the development of a 6,262 square foot auto care and service facility. The project site is approximately one (1) acre lot in size off of Table Rock Road within the C-4 Tourist and Office Professional zoning district at 4245 Table Rock Road (37-2W-01C Tax Lot 700). Jackson County Roads offers the following comments:

1. Prior to opening of this phase, Jackson County requests construction of a right-in, right-out, left-in (RIROLI) lane on Table Rock Road. This work will require a Minor Road Improvement Permit from Jackson County.
2. The applicant shall submit construction drawings including the median designs for review, comment and approval prior to the project pre-construction meeting
3. Jackson County reserves the right to restrict the left-in access at any time if it becomes a safety concern in the judgement of Jackson County. Further it is likely this restriction will be required at one or both locations within 20 years.
4. If frontage improvements are required, the applicant shall obtain a minor road improvement permit from Jackson County Roads Sidewalk placement on Table Rock Road frontage if required should be directly behind the curb. Jackson County Roads does not maintain planter strips.
5. Roads requires the removal of any existing driveways not being used on either Table Rock Road and replacing them with new curb, gutter and sidewalk. If curb, gutter and sidewalk is not existing, it will be required.
6. Utility Permits are required from Roads for any utility work within the county road right-of-way. On longitudinal trenches within a travel lane 100' or greater in length, unless otherwise approved by the Engineer, the existing pavement shall be removed and replaced to full paving-machine width (normally 10'-12') for a travel lane restoration. Drag boxes or other pull-type asphalt spreaders will not be permitted for longitudinal trench pavement replacement.

7. Per Oregon Revised Statute 209.150, any survey monuments of record removed, disturbed or destroyed within the permit area must be referenced prior to construction and replaced after construction by a registered professional land surveyor. All costs associated with this surveying work are the responsibility of the permit holder
8. Biddle Road is a County Minor Arterial and is maintained by the County. The Average Daily Traffic count was 9,984 for eastbound lanes 450 east of Hamrick Road on July 31, 2018 and 9,677 for westbound lanes on July 31, 2018 450 feet east of Hamrick Road.
9. Table Rock Road is a County Arterial and is maintained by the County. The Average Daily Traffic count was 20,723 750 north of Biddle Road on July 9, 2018.
10. If county storm drain facilities are to be utilized, the applicant's registered Engineer shall provide a hydraulic report and plans for review and approval by Jackson County Roads. Storm drainage runoff is limited to that area currently draining to the County storm drainage system. Upon completion of the project the developer's Engineer shall certify that the construction of the drainage system was constructed per the approved plan. A copy of the certification shall be sent to Chuck DeJanvier at Jackson County Roads.
11. We would like to be notified of future development proposals, as county permits may be required.
12. We concur with any right-of-way dedicated.

Sincerely,



Chuck DeJanvier, PE
Construction Engineer



ROGUE VALLEY SEWER SERVICES

Location: 138 West Vilas Road, Central Point, OR - Mailing Address: P.O. Box 3130, Central Point, OR 7502-0005
Tel. (541) 664-6300, Fax (541) 664-7171 www.RVSS.us

May 11, 2020

Justin Gindlesperger
City of Central Point Planning Department
155 South Second Street
Central Point, Oregon 97502

Re: SPAR-20002, Firestone, Tax Lot 700, Map 37 2W 01C
Ref: SUB-20001

The existing property currently does not have sewer service. There is an existing 8 inch sewer main and manhole just west of the subject property. Sewer service for the proposed development will require a sewer main extension into the property from the existing manhole. Sewer connection permits will be issued upon acceptance of the sewer main and payment of related fees.

Rogue Valley Sewer Services requests that approval of the application be subject to the following conditions:

1. The applicant must provide sewer main construction plans prepared per RVSS standards for review and approval.
2. The sewer main must be accepted by RVSS prior to the issuance of any building permits.
3. The applicant must submit architectural drawings to RVSS for the calculation of system development charges.
4. The applicant must obtain a sewer connection permit from RVSS and pay related system development charges prior to construction.

Feel free to call me with any questions.

Sincerely,

A handwritten signature in black ink, appearing to read "Nick Bakke".

Nicholas R. Bakke, PE
District Engineer

K:\DATA\AGENCIES\CENTPT\PLANNING\SITEPLANREVIEW\2020\SPAR
20002_FIRESTONE.DOC

City of Central Point, Oregon
140 S Third Street, Central Point, OR 97502
541.664.3321 Fax 541.664.6384
www.centralpointoregon.gov



Building Department
Derek Zwagerman, P.E., Building Official

May 15, 2020

Justin Gindlesperger, AICP, CFM
Community Planner II
City of Central Point

RE: SPAR-200002, 4245 Table Rock Road, Bridgestone Firestone

The facility may be required to install fire sprinklers per the below code section. Provide documentation showing tire storage at permit submittal.

OSSC 903.2.9.2 – Bulk Storage of Tires. Buildings and structures where the area for the storage of tires exceeds 20,000 cubic feet shall be equipped throughout with an *automatic sprinkler system* in accordance with Section 903.3.1.1.

Accessible parking spaces shall be constructed in accordance with the Oregon Transportation Commission standards dated August 2018.

There are no additional comments at this time.

A handwritten signature in black ink, appearing to read "Derek Zwagerman".

Derek Zwagerman, P.E.
Building Official

AVIGATION, NOISE AND HAZARD EASEMENT

WHEREAS, _____, hereinafter called "Grantor(s)," are the owners in fee of that certain parcel of land situated in the County of Jackson, State of Oregon, more particularly described in the instrument recorded as _____ (Volume, Page or Official Record number) of the deed records of Jackson County hereinafter called "Grantor(s) Property," Map and Tax Lot Number: _____ and outlined on the attached map, "Exhibit 1"; and

WHEREAS, Grantors' Property is located in an Airport Concern (AC) Overlay or an Airport Approach (AA) Overlay of Rogue Valley International-Medford Airport ("Airport"); and

WHEREAS, Jackson County, a home-rule political subdivision of the State of Oregon, hereinafter called "Grantee," is the owner of the Airport.

NOW, THEREFORE, for valuable consideration, the receipt and sufficiency of which is hereby acknowledged, Grantors, for themselves, their heirs, administrators, executors, successors and assigns, do hereby grant, bargain, sell, and convey unto Grantee, its successors and assigns, for the use and benefit of Grantee, tenants and licensees of Grantee, all users of the Airport, and the public, an easement and right-of-way, appurtenant to the Airport for the unobstructed use and passage of all types of aircraft (as hereinafter defined), in and through all the airspace above the surface of Grantors' Property to an infinite height above Grantors' Property. Notwithstanding the preceding sentence, or any other provision in this easement, Grantors may construct a water reservoir and related structures, including a photovoltaic electric generating system on the reservoir roof, of which may exceed _____ feet in height. This easement is effective regardless of any future changes in the volume of aircraft overflights, or changes in the design of the Airport or in air traffic control procedures.

Said easement shall be appurtenant to and for the benefit of the real property now known as the Airport including any additions thereto wherever located, hereafter made by Grantee or its successors and assigns, guests, and invitees, including any and all persons, firms, or corporations operating aircraft to or from the Airport.

Said easement and burden, together with all things which may be alleged to be incident to or resulting from the use and enjoyment of said easement, including, but not limited to the right to cause in all airspace above or in the vicinity of the surface of Grantors' Property such noise, vibrations, fumes, deposits of dust or other particulate matter, fuel particles (which are incidental to the normal operation of said aircraft), fear, interference with sleep and communication and any and all other effects that may be alleged to be incident to or caused by the operation of aircraft over and in the vicinity of Grantors' Property or in landing at or taking off from, maneuvering around, or operating at or on said Airport.

Grantee, the Airport and any operator, director, or manager is hereby granted; and Grantors do hereby full waive, remise, and release any right or cause of action or suit which they may now have or which they may have in the future against Grantee, its successor and assigns, due to such noise, vibrations, fumes, dust, fuel particles and all other effects, whether at current volumes or levels or increased volumes or levels, that may be caused or may have been caused by the operation of aircraft landing at, or taking off from, maneuvering around, or operating at or on said Airport.

"Aircraft" as used herein, shall mean any and all types of aircraft, whether now in existence or hereafter manufactured and developed, to include, but not limited to, jet aircraft, propeller-driven aircraft, civil aircraft, military aircraft, commercial aircraft, helicopters, blimps, balloons, gliders, ultra-lights, and all types of aircraft or vehicles now in existence or hereafter developed, regardless of existing or future noise levels, for the purpose of transporting persons or property through the air, by whomsoever owned or operated.

"Hazard" as used herein, shall specifically include, but is not limited to, electrical interference with navigational signals or radio communications, other items of interference, impaired visibility, flag poles, antennas, and bird-strike hazards.

The easement and right-of-way is for the purpose of permitting the unobstructed passage of all aircraft, by whomsoever owned or operated, in the airspace above the surface of Grantors' Property, together with the right to cause such noise as may be inherent in the operation of aircraft, and the right to use such airspace for landing on, taking off from or operating at the Airport. Additionally, this easement grants to Grantee the following continuing rights:

1. The Right to prevent the erection, modification, or growth upon Grantors' Property of any building, structure, tree, tethered balloons, kites, or other object(s), extending into the airspace above the surface of said property, in violation of any standard, rule, or

regulation established by Federal Aviation Administration (FAA), Oregon Department of Aviation (ODA), or local ordinances; or in violation of any permit issued by FAA, ODA, or Jackson County.

2. The right of ingress to, egress from, and passage over Grantors' Property to remove or lower from said air space, at the sole option of Grantee, (at Grantors' expense), as an alternative, to mark and light as obstructions to air navigation, any such building, structure, tree or other objects now upon, or which in the future may be upon Grantors' Property in violation of any standard, rule, or regulation established by FAA, ODA, or local ordinances; or in violation of any permit issued by FAA, ODA, or Jackson County.
3. The right to enter upon Grantors' Property and at Grantors' expense, remove, lower, or trim any tree or other vegetation that violates any existing or future standard, rule or regulation of FAA, ODA, or Jackson County.
4. The right to prohibit any use of Grantors' Property that may interfere with the taking off or landing of aircraft at the Airport or otherwise constitutes a hazard to operations at the Airport, and, at Grantors' expense, to enter upon Grantors' Property to abate such hazard.

AND for the consideration hereinabove set forth, Grantors, for themselves, their heirs, administrators, executors, successors, and assigns, do hereby agree that for and during the life of said easement and right of way, they will not hereafter erect, permit the erection or growth of, or permit or suffer to remain upon Grantors' Property any building, structure, tree or other object extending into the airspace above the surface of said property and that they shall not hereafter use or permit or suffer the use of Grantors' Property in such a manner as to: (a) produce glare, (b) create electrical interference with radio communication between any installation upon said Airport and aircraft, (c) make it difficult for flyers to distinguish between airport lights and others, (d) permit any use of Grantors' Property that causes a discharge of fumes, dust or smoke so as to impair visibility in the vicinity of the Airport, (e) create bird strike hazards, or (f) otherwise endanger the landing, taking off or maneuvering of aircraft. Notwithstanding the preceding sentence, or any other provision in this easement, Grantors may construct a water reservoir and related structures, including a photovoltaic electric generating system on the reservoir roof, of which may exceed _____ feet in height. If Grantors' Property is in the Runway Protection Zone, Grantors further agree they will not permit places of public assembly upon Grantors' Property, such as, but not limited to, churches, schools, office buildings, shopping centers, restaurants, child care facilities, and stadiums. Grantors

STATE OF OREGON)
) ss.
County of Jackson)

ATTESTED BEFORE ME this _____ day of _____, 20 _____, by

_____.

Notary Public for State of Oregon
My Commission Expires:
(Stamp)

(Notary's Signature)