



CITY OF CENTRAL POINT

Oregon

City Council Meeting Agenda Thursday, February 17, 2022

Next Res(1697) Ord (2086)

- I. **REGULAR MEETING CALLED TO ORDER**
- II. **PLEDGE OF ALLEGIANCE**
- III. **ROLL CALL**
- IV. **SPECIAL PRESENTATIONS**
- V. **PUBLIC COMMENTS**

Public comment is for non-agenda items. If you are here to make comments on a specific agenda item, you must speak at that time. Please limit your remarks to 3 minutes per individual, 5 minutes per group, with a maximum of 20 minutes per meeting being allotted for public comments. The council may ask questions but may take no action during the public comment section of the meeting, except to direct staff to prepare a report or place an item on a future agenda. Complaints against specific City employees should be resolved through the City's Personnel Complaint procedure. The right to address the Council does not exempt the speaker from any potential liability for defamation.

- VI. **CONSENT AGENDA**

- A. **Approval of January 27, 2022 City Council Minutes**
- B. Proclamation - Arbor Week 2022
- C. Parks and Recreation Commission Appointment

- VII. **ITEMS REMOVED FROM CONSENT AGENDA**

- VIII. **ORDINANCES, AND RESOLUTIONS**

- A. Ordinance No. _____, An Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230 (Holtey)
- B. Resolution No. _____, Accepting the Lowest Responsible Bid from Central Pipeline Inc. for the Gebhard Road Storm Drain Project and Authorizing the City Manager to Execute a Contract (Samitore)
- C. Resolution No. _____, A Resolution Approving an Intergovernmental Agreement with the Rogue Valley Council of Governments for Temporary Use of the Senior Center Building (Jacob)

- IX. **BUSINESS**

- A. Transit Oriented Development (TOD) Fence Code Update Discussion (Holtey)

Mayor
Hank Williams

Ward I
Neil Olsen

Ward II
Kelley Johnson

Ward III
Melody Thueson

Ward IV
Tanea Browning

At Large
Rob Hernandez

At Large
Michael Parsons

B. Resolution No. _____, A Resolution of the City of Central Point Setting a Water Rate Adjustment Effective March 21, 2022 (Samitore)

X. MAYOR'S REPORT

XI. CITY MANAGER'S REPORT

XII. COUNCIL REPORTS

XIII. DEPARTMENT REPORTS

XIV. EXECUTIVE SESSION

The City Council will adjourn to executive session under the provisions of ORS 192.660. Under the provisions of the Oregon Public Meetings Law, the proceedings of an executive session are not for publication or broadcast.

XV. ADJOURNMENT

Individuals needing special accommodations such as sign language, foreign language interpreters or equipment for the hearing impaired must request such services at least 72 hours prior to the City Council meeting. To make your request, please contact the City Recorder at 541-423-1026 (voice), or by e-mail to Deanna.casey@centralpointoregon.gov.

Si necesita traductor en español o servicios de discapacidades (ADA) para asistir a una junta publica de la ciudad por favor llame con 72 horas de anticipación al 541-664-3321 ext. 201

CITY OF CENTRAL POINT

Oregon

City Council Meeting Minutes Thursday, January 27, 2022

I. REGULAR MEETING CALLED TO ORDER

The meeting was called to order at 7:00 PM by Mayor Hank Williams

II. PLEDGE OF ALLEGIANCE

III. ROLL CALL

Attendee Name	Title	Status	Arrived
Hank Williams	Mayor	Present	
Neil Olsen	Ward I	Present	
Kelley Johnson	Ward II	Present	
Melody Thueson	Ward III	Present	
Taneeea Browning	Ward IV	Present	
Rob Hernandez	At Large	Present	
Michael Parsons	At Large	Present	

Staff members present: City Manager Chris Clayton; City Attorney Sydnee Dreyer (Remote); Police Chief Kris Allison (Remote); Police Captain Dave Croft; Police Captain Scott Logue (Remote); Parks and Public Works Director Matt Samitore; Planning Director Stephanie Holtey; Finance Director Steve Weber; IT Director Jason Richmond (Remote); and Accounting Business Services Coordinator Rachel Neuenschwander.

IV. PUBLIC COMMENTS

Mr. Bogenoff spoke to the council regarding someone breaking windows of houses and cars along Freeman Rd, and someone parking a trailer and van in front of his and his neighbors house for an extended period of time.

V. CONSENT AGENDA

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Rob Hernandez, At Large
SECONDER:	Taneeea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

A. Approval of January 13, 2022 City Council Minutes

VI. ITEMS REMOVED FROM CONSENT AGENDA

VII. PUBLIC HEARING

A. Resolution Authorizing Exemption from Competitive Solicitation - RH2 Engineering, Inc.

City Attorney Sydnee Dreyer presented a resolution authorizing exemption from competitive solicitation - RH2 Engineering Inc. Under city code the contracting board is authorized to exempt a contract from competitive bidding, if certain factors in your code are met. In this case there would be substantial cost savings to grant the exemption from competitive bidding.

Exemption is sought for a Design and Engineering contract for the improvements to the Central Point Little League Property. Much of the work toward these improvements was previously completed by RH2, the City's engineer, on the basis of previously authorized contracts for a conceptual plan, detailed cost estimate, project schedule and summary of tasks to be performed to complete the project.

The estimated cost for the design and engineering work is \$275,858.00 (scope and project estimate attached). Exempting this project is likely to result in significant cost savings given that the initial conceptual plan, budget and detailed summary of tasks has already been prepared by RH2, which result in lower costs to finalize the design and engineering of the improvements. Additionally, this project is subject to significant time restraints given that a portion of the funding is subject to the American Rescue Plan Act (ARPA) guidance. Awarding the contract to RH2 will allow the design and engineering to be completed much more quickly as RH2 is already in possession of a portion of the information, analysis and survey it will need to finalize these plans.

Council was concerned on how the contingency is handled the \$25,000, Parks and Public Works Director Matt Samitore stated that they have to make a request to use the contingency. In the past RH2 has been really good at not needing to use the contingency.

Council asked if the project schedule completion date of September 2022 is accurate. Mr. Samitore stated that this is what they were wanting but in this day and time it is not realistic. Summer of 2023 construction, product will be the biggest issue. City Manager Chris Clayton stated the design will get done but as we work on the design if bid is completed in the first half of the summer will contractors work during the summer. They will be meeting with the little league in the next couple of weeks. Everyone is working towards doing this as soon as possible, however the most likely it will be done the following year. The Planning Dept. will be starting the land use component, this is EFU lands with their being an existing facility it should be fine.

Kelley Johnson moved to approve Resolution No. 1696 Adopting Findings Authorizing an Exemption from Competitive Solicitation and Awarding a Contract to RH2 Engineering, Inc. For Design and Engineering of Little League Fields.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Kelley Johnson, Ward II
SECONDER:	Tanea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

VIII. ORDINANCES, AND RESOLUTIONS

A. Ordinance No. _____, An Ordinance amending Central Point Municipal Code Chapter 13.20 in Part Regarding Backflow Prevention Devices

Mr. Samitore presented the second reading of an Ordinance amending the Central Point Municipal code regarding backflow prevention devices. No changes since the first reading.

Council wanted clarification that this matches what Medford Water Commission, Matt stated yes and the City of Ashland adopted something similar this last year, and Eagle Point has already updated there requirements.

Melody Thueson moved to approve Ordinance No 2083 an Ordinance Amending Central Point Municipal Code Chapter 13.20 in Part Regarding Backflow Prevention Devices.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Melody Thueson, Ward III
SECONDER:	Michael Parsons, At Large
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

B. Ordinance No. _____, An Ordinance Amending in Part Central Point Municipal Code Chapters 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers

City Attorney Sydnee Dreyer presented the second reading of ordinance amending in part CPMC 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers requires pawnbrokers and secondhand dealers to maintain a report of all property received on deposit, pledged or purchased and to submit that electronic report to the City. There were no changes since the first reading.

Council was concerned if the current businesses know this is coming. Captain Croft stated that other cities are doing this and that they will be notified of this change.

Council questioned secondhand clothing stores having to do this. Mrs. Dreyer stated that if they are not currently using the electronic system then they would not need to do this.

Mike Parsons moved to approve Ordinance No 2084 an Ordinance Amending in Part Central Point Municipal Code 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Michael Parsons, At Large
SECONDER:	Rob Hernandez, At Large
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

C. Ordinance No. _____, An Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford for Use of the Public Right of Way Relating to Small Cell Wireless Facilities

Sydnee Dreyer presented the second reading of an Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford (“USCC”) for use of Public Right of Way Relating to small cell wireless facilities. No changes were made since the first reading.

Council was concerned if these towers interfere with the airport, Jeff Colantino with USCC answered that the proposed locations will not have any impact on air travel.

Rob Hernandez moved to approve Ordinance No. 2085 an Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford for use of the Public Right of Way Relating to Small Cell Wireless Facilities.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Rob Hernandez, At Large
SECONDER:	Kelley Johnson, Ward II
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

D. Public Hearing and First Reading - An Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230

Planning Director Stephanie Holtey presented the first reading of an Ordinance Amending the transportation system plan of Central Point Comprehensive plan to add road diet improvements to Project No 230 at the intersection of Scenic Avenue and Highway 99 when warrants are met. The project is identified in the TSP to address safety issues at the intersection. A traffic report performed by the Oregon Department of Transportation (ODOT) documented 36 accidents over a 10-year period that resulted in serious injuries and one (1) fatality. The traffic analysis shows that the current five (5) lane road configuration does not meet warrants and that adding road improvements reconfiguring Highway 99 to three (3) lanes allows signal warrants to be met as needed for the project to proceed.

There is no cost for the proposed amendment other than in-kind staff expenses associated with processing the application. It is important to note, however, that approval of the proposed amendment will allow ODOT to apply grant funds authorized through the All Roads Transportation Safety (ARTS) Program to construct Project No. 230 improvements. The total project cost is \$3M and the City, per a revised Intergovernmental Agreement, is contributing \$650K. The City was previously obligated to fund the project in its entirety as a condition for opening a new railroad crossing in Twin Creeks. Approval of the proposed TSP Amendment will allow grant fund disbursement allowing the City meet its obligation for the Twin

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Creeks Rail Crossing at significant cost savings.

Council was concerned as to why it needs to be a three lane rd. Mr. Samitore stated that there is not enough traffic to install a light with five lanes but with three lanes this would warrant a light to be installed

Melody Thueson moved to approve the first reading of Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230.

RESULT:	1ST READING [UNANIMOUS]
MOVER:	Melody Thueson, Ward III
SECONDER:	Taneea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

IX. BUSINESS

A. ARPA Project Eligibility Update

Finance Director Steve Weber presented an update on the ARPA Project Eligibility and that at the September 9, 2021 City Council meeting, staff presented a revised spending plan based on the City’s updated allocation (\$4.184 million) of the American Rescue Plan Act (ARPA) stimulus package that was approved by the Federal Government. The spending plan was compiled based staff’s interpretation of the U.S. Treasury guidelines issued at the time. For two of the projects, staff sought input from legal counsel that the projects are permitted under the referenced categories in the guidelines.

The two projects are the Dennis Richardson Memorial Project and the Central Point Little League Project, the City Attorney reviewed these projects in categories that were identified and these two projects fit in the treasury guidelines. The American Rescue Plan Act (ARPA) award is included in the City’s 2021-23 biennial budget.

B. Discussion of 2022 Water Rates

Matt Samitore presented Water Rate Study - The City rehired the FCS group to determine what rate structure adjustments the City of Central Point would need for the next decade. The revised rate would be based upon four dynamics. 1) is the projected annual increases from the MWC. 2) is that we did not achieve our water fund revenue projections/targets for FY 2020-2021. 3) dynamic involves the increasing price for goods and services. 4), with the UGB expansion forthcoming (and the associated need for a new reservoir), we need to plan for retiring debt service from the Vilas Pump Station and Reservoir by 2031, two years before what was initially planned.

Current water rates are based more on consumption then the base rates. Tiered rates are causing the consumption to go down, other cities are increasing the base rate \$1 every year. Central Point now has the 2nd lowest rates in Southern Oregon.

Two options were presented, A) is to continue to increase rates based on annual review of MWC Rate, inflation, previous water season revenue. B) would be to

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increase the base rate from \$15.57 to \$18.58, with the next potential increase in March of 2026.

Base on the math increasing the base rate is the best option, it is not popular for those who do not use much water and does not give them a chance to save. Right now the City's rates are 1/3 on base 2/3 on consumption. Consultants state you want 2/3 base and 1/3 consumption. Tiered rates encourages conservation, and certain amount of blight, base rates are regressive for residential customers on fixed incomes, we have a hardship program available. The state of Oregon has changed there metric of who is low income to 200% of the federal poverty level.

Council would like to see a stronger presentation on the Hardship Program to the seniors.

City Manager Chris Clayton stated that a phased approach to the base rate, if the council passed the plan and not implement it all at once, \$2 this year, \$2 next year, and \$1 the third year would still accomplish the city's goals.

Council shared that inflation is understandable, and cost of water. Council would like to see what a \$2 raise would do and what other options may look like.

City Manager Chris Clayton stated that this money is not eligible to be spent on anything outside of the water system, the only debt the water fund has is the water reservoir, and it will be taking on part of the corporation yard. Shortening the debt by 2yrs is to keep from having more debt on the water fund. Currently the city can only handle so much growth before we need another reservoir.

X. MAYOR'S REPORT

Mayor Hank Williams reported that he attended the Study Session.

XI. CITY MANAGER'S REPORT

City Manager Chris Clayton reported that:

- He received an email followed by a letter to the editor in the Mail Tribune about the City not displaying flags on Martin Luther King Day. Currently the flags are displayed on Presidents Day, Oregon's Birthday, Peace Officers Memorial Day, Armed Forces Day, Memorial Day, Flag Day, Independence Day, Labor Day, 9/11 Remembrance Day, Veterans Day and Pearl Harbor Day. With councils recommendation we can add Martin Luther King Day.

Discussion was made to amend the list and include MLK day.

- February Council Meeting changes, Council meeting will be on the February 17th and a Study Session on February 28th
- Changes made with opportunities for recycling electronic waste in the valley
- Dr. Bruce Dingler passed away.

- The RVCOG board approved the transition and the purchase of the senior center. Staff met with COG staff today and are working on a the plan for social and recreational opportunities for seniors.
- He has information on who is planning on purchasing the Banner Bank property
- MWC made an error in the cities favor in the amount of \$12,000 on the city's monthly statements.

XII. COUNCIL REPORTS

Council Member Kelley Johnson reported that she attended the study session, and appreciated seeing the public works building.

Council Member Neil Olsen reported that he attended the study session.

Council Member Tanea Browning reported that:

- Attended listening session in Vale, great attendance and participation from the region including Sen. Finley, several representatives from each city, including Nyssa, Burns, Ontario. Learned about the boarder board that addresses issues that arise specifically from being located near the border of Idaho and Oregon. The Board has additional tools available to these communities. Specifically, they had two onion sheds relocated to Idaho due to significant additional cost to rebuild in Oregon.
- March 10th listening session here in CP - much shorter car ride than when we all traveled to GP in my Subaru to elected essentials.
- She will be flying into DC next week for the State League Presidential fly in for NLC, this meeting along with the appointment to the state cabinet on infrastructure logistics is going to assist all of our Oregon cities in hopes to see the fed funding monies.
- Study session touring the new public works facility, very excited!

Council Member Rob Hernandez reported that:

- He attended by Zoom Jackson County/Central Point Community Center meeting.
- He attended the study session
- District 6 Bond Oversight Committee went and looked at new gym and Jewett and the new learning center.

Council Member Mike Parsons reported that:

- Attended the January Rogue Valley Services Board meeting. Housekeeping issues, personal policies updates, adjustments to the compensation plan were performed, and appointments to the budget committee were made.
- Attended the Study Session, tour of Public Works Building.

- Attended the Jackson County Public Safety Coordinating Council meeting. Funding mechanisms and collaborations to address behavior health housing needs were addressed by Julie Jackson with Jackson Care Connect.

Council Member Melody Thueson reported that she attended the study session.

XIII. DEPARTMENT REPORTS

Planning Director Stephanie reported that:

- The City's UGB Amendment is being reviewed by the State Department of Land Conservation and Development (DLCDC) and the Department of Justice. The deadline for UGB Acknowledgement is February 23, 2022. Due the fact an objection was filed, the UGB Amendment is subject to more rigorous review. However the objection had to with the State allowing prime farmland to be included in the City's urban reserves and not the City's application of criteria governing UGB Amendments. As such, no delay in the acknowledgement process is anticipated by the State. Once in the UGB, the newly added lands are not eligible to be annexed until the Transportation System Plan (TSP), Environmental Element and zoning code update and complete. All three projects are currently underway and scheduled to be completed concurrently within the next 18-24 months.
- All spaces at the new commercial building located at 98 Freeman Road are full except for one 1,100 square foot space that is seeing interest from a bagel shop and shipping center. Other spaces are leased to a dental office, physical therapy office, real estate office, Mazatlán Grill restaurant and Edward Jones office.
- Smith Crossing Phase 3 is scheduled for a Pre-Application Conference in mid-February. The development objective is to expand Smith Crossing into another phase East of Pear Valley. The site has several challenges associated with access, easements and floodplain impacts. The purpose of this meeting is to identify viable solutions to advance the project.

Police Captain Dave Croft reported that:

- A couple weeks ago the Police Department launched a pod cast called The Wide Blue Line. 1st episode was launched a couple of weeks ago, and Wednesday the 26th the 2nd episode was launched.
- In the last 2-3 weeks there was bank robbery and a robbery at the Purple Parrot, they have some good leads and good information on the vehicles and hope to have some conclusion to those soon.

Parks and Public Works Director Matt Samitore reported that:

- The restroom at Pfaff Park contractor is having some supply and demand issues, restroom not to be installed until the end of April. Temporary porta-potties will be setup.
- Meeting with ODOT and Jackson County regarding Beebe Rd extension UGB Expansion and Community Center.

- Attended a kickoff meeting on the Bear Creek Greenway plan.

Finance Director Steve Weber reported that the amended financing for the Public Works Operations Center closed

Jackson County Commissioner Dave Dotterer reported that:

- Greenway project was a great meeting.
- Special Session of the Oregon Legislature on the Marijuana and Hemp issues went well.

XIV. EXECUTIVE SESSION

XV. ADJOURNMENT

Rob Hernandez moved to adjourn. All said aye and the meeting was adjourned at 8:55 p.m.

The foregoing minutes of the January 27, 2022, Council meeting were approved by the City Council at its meeting of _____, 2022.

Dated:

Mayor Hank Williams

ATTEST:

City Recorder

Minutes Acceptance: Minutes of Jan 27, 2022 7:00 PM (CONSENT AGENDA)



City of Central Point Staff Report to Council

ISSUE SUMMARY

TO:	City Council	DEPARTMENT:	Parks and Recreation Commission
FROM:	Dave Jacob, Park Planner		
MEETING DATE:	February 17, 2022		
SUBJECT:	Proclamation - Arbor Week 2022		
ACTION REQUIRED:	Consent Agenda Item	RECOMMENDATION:	Approval

Purpose: The proclamation illustrates the value of tree awareness and of Arbor Week in Central Point.

Background: The Arbor Week proclamation is important to the City of Central Point in that it will assist in securing recertification for the eighth year of the Tree City USA designation for 2022. To qualify as a Tree City USA community, a city must meet the required standards established by The Arbor Day Foundation and the National Association of State Foresters. The standards help to ensure that every qualifying community, no matter the size, has a viable tree management plan which includes the celebration of Arbor Week. In addition to accomplishing Tree City USA goals, establishing an Arbor Week celebration will help to raise awareness of the importance of trees in Central Point, expand the tree canopy by planting additional trees, and enhance the parks and open spaces where the trees are planted.

Recommendation: Approve the Arbor Week proclamation and join Central Point Parks and Recreation Department in celebration of Arbor Week: April 3 - 9, 2022.

ATTACHMENTS:

1. ARBOR WEEK PROCLAMATION 2022.docx

ARBOR WEEK PROCLAMATION

WHEREAS, J. Sterling Morton proposed to the Nebraska Board of Agriculture in 1872 that a special day be set aside for the planting of trees; and

WHEREAS, Arbor Day was first observed with the planting of more than a million trees in Nebraska; and

WHEREAS, Arbor Day is now observed throughout the nation and the world; and

WHEREAS, trees can reduce the erosion of topsoil by wind and water, reduce heating and cooling costs, moderate the temperature, clean the air, produce oxygen, strengthen stream bank integrity, provide flood mitigation, and provide a habitat for wildlife; and

WHEREAS, trees are a renewable resource that provides paper, wood, fuel for our fires, and countless other wood products; and

WHEREAS, trees in our city increase property values, enhance the economic vitality of business areas, and beautify our community; and

WHEREAS, trees, wherever they are planted, are a source of joy and spiritual renewal,

NOW, THEREFORE, I _____ Mayor of the city of Central Point, do hereby proclaim April 3rd – 9th, 2022 as

Arbor Week

in the City of Central Point, and I urge all citizen to celebrate Arbor Week and to support efforts to protect our trees and woodland and

Futher, I urge all citizens to plant and care for trees to gladden the heart and promote the well-being of this and future generations.

Date this _____ Day of _____ in the Year _____

Mayor _____

Attachment: ARBOR WEEK PROCLAMATION 2022.docx (1507 : Proclamation - Arbor Week 2022)



City of Central Point Staff Report to Council

ISSUE SUMMARY

TO: City Council **DEPARTMENT:** Administration

FROM: Deanna Casey, City Recorder

MEETING DATE: February 17, 2022

SUBJECT: Parks and Recreation Commission Appointment

ACTION REQUIRED: Consent Agenda Item **RECOMMENDATION:**

In December 2021 Parks and Recreation Commissioner Jennifer Horton resigned because she moved out of the city limits. This leaves one vacancy on the Central Point Parks and Recreation.

City staff advertised in the city newsletter in December and January and have received one application from Eden Foster. Mrs. Foster is currently on the Budget Committee and the current board president for the Park and Recreation Foundation. She was active during the Daffodil project last year.

Staff recommends appointing Ms. Foster to the Parks and Recreation Commission to complete the current term ending December 31, 2023.

Current Parks and Recreation Commission members:

Patricia Alvarez	Lee Orr	Stephanie Hendrickson
Ronald Woodhead	Dennis Browning	Fran Cordeiro-Settell

Recommended motion:

I move to approve the Consent Agenda as presented.

ATTACHMENTS:

1. Eden Foster Application_Redacted

City of Central Point, Oregon
140 S 3rd Street, Central Point, OR 97502
541.664.3321 Fax 541.664.6384
www.centralpointoregon.gov



Administration Department
Chris Clayton, City Manager
Deanna Casey, City Recorder
Elizabeth Simas, Human Resource Director

**APPLICATION FOR APPOINTMENT TO
CITY OF CENTRAL POINT COMMITTEE**

Name: Eden Foster Date: 2-3-2022

Address: [Redacted] Shaker Drive, Central Point, OR 97502

Home Phone: _____ Business Phone: _____ Cell Phone: [Redacted]

Fax: _____ E-mail: [Redacted]@gmail.com

Are you a registered voter with the State of Oregon? Yes X No _____

Are you a city resident? Yes X No _____ If Yes, How long: 2006

Which committee(s) would you like to be appointed to: *(Please make sure the dates below work with your schedule before applying. Council and Planning Commission members are required to file an Annual Statement of Economic Interest to the State of Oregon.)*

- Meeting Dates (All meeting dates are subject to change or additions, times vary for each committee):**
- Budget Committee: Meetings vary in April Bi-Annually
 - Citizens Advisory Committee: 2nd Tuesday of quarterly
 - Planning Commission: 1st Tuesday of each month
 - Parks and Recreation Committee/Foundation: Meeting dates vary

Employment, professional, and volunteer background:

Community affiliations and activities:

Previous City appointments, offices, or activities:

Attachment: Eden Foster Application_Redacted (1508 : Park Commission Appointment)

Central Point Committee Application
Page 2

To provide additional background for the Mayor and City Council, please answer the following questions.

1. Please explain why you are interested in the appointment and what you would offer to the community.

2. Please describe what you believe are the major concerns of the City residents and businesses that this committee should be concerned about.

3. Please provide any additional information or comments which you believe will assist the City Council in considering your application.

4. Do you anticipate that any conflicts of interest will arise if you are appointed; and if so, how would you handle them?

Please feel free to use additional sheet if you have more information to help the Council make a final decision.

My signature affirms that the information in this application is true to the best of my knowledge. I understand that misrepresentation and/or omission of facts are cause for removal from any council, advisory committee, board or commission I may be appointed to. All information/documentation related to service for this position is subject to public record disclosure.

Signature: _____



Date: _____

2-3-2022

Attachment: Eden Foster Application_Redacted (1508 : Park Commission Appointment)

Application for Employment to City of Central Point

Parks and Recreation Committee

Eden Foster

February 3, 2022

Employment, professional and volunteer background:

I worked for 25 years in land conservation and public garden education programming and management for various public gardens, arboreta, and land conservation organizations around the country. During that period I also earned a master's degree in Urban Affairs and Planning from Boston University. I later became a full-time freelance writer for twelve years. After moving to Oregon, I worked as a receptionist for Twin Creeks Retirement from 2009-2011, and since then have led an active retired life.

Community affiliations and activities:

Co-founder, Plant a Thousand Daffodils, a program jointly sponsored by the Parks and Recreation Foundation and the Parks and Recreation Department. Recruited more than 100 volunteers to plant 1,700 daffodil bulbs in three public parks and two public areas in Central Point. Raised money to pay for the program and to benefit the Foundation. Am currently involved in designing a poetry contest for elementary students and art contest for middle and high school students that will take place this spring.

Have volunteered for special events sponsored by Parks and Recreation since approximately 2010 and have taught several classes on a voluntary basis.

Central Point Friends of the Library: serve as interim secretary, member of Book Buffs, steward for Little Free Library at Don Jones Park, 2020 to present

Delivered meals daily for six weeks to fire victims and did various miscellaneous service activities, fall, 2020

Travelled to Guatemala to deliver water filtration systems to villages with the Medford Rogue Rotary, annually 2014-2019

Worked with Rogue Climate to lobby State legislators on climate-related bills, various years

Served on the board of directors for Armadillo Technical Institute in Phoenix, OR from 2006-2011

Homeowner's Association Board of Directors, 2007-2011

Previous City appointments, offices or activities:

Parks and Recreation Foundation board member and current president, 2017-present

Central Point Budget Committee, 2021-present

1. Please explain why you are interested in the appointment and what you would offer to the community.

I believe that parks and recreational activities play an integral role in the physical, mental and emotional health of our community. My background in garden programing and management as well as my interest in city planning is easily translated into work with the Parks and Recreation Department. Over the years I have forged positive relationships with City staff in the department and with volunteers and businesses in the community. I believe these relationships will benefit my work with the Parks and Recreation Committee.

2. Please describe what you believe are the major concerns of the City residents and businesses that this committee should be concerned about.

Health in all its forms has become even more critically important since the advent of COVID-19. Parks and Recreation can provide safe, high-quality opportunities to participate in healthful and fun activities while building a strong sense of community. Central Point has long been admired for its numerous parks and the city is now a sought-after destination to both live and work. I believe that a long-range plan for keeping the current park's infrastructure sound and planning for the development of new open spaces is of critical importance to this committee so that we continue to attract families and businesses to our community. I also believe that an emphasis on diversity as it relates to programing and accessibility is also extremely important.

3. Please provide any additional information or comments which you believe will assist the City Council in considering your application.

I have experienced great joy in volunteering with the Parks and Recreation department for more than a decade. The staff has been generous with their time and ideas and has been warm and welcoming. We are so fortunate to have such high-quality professionals who have selected Central Point as their place to work.

4. Do you anticipate that any conflicts of interest will arise if you are appointed?

I do not anticipate any conflicts of interest.



City of Central Point Staff Report to Council

ISSUE SUMMARY

TO: City Council **DEPARTMENT:** Community Development

FROM: Stephanie Holtey, Planning Director

MEETING DATE: February 17, 2022

SUBJECT: Ordinance No. _____, An Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230

ACTION REQUIRED: Ordinance 2nd Reading **RECOMMENDATION:** Approval

BACKGROUND INFORMATION:

On January 11, 2022, the Planning Commission unanimously approved Resolution No. 892 recommending the City Council approve a Major Amendment to the Transportation System Plan (TSP), to add road diet improvements to Project No. 230. As currently written, Project No. 230 is to install a traffic signal at the intersection of Scenic Avenue and Highway 99 *when warrants are met*. The project is identified in the TSP to address safety issues at the intersection. A traffic report performed by the Oregon Department of Transportation (ODOT) documented 36 accidents over a 10-year period that resulted in serious injuries and one (1) fatality. The traffic analysis shows that the current five (5) lane road configuration does not meet warrants and that adding road improvements reconfiguring Highway 99 to three (3) lanes allows signal warrants to be met as needed for the project to proceed.

During deliberations, the Planning Commission discussed concerns associated with potential impacts of the road diet on emergency evacuations and future traffic volume increases. These concerns were addressed in the Staff Report dated January 11, 2022 and during the meeting. The Planning Commission's recommendation reflects resolution of the issues and is based on the documented need to address unsafe conditions at the intersection and findings of fact and conclusions of law that demonstrate the proposed amendment is consistent with applicable state and local criteria.

At the January 27, 2022 City Council meeting staff presented the proposed amendment for public hearing and first reading of the ordinance approving the amendment.

FINANCIAL ANALYSIS:

There is no cost for the proposed amendment other than in-kind staff expenses associated with processing the application. It is important to note, however, that approval of the proposed amendment will allow ODOT to apply grant funds authorized through the All Roads Transportation Safety (ARTS) Program to construct Project No. 230 improvements. The total project cost is \$3M and the City, per a revised Intergovernmental Agreement, is contributing

\$650K. The City was previously obligated to fund the project in its entirety as a condition for opening a new railroad crossing in Twin Creeks. Approval of the proposed TSP Amendment will allow grant fund disbursement allowing the City meet its obligation for the Twin Creeks Rail Crossing at significant cost savings.

LEGAL ANALYSIS:

The proposed Comprehensive Plan Amendment to the TSP is subject to the approval criteria in CPMC 17.96.500 for Major Amendments. To approve a Major Amendment, the City Council must find that it is consistent with applicable Statewide Planning Goals, the City of Central Point Comprehensive Plan and the State Transportation Planning Rule. As demonstrated in the Findings of Fact and Conclusions of Law, the proposed amendment is consistent with the applicable state and local criteria.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

The proposed TSP Amendment is consistent with the following goals and strategies in the 2040 Strategic Plan:

- Community Investment, Goal 5 - Plan, design and construct modern and efficient infrastructure in all areas and systems.

The current configuration at the intersection of Highway 99 and Scenic Avenue is skewed and has not been upgraded to urban standards. The proposed urban upgrade improvements are generally consistent with Goal 5 and specifically Strategy 3, which has to do with seeking partnerships with the County and ODOT to eliminate infrastructure deficiencies and build for the future. This proposed amendment will allow the City to capitalize on partnerships with ODOT and the County to fund needed improvements to the intersection.

- Responsible Governance, Goal 1 – Maintain a strong financial position that balances the need for adequate service levels and capital requirements against the affordability that is desired by our citizens.

Strategies 2 and 3 address working with and soliciting support from partnering agencies for the purpose of maximizing efficiency and maintaining a robust financial position. The proposed amendment supports the City's ability to partner with ODOT and the County to design and build improvements that maximize cost savings to City will providing improvements needed to minimize traffic and hazards and provide connectivity for vehicle, bicycle, pedestrian and railroad transportation modes.

- Vibrant Economy, Goal 1. Manage growth to provide a timely and orderly provision of facilities and services.

This goal recognizes the connection between transportation, land use and the City's ability to support a vibrant economy. The proposed amendment will allow an identified and needed project in the TSP to be built for the benefit of existing and new development within the City's UGB.

STAFF RECOMMENDATION:

Consider the second reading of the Ordinance Approving a Major Amendment to the Comprehensive Plan Adding Road Diet Improvements to Project No. 230 in the Transportation System Plan and forward it to a second reading without changes.

RECOMMENDED MOTION:

I move to approve Ordinance No. _____, an Ordinance Approving a Major Amendment to the Comprehensive Plan Adding Road Diet Improvements to Project No. 230 in the Transportation System Plan.

ATTACHMENTS:

1. TSP Amendment Ordinance (First Reading)
2. Exhibit 1 to CC Ordinance

ORDINANCE NO. _____

AN ORDINANCE AMENDING THE TRANSPORTATION SYSTEM PLAN (TSP) OF THE CENTRAL POINT COMPREHENSIVE PLAN TO ADD ROAD DIET IMPROVEMENTS TO PROJECT NO. 230.

Recitals:

- A. Words ~~lined through~~ are to be deleted and words **in bold** are added.
- B. The City of Central Point (City) is authorized under Oregon Revised Statute (ORS) Chapter 197 to prepare, adopt and revise comprehensive plans and implementing ordinances consistent with the Statewide Land Use Planning Goals.
- C. The City has coordinated its planning efforts with the State in accordance with ORS 197.040(2)(e) and OAR 660-030-0060 to assure compliance with goals and compatibility with City Comprehensive Plans.
- D. Pursuant to the requirements set forth in CPMC Chapter 17.96.100 Comprehensive Plan and Urban Growth Boundary Amendments – Purpose and Chapter 17.05.500, Type IV Review Procedures, the City has initiated an application and conducted the following duly advertised public hearings to consider the proposed amendment:
 - a) Planning Commission hearing on January 11, 2022
 - b) City Council hearing on January 27, 2022.

THE PEOPLE OF THE CITY OF CENTRAL POINT DO ORDAIN AS FOLLOWS:

Section 1. Amendments to TSP Chapter 7 – Street System, 2008-2030 to read:

Table 7.4 Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.

Section 2. Amendments to TSP Chapter 12 – Transportation System Financing Program to read:

Table 12.5. Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.

Section 3. Evidence. Based on all the information received, the City Council adopts Planning Commission Resolution No. 892 and its attachments (Exhibit 1) as evidence that justifies adoption of the TSP Amendment as set forth in Sections 1 and 2 of this Ordinance.

Section 4. The City Manager is directed to conduct post acknowledgement procedures defined in ORS 197.610 et seq. upon adoption of the TSP Amendment.

Section 5. Effective Date. The Central Point City Charter states that an ordinance enacted by the council shall take effect on the thirtieth day after its enactment. The effective date of this ordinance will be the thirtieth day after the second reading.

Passed by the Council and signed by me in authentication of its passage this _____ day of _____, 20____.

Mayor Hank Williams

ATTEST:

City Recorder

PLANNING COMMISSION RESOLUTION NO. 892

A RESOLUTION OF THE PLANNING COMMISSION FORWARDING A FAVORABLE RECOMMENDATION TO THE CITY COUNCIL TO AMEND THE COMPREHENSIVE PLAN TO ADD ROAD DIET IMPROVEMENTS TO PROJECT NO. 230 IN THE TRANSPORTATION SYSTEM PLAN

(File No: CPA-20001)

WHEREAS, on January 11, 2022 the City of Central Point Planning Commission held a duly-noticed public hearing, reviewed staff reports, findings of fact and heard public testimony on a Major Revision to the Central Point Comprehensive Plan, Transportation System Plan; and

WHEREAS, the Planning Commissions determined that the revisions as proposed were in the public interest and that the general welfare of the public will benefit by the proposed revisions to improve safety at the Highway 99/Scenic Avenue intersection; and

WHEREAS, after reviewing the requested proposal and considering public testimony it is the determination of the Central Point Planning Commission that the proposed amendment as set forth in attached Exhibit "A" dated January 11, 2022 are adjustments that do not alter, or otherwise modify the uses and character of development and land use within the City of Central Point, and is therefore determined to be consistent with all of the goals, objectives, and policies of the City's Comprehensive Plan and State Planning Goals.

NOW, THEREFORE, BE IT RESOLVED that the City of Central Point Planning Commission by Resolution No. 892 does hereby accept, and forward to the City Council a recommendation that the City Council favorably consider amending the City of Central Point Comprehensive Plan, Transportation System Plan (TSP) as set forth in the attached Exhibit "A" including Attachments "A" through "E" provided therein.

PASSED by the Planning Commission and signed by me in authentication of its passage this 11th day of January, 2022.


Planning Commission Chair

ATTEST:


City Representative

Approved by me this 11th day of January, 2022.


Planning Commission Chair

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



Staff Report

Transportation System Plan (TSP) Amendment (Project No. 230)
File No. CPA-20001

January 11, 2022

Item Summary

Consideration of a Comprehensive Plan Text Amendment to the Transportation System Plan (TSP) concerning improvements at and around Scenic Avenue and Highway 99 (Project No. 230). Applicant: City of Central Point.

Staff Source

Stephanie Holtey, Planning Director

Background

The Oregon Department of Transportation applied and received funding through the All Road Transportation Safety (ARTS) Program to signalize the intersection at Highway 99 and Scenic Avenue and apply a road diet (i.e. 5 lanes to 3 lanes with a center turn lane from the Exit 35 interchange (MP 0.35) to MP 1.95 just south of Brookhaven Drive) (Attachment "A" and "C"). This project addresses known traffic safety issues by reducing the number and severity of traffic accidents at this location (Attachment "B"). The project is consistent with the City of Central Point TSP Project No. 230 with the exception of the road diet improvements. Based on current volumes and the five (5) lane configuration on Highway 99, signal warrants are not met. Adding the road diet will allow signal warrants to be met as needed for the project to proceed. At the request of ODOT, staff introduced a proposed revision to the TSP at the October 5th Planning Commission meeting to add the road diet to Project No. 230. After asking questions and expressing some concerns, the Planning Commission directed staff to schedule a public hearing to consider the proposed TSP Amendment.

At the January 11, 2022 Planning Commission meeting staff will present the proposed amendment to the TSP for a public hearing. The proposed revision is a Major Revision to the Central Point Comprehensive Plan. At the conclusion of the public hearing, the Planning Commission will consider the revision relative to the approval criteria in CPMC 17.96.500 and make a recommendation to the City Council for final decision.

Issues

At the October 5, 2021 Planning Commission meeting there was discussion and some concern that applying a road diet would worsen the ability of residents to evacuate the city in the event of wildfire. There were also concerns relating to heavy traffic volumes on Highway 99, particularly related to incoming traffic from Highway 62. Provided below is information related to the issues of evacuation safety and traffic volume on Highway 99.

Evacuation: The proposed TSP Amendment adding a road diet will reduce the number of lanes in the city limits but not the pavement width. Based on consultation with Public Works, the full pavement width can be used during times of emergency to evacuate residents if necessary. The City is in the process of updating its TSP. As part of that planning effort, the consultant will be looking at transportation needs and mitigation measures during times of emergency to look at community concerns related to wildfire evacuations. In the meantime, the City has created Neighborhood Evacuation Zones to phase evacuations based on location to minimize congestion and facilitate more orderly traffic movements during emergencies.

Traffic Volume: ODOT prepared a Traffic Report that looks that traffic volumes on Highway 99 now and in 2040 (Attachment "D"). The report found that there are 6,800 Average Daily Trips currently with 18.5% being heavy vehicle traffic. In 2040 this is forecast to increase to 8,870 Average Daily Trips. This is much less than other arterials in the City. Public Works reported that Pine Street has 21,000; Beall Lane has 15,000 and Twin Creeks Crossing has 5,000 Average Daily Trips. Based on this data, the impacts from traffic volume are not likely to negatively impact this transportation facility in the next 20-years. However, if the conditions change, the Highway 99 can be reconfigured back to five (5) lanes.

Findings of Fact & Conclusions of Law

The Major Comprehensive Plan Amendment to the TSP has been evaluated against the applicable criteria set forth in CPMC 17.96 and found to comply as evidenced in the Planning Department Findings of Fact and Conclusions of Law (Attachments "D").

Attachments

- Attachment "A" – Proposed TSP Amendment (Tables 7.4 and 12.5)
- Attachment "B" – Project Location Map
- Attachment "C" – Proposed Project Plans (Sheets A01, QB01-QB10)
- Attachment "D" – ODOT Traffic Report
- Attachment "E" – Planning Department Findings of Fact and Conclusions of Law
- Attachment "F" – Draft Planning Commission Resolution No. 892

Action

Consider the proposed Major Comprehensive Plan Amendment to the TSP and 1) approve; 2) approve with revisions; or 3) deny the application.

Recommendation

Approve Resolution No. 892 recommending the City Council approve the TSP Amendment without revisions per the Staff Report dated January 11, 2022 including all attachments.

ATTACHMENT "A" – Proposed Transportation System Plan Revisions

Table 7.4, Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Year	ODOT	County	Central Point	Medford	Other	
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	•	•	•					√			√	2012	♦	♦	♦			

Table 12.5, Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Tier	ODOT	County	Central Point	Medford	Other	Total Project Cost	
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	■	■	■					√			√	Tier 1, Long	♦	♦	♦				\$2,737,300



TSP Amendment: Project No. 230 (Highway 99/Scenic Avenue)

ATTACHMENT "B"

Project Location Map (Approximate)
CPA-20001

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

ATTACHMENT "C"

STATE OF OREGON
 DEPARTMENT OF TRANSPORTATION
 PLANS FOR PROPOSED PROJECT
 GRADING, PAVING, CURB RAMPS, SIGNING & SIGNALS
OR99: I-5 TO SCENIC AVE SECTION
 ROGUE VALLEY HIGHWAY
 JACKSON COUNTY
 SEPTEMBER 2022

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
A01	Title Sheet
A02	Index Of Sheets Cont. & Std. Day: Nos.

??V-???



Overall Length Of Project - 1.54 Miles

ATTENTION:
 Oregon Law Requires That All Plans Adopted
 By The Oregon Utility Notification Center
 Through OAR 552-001-0100.
 You May Be Notified By Calling
 The Center Those Plans Are For.
 The Oregon Utility Notification Center is
 (503) 232-1987.

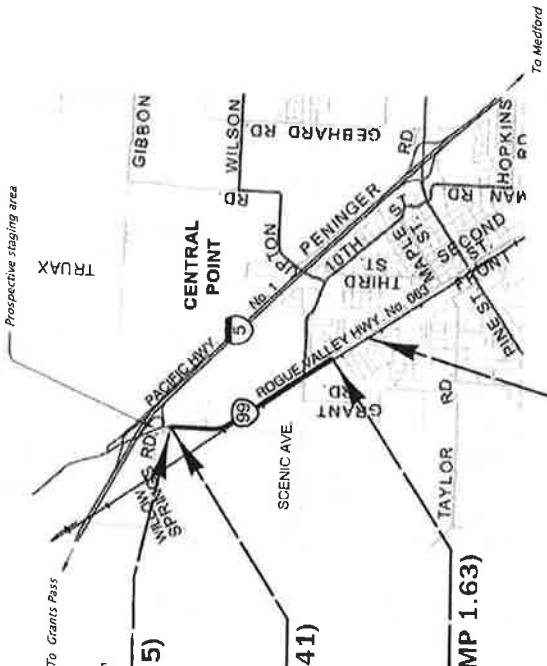
LET'S ALL
 WORK TOGETHER
 TO MAKE THIS
 JOB SAFE

**PRELIMINARY COPY
 INFORMATION ONLY**

OREGON TRANSPORTATION COMMISSION
 Chair
 Commissioner
 Commissioner
 Commissioner
 Commissioner
 Director Of Transportation
 These plans were developed using ODOT design standards
 and approved by the ODOT Chief Engineer or their delegate
 authority.
 Approving Authority: _____
 Signature & date
 Mark Thompson, Reg. 3 Tech. Cr. Manager
 Print name and title
 Concurrence by ODOT Chief Engineer



T. 36 S., R. 2 W., W.M.
 T. 37 S., R. 2 W., W.M.



BEGINNING OF CONTRACT
STA. "99 RW" 457+88.19 (MP 0.35)

BEGINNING OF PROJECT
STA. "99 RW" 460+95.94 (MP 0.41)

END OF PROJECT
STA. "99 RW" 526+12.43 (MP 1.63)

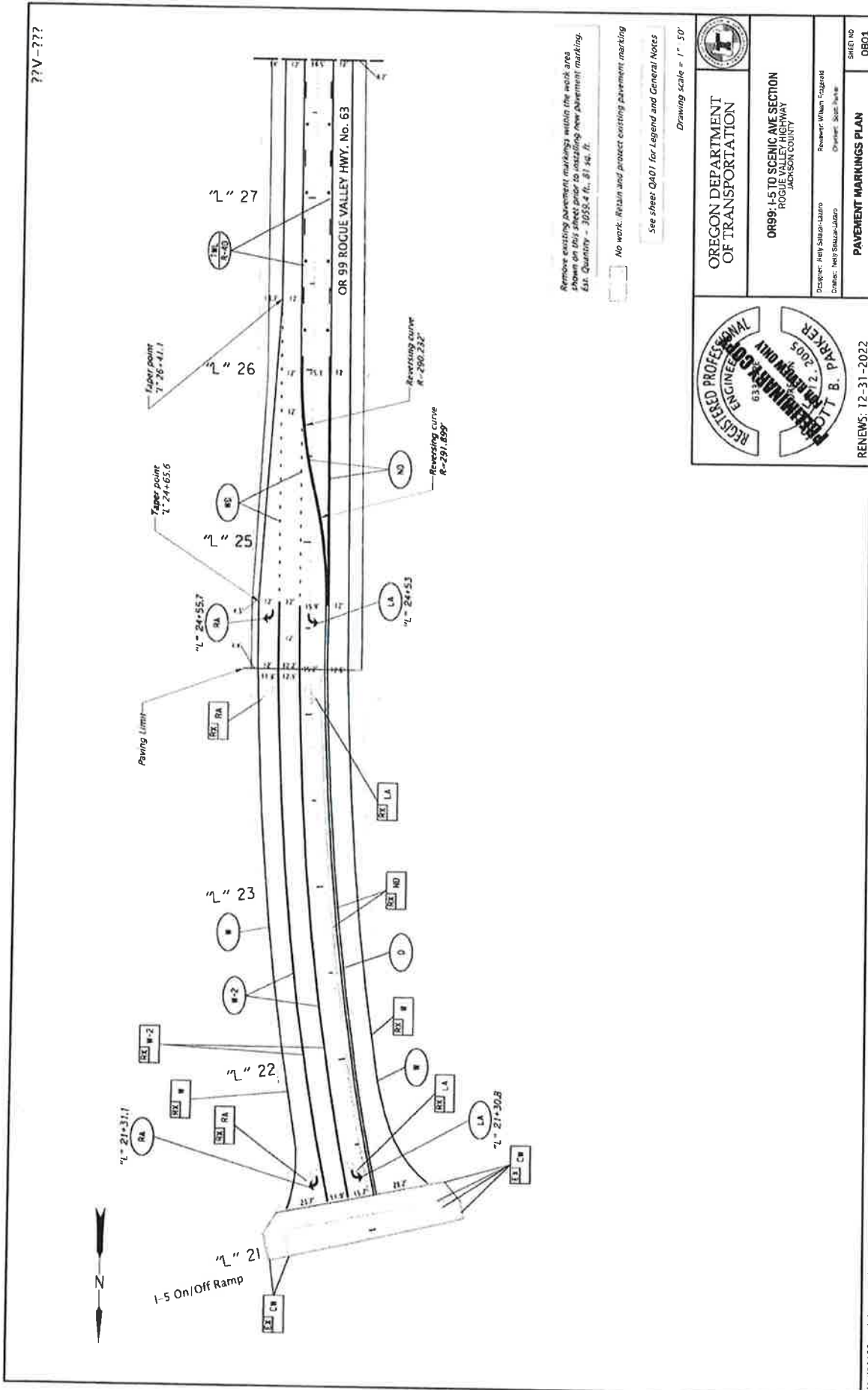
END OF CONTRACT
STA. "99 RW" 542+87.69 (MP 1.95)

OR99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY		
FEDERAL HIGHWAY ADMINISTRATION	PROJECT NUMBER	SHEET NO.
OREGON DIVISION	K20285	A01

FINAL ELECTRONIC DOCUMENT
 AVAILABLE UPON REQUEST
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PC02982-000



Remove existing pavement markings within the work area shown on this sheet prior to installing new Abatement marking. Est. Quantity - 3055.4 fl. 87 sq. ft.

No work: Retain and protect existing pavement marking

See sheet Q401 for Legend and General Notes

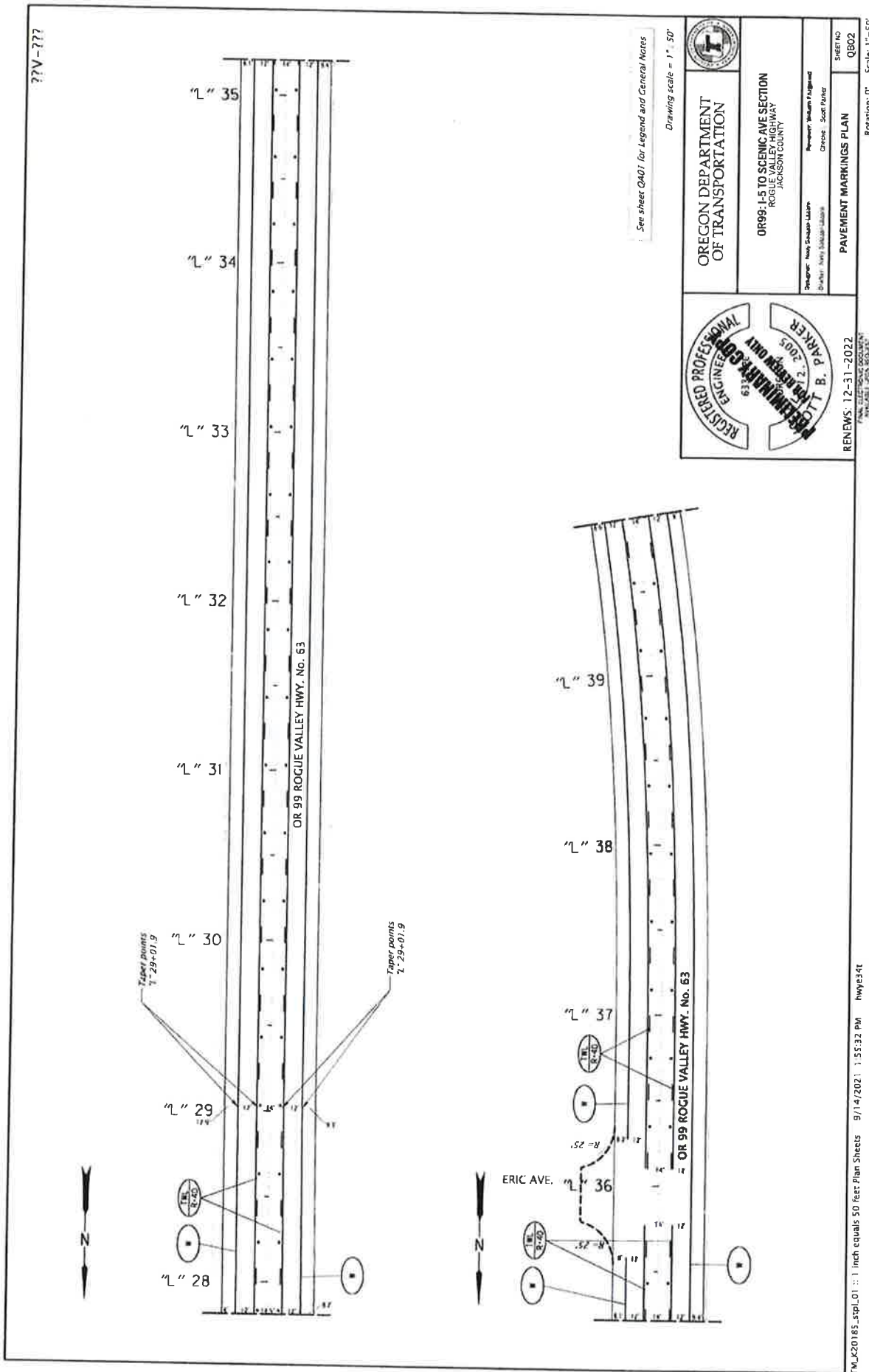
Drawing scale = 1" = 50'

<p>OREGON DEPARTMENT OF TRANSPORTATION</p>	<p>OR99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY</p>	<p>Designer: Amy Salomon-Laurio Checker: Scott Taylor</p>	<p>SHEET NO Q801</p>
		<p>Reviewer: William Fitzgerald</p>	<p>PAVEMENT MARKINGS PLAN</p>

REGISTERED PROFESSIONAL ENGINEER
 LICENSE NO. 1187
 B. PARKER
 9/2/2005
 613 E. Main Street
 Medford, OR 97504

REVISIONS: 12-31-2022
 FROM ELECTRONIC DRAWING
 TO PAPER PRINTING

TM_K20185_spl_01 (1) inch equals 50 feet Plan Sheets 9/14/2021 11:55:31 PM hwy634r Scale: 1" = 50'



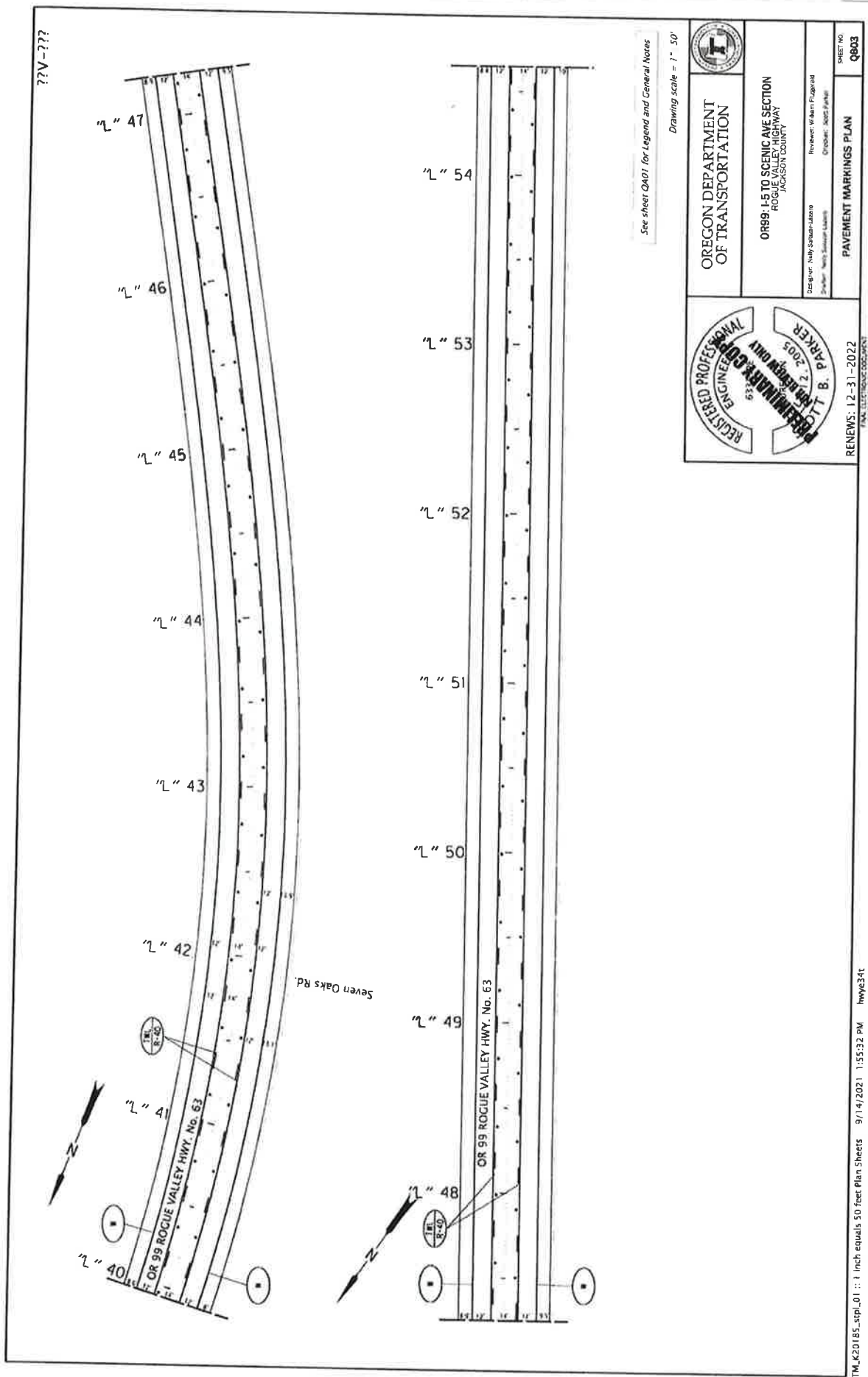
See sheet Q407 for Legend and General Notes

Drawing scale = 1" = 50'

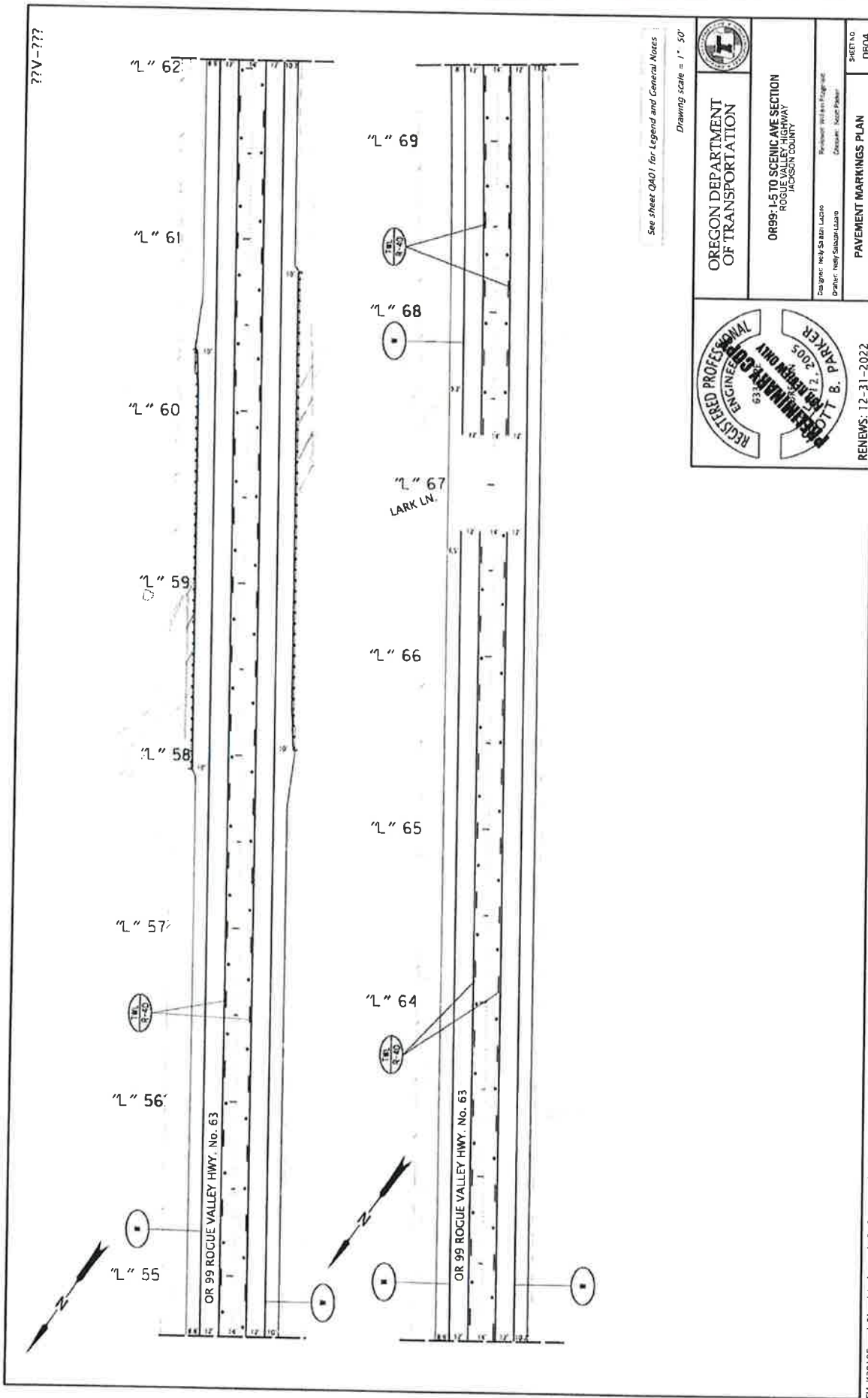
<p>REGISTERED PROFESSIONAL ENGINEER BRETT B. PARKER LICENSE NO. 2005 STATE OF OREGON</p>	<p>OREGON DEPARTMENT OF TRANSPORTATION</p>
	<p>OR99: L5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY</p>
<p>Designer: Andy Swanson-Lalor Checker: Andy Swanson-Lalor</p>	<p>Preparer: William Polglund Checker: Scott Parker</p>
<p>RENEWS: 12-31-2022</p>	<p>PAVEMENT MARKINGS PLAN SHEET NO. Q802</p>

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Rotation: 0°

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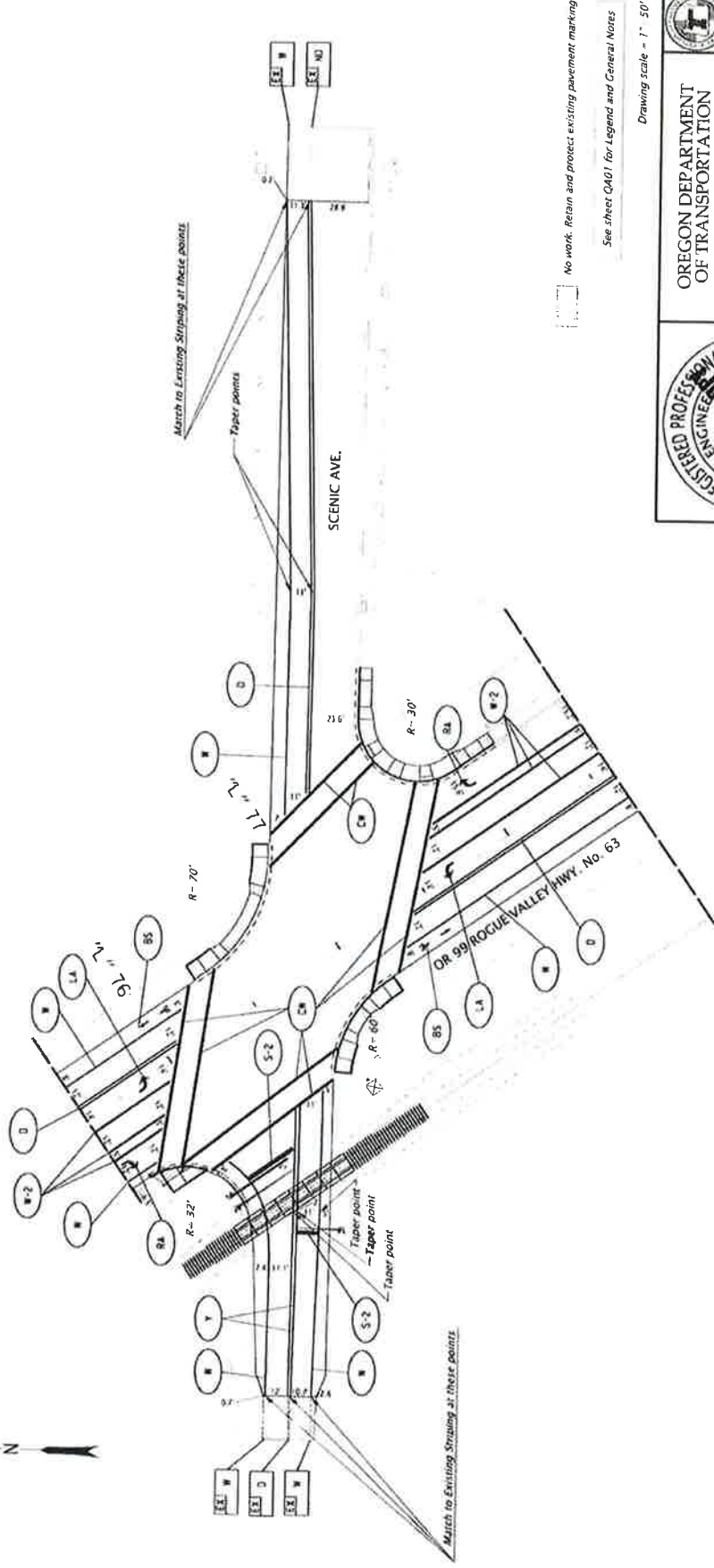


Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

??V-???



No work. Retain and protect existing pavement marking

See sheet Q401 for Legend and General Notes

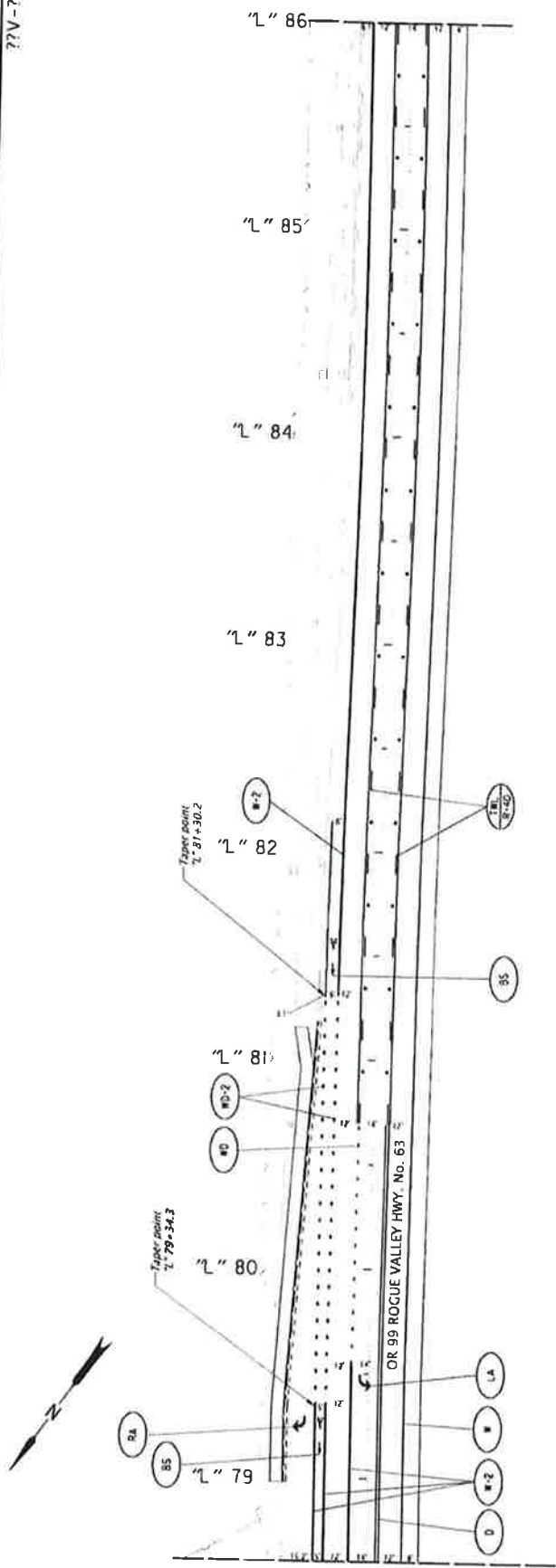
Drawing scale = 1" = 50'

	OREGON DEPARTMENT OF TRANSPORTATION OR99-15 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY
	Designer: Nelly Salazar-Lizaso Designer: Leahy Salazar-Lizaso Reviewer: William Fitzgerald Checker: Scott Phelan
PAVEMENT MARKINGS PLAN	
SHEET NO. QB05 RENEWS: 12-31-2022	

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??V-???



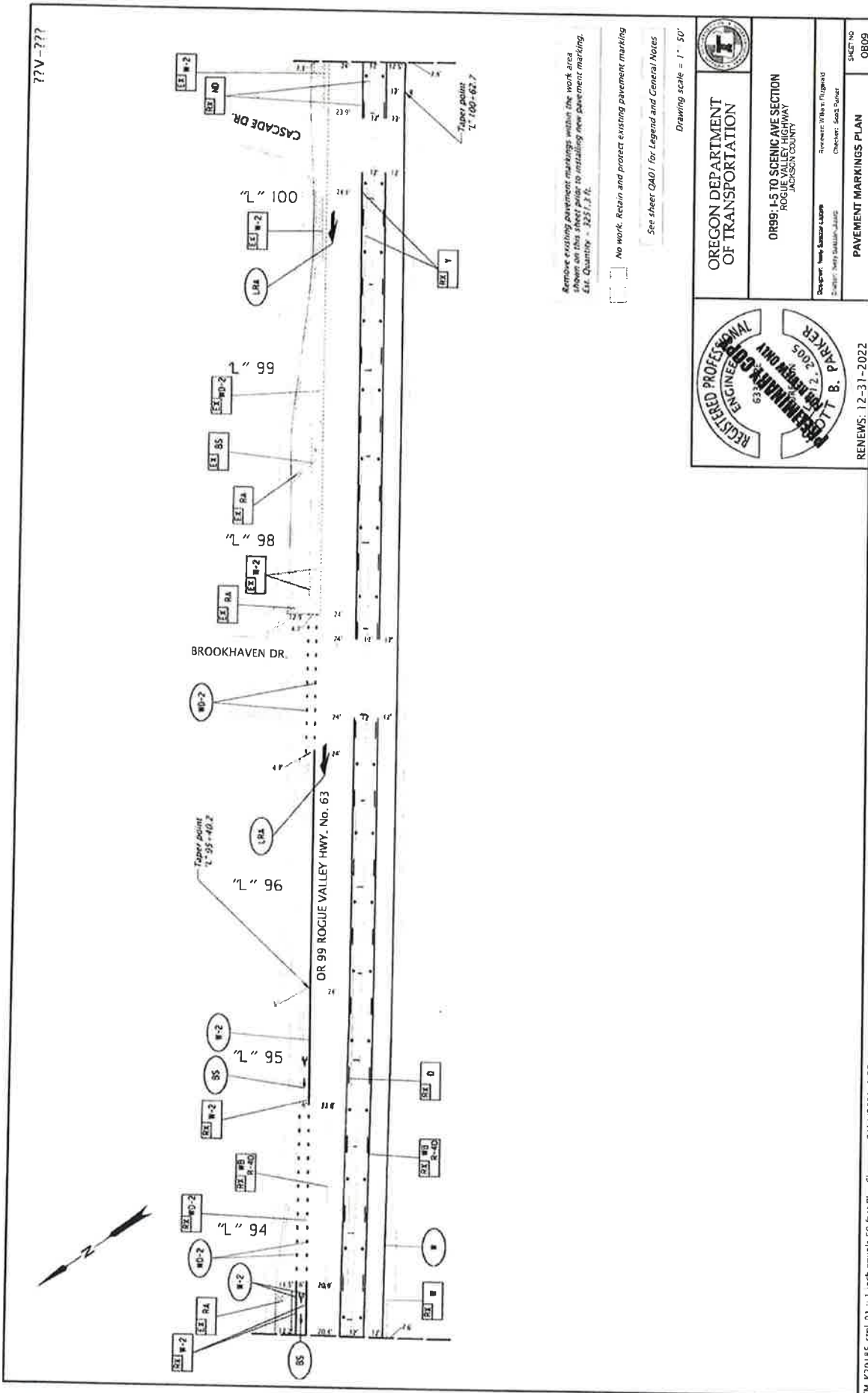
See sheet Q401 for Legend and General Notes

Drawing scale = 1" = 50'

	OREGON DEPARTMENT OF TRANSPORTATION
	OR99 - LE TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY
Designer: Andy Salsman/Landis Drafter: Andy Salsman/Landis	Designer: Alan Ferguson Checker: Scott Porter
REVISIONS: 12-31-2022 <small>FOR ELECTRONIC DELIVERY PRINTABLE PDF REQUIRED</small>	SHEET NO. Q407 SHEET NAME PAVEMENT MARKINGS PLAN

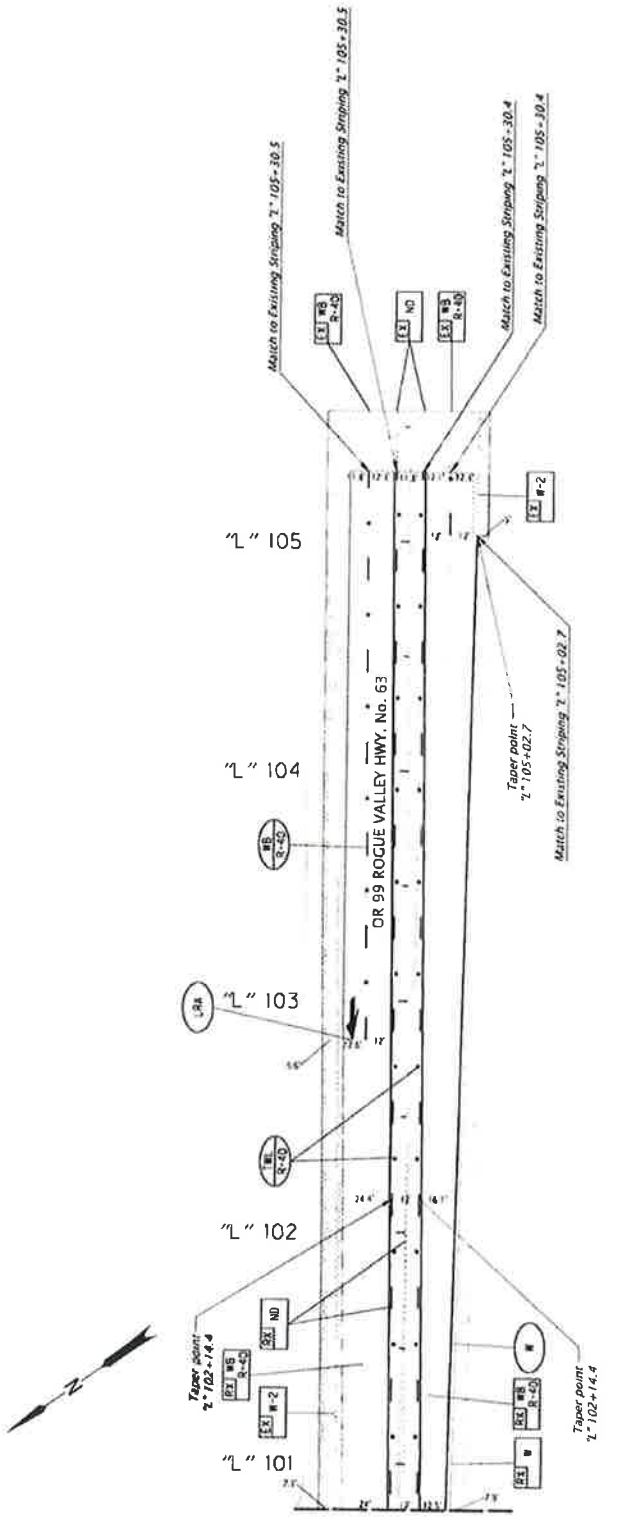
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Rotation: 0° Scale: 1" = 50'



Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

??V-???



Remove existing pavement markings within the work area shown on this sheet prior to installing new pavement markings. Etc. Quantity = 2484.8 Ft.

No work. Retain and protect existing pavement marking.

See sheet Q401 for Legend and General Notes.

Drawing scale = 1" = 50'

	OREGON DEPARTMENT OF TRANSPORTATION
	OR99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY
Designer: Kelly Johnson-Lubard Checker: Kelly Johnson-Lubard	Reviewer: William Fitzgerald Checker: Scott Perine
PAVEMENT MARKINGS PLAN	SHEET No. QB10
RENEWS: 12-31-2022 <small>REGISTERED PROFESSIONAL ENGINEER</small>	Rotation: 0° Scale: 1"=50'

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Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



Oregon

Kate Brown, Governor

Department of Transportation

**Region 3 Traffic
District # 8**

100 Antelope Rd
White City, OR 97502
Phone 541-774-6359

Traffic Report

**OR 99: I-5 to Scenic
Rogue Valley Highway No. 63 M.P. 0.42 – 1.64
KN 20185
Jackson**

Prepared By:

William Fitzgerald, PE
Traffic Operation Engineer



Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

Traffic Data

On OR 99, there are two lanes in each direction of travel between MP 0.42 and 1.64. Average Annual Daily Traffic (AADT) in this section is 6800 vehicles with approximately 18.5% heavy vehicles. Future (2040) AADT is projected to be 8870 vehicles. The project limits are shown in the figure below.



Figure 1 Area Map.

Introduction

This project will install a road diet along OR 99 throughout this section of highway. There will be a grind inlay to facilitate the road diet. A signal will be installed at the intersection of OR 99 @ Scenic Ave. This project is a safety project due to the number of severe crashes that have been occurring along this section of highway. This is an ARTS funded safety project. Traffic section control The Example Project brief scoping level description including type of work (paving, etc.), reason for project and other current known details. Indicate funding source, and especially emphasize if project is a safety related project and the Traffic section's involvement/intentions.

Safety Analysis

In the past 10 years there have been 37 recorded crashes within the project limits, a summary of crash types and injury severity are shown in the table below. Leading crash causes included: *Almost all of the crashes were due to failure to yield ROW.*

Crash Severity	
Fatal	1
Inj A	3
Inj B	15
Inj C	6
No<5	0
PDO	12

Crash Types	
Fixed Object	6
Sideswipe-Meeting	1
Rear-End	1
Non Collision	0
Other	0
Angle	19
Turning Movement	10
Head-On	0
Parking Maneuver	0
Pedestrian	0

The majority of these crashes occurred at the Scenic @ OR 99 intersection. The failure to yield ROW cause of the crashes occur when drivers take inadequate gaps in traffic. Crashes were occurred from 5 am to 10 pm with spikes at 8 am and 1pm to 5 pm. These spikes are likely due to peak hour traffic and peak school traffic. Scenic Middle school is very close by which leads to peak queuing in the morning and afternoon. The signal portion of this project will reduce the angle type crashes by 67%. All but one of the Injury A crashes are angle type and the fatal is angle type, so the installation of the signal should reduce the severity of crashes as well. The road diet will help reduce all crashes on this section of roadway by about 29%. Considering the improvements being installed this project should help reduce the number of crashes on this section of roadway.

Traffic Operations Analysis

Existing year traffic operates at a Level of Service (LOS) A on the highway straight away sections and LOS F and a V/C of 1.42 for the Scenic Ave intersection during the peak hours. The LOS F for the Scenic Ave intersection is due traffic peaking on Scenic Ave during the AM, School and PM peak hours. Traffic has a difficult time existing Scenic onto OR 99 due to the low number of gaps, high speed of the highway and number of lanes. This problem has led to the inclusion of a traffic signal in this project. An analysis of the intersection shows that a traffic signal is not warranted with the existing 5 lane section. Due to the road diet being included in the project the warrant analysis can be completed using the proposed three lane section. A traffic signal is warranted with the proposed three lane section. With a traffic signal installed at the intersection of OR 99 and Scenic Ave the LOS improves to B and with a V/C ratio of 0.70. The LOS of the straight away section is reduced from LOS A to B with the road diet. Future year (2040) traffic is expected to operate at a LOS B with a B/C ratio of 0.77

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

at the intersection of Scenic Ave and OR 99. The addition of the traffic signal will reduce the LOS and V/C ratio for both existing and future conditions at the intersection of Scenic Ave and OR 99. The existing straight away section will have slightly lowered LOS with a slightly higher V/C but still within acceptable 20 year design life. Table 1 below provides the V/C, LOS, and 95th queue length for build, no build conditions and provides the same data for the future year (2040) scenario.

Table 1						
No Build						
Direction	2020			2040		
	V/C	LOS	95th Queuing	V/C	LOS	95th Queuing
WBL	1.54	F	580	4.8	F	840
WBT						
WBR						
EBL	0.77	F	100	1.64	F	170
EBT						
EBR						
NBL	0.06	A	10	0.08	A	20
NBT	~		~	~		~
NBR	~		~	~		~
SBL	0.08	A	10	0.11	A	20
SBT	~		~	~		~
SBR	~		~	~		~
Road Diet w/Signal						
Direction	2020			2040		
	V/C	LOS	95th Queuing	V/C	LOS	95th Queuing
WBL	0.55	B	80	0.66	B	210
WBT						
WBR						
EBL	0.26	B	45	0.31	B	100
EBT						
EBR						
NBL	0.14	B	10	0.19	B	35
NBT	0.70		100	0.77		160
NBR	0.27		40	0.30		45
SBL	0.21	B	30	0.30	B	40
SBT	0.50		90	0.56		120
SBR	0.05		25	0.06		25

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

Traffic Control

Traffic volumes allow for the roadway to be reduced to a single lane with alternating bidirectional traffic during the day, recommended lane restrictions are shown below.

Recommended Lane Restrictions – 00220.40(e)(1)

1. Closed Lanes – One traffic lane may be closed in each direction when allowed, shown, or directed during the following periods of time except as indicated in 00220.40(e-2).

- 7:00 p.m. Sunday through 3:00 p.m. Friday

Estimated Delay:

- *<5 minutes of average additional travel time is expected when following the closure recommendations.*

Conclusion

There are safety and capacity concerns at the intersection of OR 99 and Scenic Ave. Crash data revealed there are many angle and turning crashes that are occurring on this section of highway. The addition of a traffic signal and road diet will reduce the number of those types of crashes. The side street traffic entering the highway from Scenic Ave has a difficult time finding a gap. A traffic signal will allow safe gaps in highway traffic to allow traffic to exit Scenic Ave. The mainline traffic on OR 99 will not have queuing, LOS or capacity concerns with the addition of a traffic signal. With the addition of the road diet work may occur during day hours, weekly.

ATTACHMENT "E"

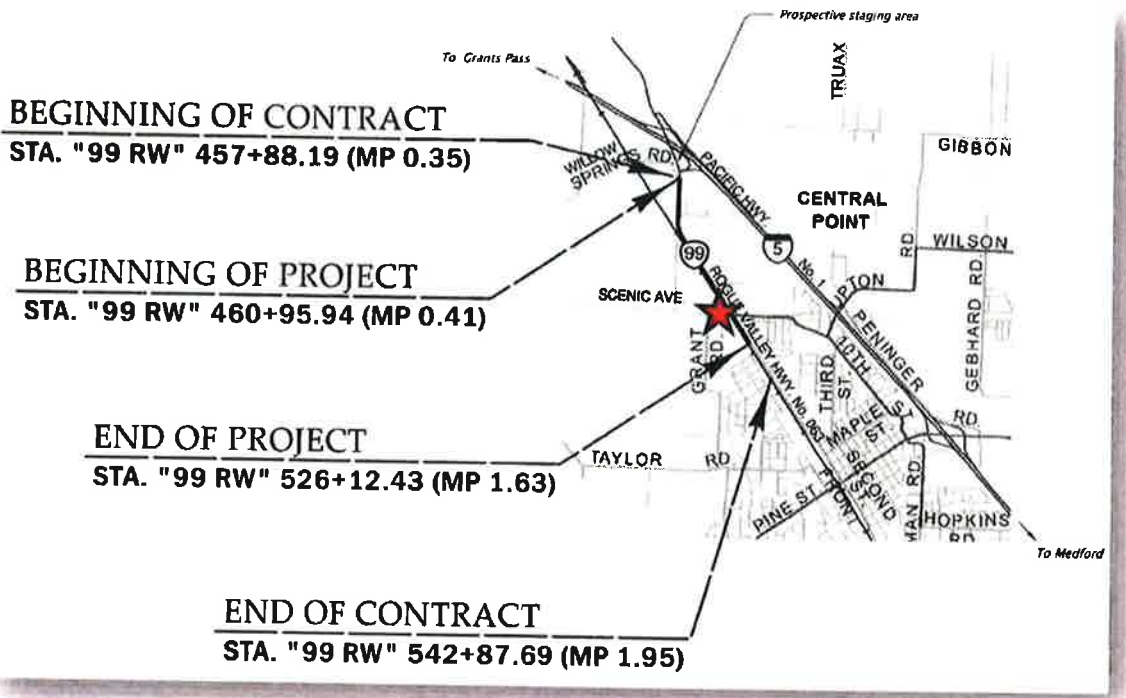


Findings of Fact & Conclusions of Law
 Major Comprehensive Plan Amendment: TSP Project No. 230
 Applicant: City of Central Point
 File No. CPA-20001

I. Introduction

The City of Central Point ("City") is proposing a Major Revision to the Transportation System Plan (TSP), an Element of the Comprehensive Plan ("TSP Amendment"). The proposed revision adds road diet improvements to Highway 99 as part of Project No. 230. As currently written, Project No 230 is to install a traffic signal at the Scenic Avenue intersection with Highway 99 when warrants are met.

Figure 1 - Project Location



The purpose of this project is to meet signal warrants as needed to install a traffic signal at the intersection of Scenic Avenue and Highway 99 to reduce the number and severity of traffic accidents at that location. The road diet, including tapering improvements, will extend from the Interstate 5 Exit 35 (Mile Post (MP) 0.35 to just south of Cascade Drive in the city limits (MP

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

1.95). Most of the project is along portions of Highway 99 that are owned and operated by the Oregon Department of Transportation (ODOT).

The proposed TSP Amendment is 1.6 miles in length and crosses jurisdictional boundaries. Based on the scope of the project and the potential impacts beyond the Scenic Avenue intersection, it is considered a Major Revision in accordance with CPMC 17.96.300. The TSP Amendment has been processed using Type IV (Legislative) procedures set forth in CPMC 17.05.500.

These Findings of Fact and Conclusions of Law are presented in six (6) parts as follows:

1. Introduction
2. Statewide Planning Goals
3. Transportation Planning Rule
4. Central Point Comprehensive Plan
5. CPMC 17.96.500
6. Summary Conclusion

II. Statewide Planning Goals

Major Comprehensive Plan Amendments must address compliance with the Statewide Planning Goals. Due to Central Point's geographic location outside the Willamette Valley and coastal areas, Goals 15-19 do not apply and are not addressed further. Findings relative to the TSP Amendment are set forth below.

Goal 1 – Citizen Involvement

To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

Finding, Goal 1: The proposed TSP Amendment was developed and processed in accordance with the City's citizen involvement program. The Citizen's Advisory Committee (CAC) considered a possible TSP Amendment on August 11, 2020 early in the planning process. At that time, the CAC unanimously forwarded a recommendation to the Planning Commission to consider a TSP Amendment necessary to improve safety along this corridor and at the Scenic Avenue and Highway 99 intersection. On October 5, 2021, staff presented a proposed TSP Amendment to the Planning Commission at which time the Planning Commission voted to direct staff to schedule a public hearing. A duly noticed legislative hearing was held on January 11, 2022 and January 27, 2022. Notice was published in the Mail Tribune and mailed to property owners within 250-feet of the intersection.

Conclusion, Goal 1: Consistent.

Goal 2 – Land Use

To establish a land use planning process and policy framework as a basis for all decision and actions related to use of land and to assure an adequate factual basis for such decisions and actions.

Finding, Goal 2: The TSP amendment follows the land use review procedures outlined in Central Point Municipal Code and these findings provide an adequate factual basis for action. The TSP amendment therefore conforms to the established land use planning process and framework consistent with Goal 2.

Conclusion, Goal 2: Consistent.

Goal 3 – Agricultural Lands

To preserve and maintain agricultural lands.

Finding, Goal 3: The proposed TSP amendment does not involve or otherwise affect lands designated for agricultural use.

Conclusion, Goal 3: Not applicable.

Goal 4 – Forest Lands

To conserve forest lands by maintaining the forest land base and to protect the state's forest economy by making possible economically efficient forest practices that assure the continuous growing and harvesting of forest tree species as the leading use on forest land consistent with sound management of soil, air, water, and fish and wildlife resources and to provide for recreational opportunities and agriculture.

Finding, Goal 4: The proposed TSP amendment does not involve or otherwise affect lands designated for forest use.

Conclusion, Goal 4: Not applicable.

Goal 5 – Open Space, Scenic and Historic Areas, and Natural Resources

To protect natural resources and conserve scenic and historic areas and open spaces.

Finding, Goal 5: Public road projects are required to comply with provisions under the Endangered Species Act (ESA) for aquatic species that exceed the Goal 5 and associated Central Point Municipal Code requirements for riparian protection. As such, by complying with the ESA, such public road projects also will adequately address riparian protections otherwise required by regulations applicable to riparian area development. The treatment of other resources regulated under Goal 5 will not change because of the TSP amendment, and therefore the goal is otherwise not relevant to this transportation system plan amendment.

Conclusion, Goal 5: Consistent.

Goal 6 – Air, Water and Land Resources Quality

To maintain and improve the quality of the air, water and land resources of the state.

Finding, Goal 6: The proposed TSP amendment does not involve or otherwise affect local, state, or federal regulations managing the quality of air, water and land resources.

Conclusion, Goal 6: Not applicable.

Goal 7 – Areas Subject to natural Hazards and Disasters.

To protect people and property from natural hazards.

Finding, Goal 7: The proposed TSP amendment does not involve or otherwise affect regulations protecting the citizens of Central Point from natural hazards.

Conclusion, Goal 7: Not applicable.

Goal 8 – Recreation Needs

To satisfy the recreational needs of the citizens of the state and visitors and, where appropriate, to provide for the siting of necessary recreational facilities.

Finding, Goal 8: The proposed TSP amendment does not involve or otherwise affect the City's provision of necessary recreational facilities.

Conclusion, Goal 8: Not applicable.

Goal 9 – Economy of the State

To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare and prosperity of Oregon's citizens.

Finding, Goal 9: The proposed TSP amendment will add a lane reconfiguration project to an already listed signal project and will provide for the continued orderly development of the City's road network, which is a vital infrastructure component necessary to support continued economic development.

Conclusion, Goal 9: Consistent.

Goal 10 – Housing

To provide for the housing needs of citizens of the state.

Finding, Goal 10: The proposed TSP amendment does not involve or otherwise affect the City's ability to provide needed housing to the citizens of Central Point or the state of Oregon.

Conclusion, Goal 10: Not applicable.

Goal 11 – Public Facilities and Services

To plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development.

Finding, Goal 11: Transportation facilities are identified as public facilities under this goal. It is acknowledged by the TSP, by reference, is part of the City's Comprehensive Plan Public Facilities Element. The addition of the lane reconfiguration project allows for orderly and timely provision of needed transportation facilities to provide for safe and reliable transportation along Highway 99 and in particular at the intersection with Scenic Avenue.

Conclusion, Goal 11: Consistent.

Goal 12 – Transportation

To provide and encourage a safe, convenient and economic transportation system.

Finding, Goal 12: Transportation, directs state and local jurisdictions "to provide and encourage a safe, convenient and economic transportation system." It establishes that a transportation plan consider all modes of transportation, be based upon an inventory of local, regional and state transportation needs, consider the differences in social consequences that would result from utilizing differing combinations of transportation

modes, avoid principal reliance upon any one mode of transportation, minimize adverse social, economic and environmental impacts and costs, conserve energy, meet the needs of the transportation disadvantaged by improving transportation services, facilitate the flow of goods and services so as to strengthen the local and regional economy, and conform with local and regional comprehensive land use plans.

The City's TSP (2008) currently lists the signal at OR 99 and Scenic Avenue as Project No. 230, when signal warrants are met. The amendment to the TSP is to include the road diet from MP .42 to MP 1.64. The Traffic Report indicated that the traffic signal is not warranted with the existing 5-lane section of roadway. However, with the road diet a traffic signal is warranted. The functional classification of OR 99 through this section remains as an Urban Principal Arterial. ODOT completed a Traffic Report that supports the road diet. The Traffic Report concluded that the addition of a traffic signal and road diet would reduce the number of angle and turning crashes that are occurring on this section of OR 99. The Report concluded that the mainline traffic on OR 99 would not have queuing (LOS or capacity) concerns with the addition of the traffic signal or the road diet.

Conclusion, Goal 12: Consistent.

Goal 13 – Energy

To conserve energy.

Finding, Goal 13: The proposed TSP amendment does not involve, or otherwise affect development standards or regulations that address conservation of energy.

Conclusion, Goal 13: Not applicable.

Goal 14 – Urbanization

To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

Finding, Goal 13: The proposed TSP amendment addresses the multi-modal transportation needs for a project that is already listed in the TSP. The proposed project will address transportation for all modes by addressing the lack of bicycle lanes in the urban area.

Conclusion, Goal 13: Consistent.

III. Transportation Planning Rule (OAR 660-012)

The purpose of the Transportation Planning Rule (TPR) is to facilitate implementation of Statewide Planning Goal 12 (Transportation). It sets forth the requirements for preparation of local TSPs.

Finding OAR 660-012: The City's adopted TSP (Ordinance No. 1922) is based on and complies with the TPR. The proposed amendment is limited to the addition of a lane reconfiguration improvements as part of Project No. 230, which includes installing a signal at the intersection of Highway 99 and Scenic Avenue when warrants are met. The proposed amendment is in response to a Traffic Report that found significant safety concerns at the intersection of Highway 99 and Scenic Avenue and a finding that the signal installation is not warranted with the existing five (5) lane configuration of Highway 99. The proposed addition of a

road diet will allow signal warrants to be met as needed to address a project identified in the TSP. The Traffic Report found that the impacted transportation facilities will not experience any change in functional classification. The proposed amendment will improve intersection performance and reduce angle and turning movement collisions that have resulted in 1 fatality and several injuries in the past ten (10) years. Finally the proposed revision implements Project KN 20185 in the State Transportation Improvement Plan (STIP) and is consistent with the Regional Transportation Plan and Jackson County Transportation Plan. As such, the City finds that there are no changes the TSP's compliance with OAR 660-012 and that no further discussion of TPR compliance is necessary.

Conclusion: OAR 660-012. The proposed TSP Amendment adding the road diet to Project No. 230 does not alter the TSPs compliance with OAR 660-012 and the proposed amendment is based on technical data that resolves a safety issue and allows an existing TSP project (230, intersection signal installation) to be implemented.

IV. Central Point Comprehensive Plan

The City's Comprehensive Plan addresses compliance with Statewide Planning Goals and establishes the goals and polices to carry out the City's mission and vision as articulated in the City's Strategic Plan. The adopted TSP (Ordinance No. 1922) was prepared based on the land use classification and distribution in the City's Land Use Element of the Comprehensive Plan. The proposed amendment addresses transportation facilities in the TSP. Applicable policies are limited to 5.1.1 and 7.1.11 in the TSP, which is part of the Public Facilities Element. These are addressed below:

TSP Policy 5.1.1

The City shall make every effort to maintain mobility standards that result in a minimal level of service (LOS) "D." The City defines LOS D as the equivalent to a volume to capacity ratio of 0.9.

Finding, TSP Policy 5.1.1: According the Traffic Report prepared by the Oregon Department of Transportation (ODOT) for STIP Project No. KN 20185, the Scenic Avenue intersection with Highway 99 operates at a LOS F and a volume to capacity ratio of 1.42 during peak hours. The report states that these conditions are due to school related traffic and this has caused a signal to be included in the STIP Project KN 20185 as well as the City of Central Point Project No. 230 in the adopted TSP. The traffic signal must meet warrants to be installed. Per the traffic report this occurs when the Highway 99 street section is reconfigured to three (3) lanes. Analysis shows that implementation of the revised project improves the intersection operation to a LOS B and a volume to capacity ration of 0.70, which brings this facility into compliance with the City's mobility standards. Without the road reconfiguration, the intersection would continue to operate at an unacceptable LOS and volume to capacity ratio.

Conclusion, TSP Policy 5.1.1: The proposed amendment is necessary to install a traffic signal and meet the City's mobility standards at the intersection of Scenic Avenue and Highway 99 consistent with this policy.

TSP Policy 7.1.11

The City shall place a higher priority on funding and constructing street projects that address identified vehicular, bicycle and pedestrian safety problems than those projects that solely respond to automotive capacity deficiencies in the street system. Exceptions are those capacity improvements that are designed to also resolve identified safety problems.

Finding, TSP Policy 7.1.11: Based on the ODOT Traffic Report, there have been 37 crashes recorded within the project limits during the past 10 year period. The proposed revision adding a road diet is in response to the safety concerns. By adding the road diet, signal warrants can be met as needed to reduce the number and severity of collisions at this intersection. Funding has been obtained by ODOT through the All Road Transportation Safety (ARTS) Program to implement the project. The City finds that this TSP Amendment is consistent with prioritizing projects that address safety concerns.

Conclusion, TSP Policy 7.1.11: Consistent.

V. CPMC 17.96, Comprehensive Plan and Urban Growth Boundary Amendments.

This chapter establishes the procedures and approval criteria for amending the Comprehensive Plan. The type of amendment and procedures applied are addressed in Section I of these findings. The approval criteria set forth in CPMC 17.96.500 are addressed below:

CPMC 17.96.500(A)

Approval of the request is consistent with the applicable statewide planning goals;

Finding, CPMC 17.96.500(A): See Section II Findings.

Conclusion, CPMC 17.96.500(A): Consistent.

CPMC 17.96.500(B)

Approval of the request is consistent with the Central Point comprehensive plan;

Finding, CPMC 17.96.500(1): See Section IV Findings.

Conclusion, CPMC 17.96.500(1): Consistent.

CPMC 17.96.500(C)

For urban growth boundary amendments findings demonstrate that adequate public services and transportation networks to serve the property are either available, or identified for construction in the city's public facilities master plans (major and minor amendments); and

Finding, CPMC 17.96.500(C): The proposed TSP Amendment does not involve a UGB Amendment.

Conclusion, CPMC 17.96.500(1): Not applicable.

CPMC 17.96.500(D)

The amendment complies with OAR 660-012-0060 of the Transportation Planning Rule.

Finding, CPMC 17.96.500(1): See Section III Findings.

Conclusion, CPMC 17.96.500(1): Consistent.

VI. Summary Conclusion

The TSP Amendment as presented in Exhibit 1 is consistent with all applicable approval criteria as demonstrated herein.

EXHIBIT 1 - Transportation System Plan Revisions

Table 7.4, Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Year	ODOT	County	Central Point	Medford	Other
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	•	•	•					√			√	2012	♦	♦	♦		

Table 12.5, Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Tier	ODOT	County	Central Point	Medford	Other	Total Project Cost
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	■	■	■					√			√	Tier 1, Long	♦	♦	♦			\$2,737,300



City of Central Point **Staff Report to Council**

ISSUE SUMMARY

TO:	City Council	DEPARTMENT: Public Works
FROM:	Matt Samitore, Parks and Public Works Director	
MEETING DATE:	February 17, 2022	
SUBJECT:	Resolution No. _____, Accepting the Lowest Responsible Bid from Central Pipeline Inc. for the Gebhard Road Storm Drain Project and Authorizing the City Manager to Execute a Contract	
ACTION REQUIRED: Resolution		RECOMMENDATION: Approval

BACKGROUND INFORMATION:

The City conducted a bid letting procedure for the Gebhard Road Storm Drain project. This joint project will provide drainage for the remaining undeveloped areas of East Central Point as well as future growth areas as part of the current Urban Reserve. The project will include the installation of 1800 feet of storm drain from Gebhard Road to Bear Creek through land currently owned by Jackson County and managed by the Expo.

The engineer's estimate for the project was \$521,000. The City received six qualified bids for the project. The lowest responsible bidder for the project is from Central Pipeline Inc. for \$488,515.00. The project will be paid for from the storm drain and street funds.

FINANCIAL ANALYSIS:

The project was initially budgeted in the 2021/23 Storm Drain Fund for \$488,515. The additional monies will be funded by the Street Fund for the work occurring within the limits of Gebhard Road.

LEGAL ANALYSIS:

The City of Central Point public contracts/bidding is governed by Oregon Revised Statute (ORS) Chapter 279 and Central Point Municipal Code (CPMC) Chapter 2.40.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

Community Investment; Goal 5 – Plan, design, and construct modern and efficient infrastructure in all areas and systems. Strategies 3 and 4.

STAFF RECOMMENDATION:

Approve a resolution awarding the Gebhard Road Stormdrain project to Central Pipeline, Inc.

RECOMMENDED MOTION:

I move to approve Resolution No. ____ accepting the lowest responsible bid from Central Pipeline Inc. for \$488,515 for the Gebhard Road Storm Drain project and authorizing the City Manager to execute a contract.

ATTACHMENTS:

1. Gebhard Rd SD Outfall - Engineers Estimate_20210803
2. gebhard road sd bid ad
3. gebhard road sd project overview
4. resol_gebhard road sd

RH2		ENGINEER'S OPINION OF PROBABLE COST																		
Gebhard Road Stormwater Outfall		AGENCY City of Central Point		Probable Cost		Central Pipeline		Pilot Rock		Ledford		Jesse Rodriguez		Knife River Materials		JRT		Upper Rogue		
KIND OF WORK Storm Drainage Improvements		DATE 2/10/22																		
ITEM #	SPEC #	ITEM DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL
200 TEMPORARY FEATURES AND APPURTENANCES																				
10	210	MOBILIZATION	LS	1	\$60,000.00	\$60,000.00	\$41,218.00	\$41,218.00	\$12,000.00	\$12,000.00	\$25,000.00	\$25,000.00	\$55,000.00	\$55,000.00	\$13,450.00	\$13,450.00	\$75,000.00	\$75,000.00	\$40,000.00	\$40,000.00
20	225	TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE	LS	1	\$1,500.00	\$1,500.00	\$1,225.00	\$1,225.00	\$1,500.00	\$1,500.00	\$2,000.00	\$2,000.00	\$4,567.00	\$4,567.00	\$2,000.00	\$2,000.00	\$5,000.00	\$5,000.00	\$12,000.00	\$12,000.00
30	280	EROSION AND SEDIMENT CONTROL	LS	1	\$2,500.00	\$2,500.00	\$7,000.00	\$7,000.00	\$6,500.00	\$6,500.00	\$7,300.00	\$7,300.00	\$3,000.00	\$3,000.00	\$6,300.00	\$6,300.00	\$7,000.00	\$7,000.00	\$18,000.00	\$18,000.00
300 ROADWORK																				
40	305	CONSTRUCTION SURVEY WORK	LS	1	\$2,500.00	\$2,500.00	\$3,500.00	\$3,500.00	\$1,700.00	\$1,700.00	\$2,000.00	\$2,000.00	\$5,975.00	\$5,975.00	\$1,620.00	\$1,620.00	\$2,000.00	\$2,000.00	\$10,000.00	\$10,000.00
50	330	GENERAL EXCAVATION	CY	160	\$30.00	\$4,800.00	\$18.00	\$2,880.00	\$25.00	\$4,000.00	\$15.00	\$2,400.00	\$27.00	\$4,320.00	\$26.00	\$4,160.00	\$51.00	\$8,160.00	\$60.00	\$9,600.00
400 DRAINAGE AND SEWERS																				
60	445	48 INCH CONC PIPE	LF	1118	\$350.00	\$391,300.00	\$335.00	\$374,530.00	\$360.00	\$402,480.00	\$350.00	\$391,300.00	\$441.00	\$493,038.00	\$436.00	\$487,448.00	\$502.00	\$561,236.00	\$360.00	\$402,480.00
70	445	SLOPED END SECTIONS, 48 INCH	EA	1	\$1,000.00	\$1,000.00	\$1,100.00	\$1,100.00	\$1,500.00	\$1,500.00	\$1,000.00	\$1,000.00	\$2,500.00	\$2,500.00	\$1,700.00	\$1,700.00	\$700.00	\$700.00	\$16,000.00	\$16,000.00
80	460	PAVED CULVERT END SLOPES	SF	98	\$45.00	\$4,410.00	\$15.00	\$1,470.00	\$40.00	\$3,920.00	\$70.00	\$6,860.00	\$42.00	\$4,116.00	\$46.00	\$4,508.00	\$50.00	\$4,900.00	\$87.00	\$8,526.00
90	470	CONCRETE STORM SEWER MANHOLES	EA	4	\$12,000.00	\$48,000.00	\$12,478.00	\$49,912.00	\$15,500.00	\$62,000.00	\$13,000.00	\$52,000.00	\$15,575.00	\$62,300.00	\$18,000.00	\$72,000.00	\$21,700.00	\$86,800.00	\$29,500.00	\$118,000.00
1000 RIGHT OF WAY DEVELOPMENT AND CONTROL																				
100	1040	PLANT MATERIALS - WILLOW PLUGS	EACH	475.00	\$10.00	\$4,750.00	\$12.80	\$6,080.00	\$8.00	\$3,800.00	\$14.00	\$6,650.00	\$9.00	\$4,275.00	\$8.00	\$3,800.00	\$10.00	\$4,750.00	\$23.00	\$10,925.00
SUBTOTAL:					\$521,000.00		\$488,915.00		\$499,400.00		\$496,510.00		\$639,091.00		\$596,986.00		\$755,546.00		\$645,531.00	
PROJECT TOTAL					\$521,000.00		\$488,915.00		\$499,400.00		\$496,510.00		\$639,091.00		\$596,986.00		\$755,546.00		\$645,531.00	

Attachment: Gebhard Rd SD Outfall - Engineers Estimate_20210803 (1510 : Gebhard Road Storm Drain - Bid Opening)

AFFIDAVIT OF PUBLICATION

**** THIS IS NOT A BILL ****

On Behalf of:
CITY OF CENTRAL POINT-Legal
140 S THIRD ST
CENTRAL POINT, OR 97502

STATE OF OREGON COUNTY OF JACKSON:

I, Kristy Gallon, being first duly sworn, depose and say that I am the principal clerk of Mail Tribune, a newspaper of general circulation, as defined by ORS 193.010 and 193.020; printed at Medford in the aforesaid county and state; that the public notice, a printed copy of which is hereto annexed, was published in the entire issue of said newspaper for 2 successive and consecutive insertion(s) in the following issues 1/9/2022, 1/16/2022 (HERE SET FORTH DATES OF ISSUE).

Kristy Gallon
Affiant

Subscribed and sworn to before me this
3rd day of Feb., 2022.

Terrie Rogers
Notary State of Oregon County of Jackson

My commission expires Sept 24th, 2022



Rosebud Media - Mail Tribune
111 N Fir St, Medford, OR 97501

Publication Cost: \$388.74 | Department of Public Works
Ad No: 4964 Customer No: 100867

COPY OF ADVERTISEMENT

Department of Public Works
ADVERTISEMENT FOR BIDS

NOTICE TO CONTRACTORS
PUBLIC IMPROVEMENT PROJECT:
Gebhard Road Stormwater Outfall
PROJECT #8097070

Sealed bids will be opened and publicly read at the City of Central Point, City Hall, 140 S. 3rd Street, Central Point, OR 97502 at 2:00 PDT on Thursday, February 8, 2022 for the above referenced project. Bids must be submitted to Matt Samlors, Parks and Public Works Director, at the same address prior to 2:00 PDT on the above date. Subcontractor Disclosure forms must be submitted prior to 4:00 PDT on the same date.

Bidders must be prequalified in order to be eligible for award. Pre-qualification may be with the City of Central Point, City of Medford or the Oregon Department of Transportation. If the bidder is disqualified in any of these jurisdictions, the bidder will be ineligible for this contract.

Award of contract will not be final until the later of: 1) three business days after the City of Central Point announces Notice of Intent to Award, or 2) the City of Central Point provides a written response to each timely protest, denying the protest and affirming the award.

On all projects, work performed by the Contractor's own organization must be at least 30% of the awarded contract amount.

Plans and specifications will be available online only www.questodr.com beginning January 16th, 2022. Any addenda issued will be posted on the above websites.

COST ESTIMATE: \$300,000 - \$425,000

This project consists of storm drainage improvements from the east side of Gebhard Road to Bear Creek. This is a City funded project.

Work shall begin no earlier than February 22, 2022 and must be completed no later than July 15th, 2022. Please direct all questions to Greg Graves at 541-664-3321 (x225) or greg.graves@centralpoint.gov. The deadline for questions is February 3, 2022.

No bid will be received or considered by the City of Central Point unless bidder signs the bid statement.

The contract is for public work subject to ORS 279C.800 to 279.870. This project is subject to Oregon prevailing wage rates.

The City of Central Point may reject any bid not in compliance with all public bidding procedures and requirements and may reject for good cause any or all bids upon a finding by the City of Central Point that it is in the public interest to do so.

CITY OF CENTRAL POINT

Matt Samlors, Parks and Public Works Director

PUBLISHED:
Mail Tribune
PUBLISH DATE:
January 9th, 2022 and January 16, 2022

January 9 and 16, 2022

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

INVOICE

DJC Oregon

a division of BridgeTower Media
 PO Box 745929
 Atlanta, GA 30374-5929
 1 (612) 333-4244

City of Central Point
Cyndi Weeks
 140 S 3rd St
 Central Point, OR 97502-2216

Account #	8.B.b
10032589	
Invoice Date	1/14/2022
Invoice #	745289209
Order #	12066906
Terms	NET 30
PO/Case #	
Salesrep	

Days/Inserts	Description	Size/Qty	Unit Price	Amount
2	<p align="center">GEBHARD ROAD STORMWATER OUTFALL</p> <hr/> <p>Daily Journal of Commerce (OR) Bids - Other / Construction City of Central Point; Bid Location Central Point, OR, Jackson County; Due 02/08/2022 at 02:00 PM 01/07/2022, 1/14 -Base Charge</p> <p align="center"><i>\$670.50</i></p>	1 col x 8.10in 416 wrd / 76 ln		311.60
TOTAL DUE				311.60

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

Acceptable Payment Methods:

To Pay by Check:
 BridgeTower OpCo, LLC
 PO Box 745929
 Atlanta, GA 30374-5929

To Pay by ACH:
 Bank: Bank of America
 Contact Linda Burnette
 Account Number: 237025443017
 Routing Number: 053000196

To Pay by Credit Card:
 Contact Accounts Receivable:
 866-802-8214
 Please have your Invoice Number
 and Credit Card Number Ready

AFFIDAVIT OF PUBLICATION



DJCOREGON

11 NE Martin Luther King Jr. Blvd. Suite 201 / Portland, OR 97232-3579
(503) 226-1311

STATE OF OREGON, COUNTY OF MULTNOMAH--ss.

I, **Nick Bjork**, being first duly sworn, depose and say that I am a **Publisher** of the **Daily Journal of Commerce**, a newspaper of general circulation in the counties of CLACKAMAS, MULTNOMAH, and WASHINGTON as defined by ORS 193.010 and 193.020; published at Portland in the aforesaid County and State; that I know from my personal knowledge that the Construction notice described as

Case Number: NOT PROVIDED
GEBHARD ROAD STORMWATER OUTFALL
City of Central Point; Bid Location Central Point, OR, Jackson County; Due 02/08/2022 at 02:00 PM

a printed copy of which is hereto annexed, was published in the entire issue of said newspaper for 2 time(s) in the following issues:

1/7/2022

1/14/2022

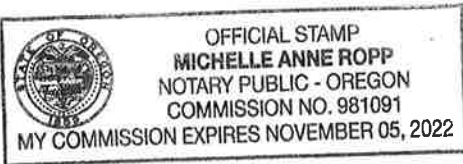
State of Oregon
County of Multnomah

SIGNED OR ATTESTED BEFORE ME
ON THE 14th DAY OF January, 2022

Nick Bjork

**SEE
EXHIBIT A**

Notary Public-State of Oregon



Cyndi Weeks
City of Central Point
140 S 3rd St
Central Point, OR 97502-2216

Order No.: 12066906
Client Reference No:

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

**CITY OF CENTRAL POINT
DEPARTMENT OF PUBLIC
WORKS**

**PUBLIC IMPROVEMENT PROJECT:
GEBHARD ROAD STORMWATER
OUTFALL**

**Bids due 2:00 pm, February 8, 2022
ADVERTISEMENT FOR BIDS
PROJECT #8097070**

Sealed bids will be opened and publicly read at the City of Central Point, City Hall, 140 S. 3rd Street, Central Point, OR 97502 at 2:00 PDT on Thursday, February 8, 2022 for the above referenced project. Bids must be submitted to Matt Samitore, Parks and Public Works Director, at the same address prior to 2:00 PDT on the above date. Subcontractor Disclosure forms must be submitted prior to 4:00 PDT on the same date.

Bidders must be prequalified in order to be eligible for award. Pre-qualification may be with the City of Central Point, City of Medford or the Oregon Department of Transportation. If the bidder is disqualified in any of these jurisdictions, the bidder will be ineligible for this contract.

Award of contract will not be final until the later of: 1) three business days after the City of Central Point announces Notice of Intent to Award, or 2) the City of Central Point provides a written response to each timely protest, denying the protest and affirming the award.

On all projects, work performed by the Contractor's own organization must be at least 30% of the awarded contract amount.

Plans and specifications will be available online only www.questcdn.com beginning January 15th, 2022. Any addenda issued will be posted on the above websites.

COST ESTIMATE: \$300,000 - \$425,000

This project consists of storm drainage improvements from the east side of Gebhard Road to Bear Creek. This is a City funded project.

Work shall begin no earlier than **February 22, 2022** and must be **completed no later than July 15th, 2022**. Please direct all questions to Greg Graves at 541-664-3321 (x225) or greg.graves@centralpoint.gov. The deadline for questions is **February 3, 2022**.

No bid will be received or considered by the City of Central Point unless bidder signs the bid statement.

The contract is for public work subject to ORS 279C.800 to 279.870. This project is subject to Oregon prevailing wage rates.

The City of Central Point may reject any bid not in compliance with all public bidding procedures and requirements and may reject for good cause any or all bids upon a finding by the City of Central Point that it is in the public interest to do so.

CITY OF CENTRAL POINT
Matt Samitore, Parks and
Public Works Director

Published Jan. 7 & 14, 2022.

12066906

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)



CITY OF CENTRAL POINT

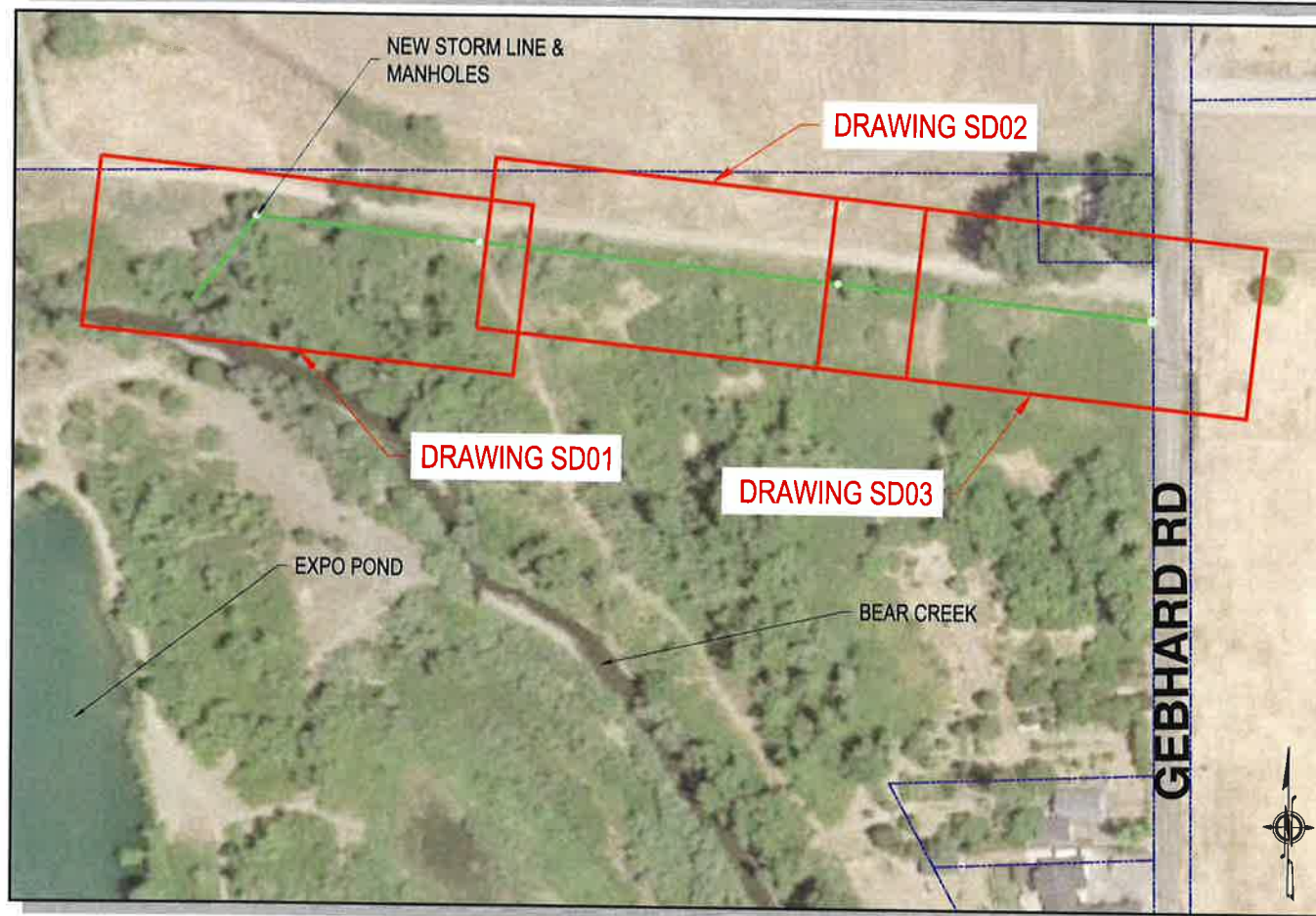
GEBHARD ROAD STORMWATER OUTFALL

SUMMER 2021

PROJECT VICINITY MAP



PROJECT LOCATION MAP



DRAWING INDEX

Sheet Number	Sheet Title	Dwg No
01	COVER	COV
02	GENERAL NOTES	G01
03	STORMWATER PLAN AND PROFILE - SHEET 1 OF 3	SD01
04	STORMWATER PLAN AND PROFILE - SHEET 2 OF 3	SD02
05	STORMWATER PLAN AND PROFILE - SHEET 3 OF 3	SD03
06	STORMWATER DETAILS	SD04

CALL 48 HOURS BEFORE YOU DIG ONE CALL 811

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER AT 503-232-1978

REPORT SPILLS

ATTENTION: OREGON LAW REQUIRES THAT SPILLS BE REPORTED TO THE FOLLOWING ENTITIES:

OREGON EMERGENCY RESPONSE SYSTEM: 1-800-452-0311
THE NATIONAL RESPONSE CENTER: 1-800-424-8802

CONTACT PERSONNEL

CONTACT	AGENCY	PHONE
TYLER DUNCAN, P.E. (PROJECT MANAGER)	RH2 ENGINEERING	(541) 326-4448
GREG GRAVES	CITY OF CENTRAL POINT	(541) 664-3321 x225
NICK BAKKE	RVSS	(541) 665-6300
RYLAN WOOD	PACIFIC POWER	(541) 858-3204

SECTION AND DETAIL REFERENCES

THE FOLLOWING CONVENTIONS HAVE BEEN USED WITHIN THESE DRAWINGS TO REFER THE READER BETWEEN THE SECTION/DETAIL AND THE PLAN FROM WHICH IT IS REFERENCED.

REFERENCE BUBBLES



PLAN REFERENCE BUBBLE - REFERS READER BACK TO THE PLAN FROM WHICH THE DETAIL OR SECTION ORIGINATED.



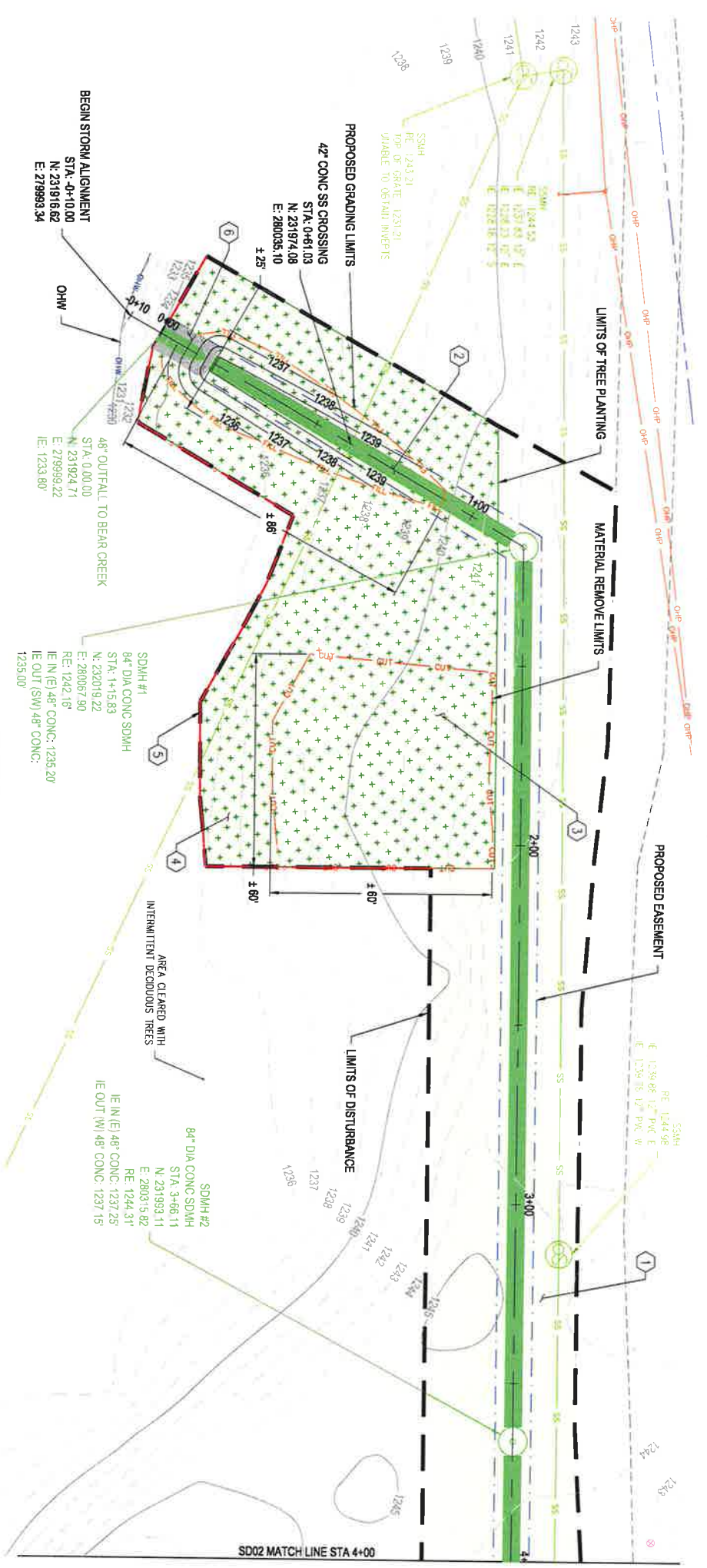
DETAIL/SECTION REFERENCE BUBBLE - REFERS READER TO THE DRAWING ON WHICH THE DETAIL OR SECTION IS LOCATED.

WHERE, ID = SECTION/DETAIL REFERENCE NUMBER
= DRAWING NUMBER ON WHICH DETAIL ORIGINATED OR RESIDES.

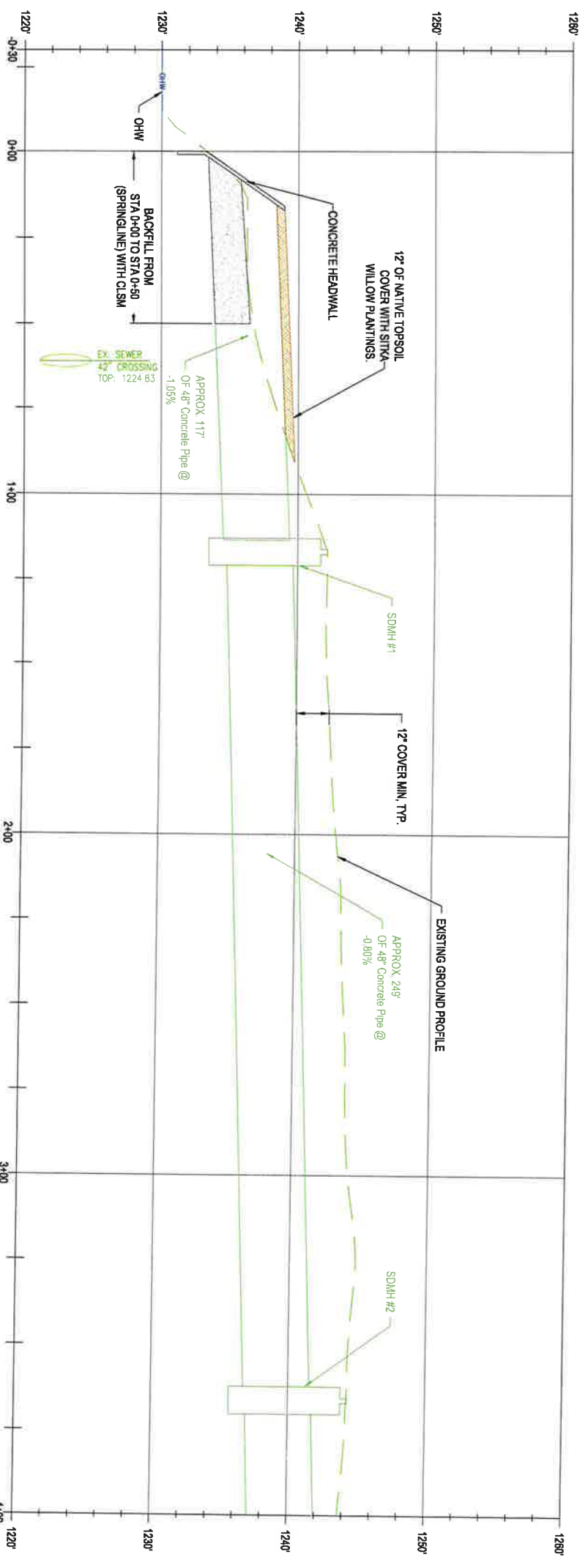
SECTION/DETAIL REFERENCE NUMBER CONVENTIONS:
SECTIONS OR ELEVATIONS SHOULD HAVE A LETTER REFERENCE NUMBER (A THROUGH ZZ).



Attachment: gebhard road sd project overview (1510 : Gebhard Road Storm Drain - Bid Opening)



PLAN VIEW
1" = 20'



STORM PROFILE
H: 1" = 20', V: 1" = 5'

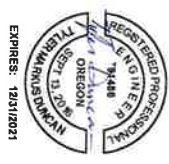
GENERAL NOTES

- 1) LOCATION OF EX UTILITIES AND STRUCTURES ARE FROM INFORMATION AVAILABLE AT THE TIME OF DESIGN. EXACT LOCATION AND COMPLETENESS ARE NOT GUARANTEED. CALL SERVICES ALERT 911 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION AND POT HOLE FOR EXISTING UTILITY LOCATIONS.
- 2) WHEREVER TOPSOIL IS REMOVED FOR TRENCHING OR OTHER EXCAVATIONS, STOCKPILE TOPSOIL SEPARATELY AND REINSTALL STOCKPILED NATIVE TOPSOIL FOR UPPER 12" MINIMUM OF TRENCH BACKFILL AND IN ALL AREAS WHERE BACKFILL OR FILL IS REQUIRED.
- 3) PIPE SLOPES ARE REFERENCING ACTUAL LAYING SLOPE OF PIPE BETWEEN EDGE OF STRUCTURES. PIPE LENGTHS ARE REFERENCED TO CENTER OF STRUCTURES PER PAYMENT LENGTH. REFER TO DETAIL.
- 4) ALL WORK FROM STA 0+00 TO STA 1+1+5 (FROM BEAR CREEK TO SDMH #1) SHALL OCCUR WITHIN THE IN-WATER WORK PERIOD JUNE 15TH THROUGH SEPTEMBER 15TH.

KEY NOTES

- 1) SEED BARE EARTH/DISTURBED AREAS WITH AN APPROVED GRASS SEED MIX.
- 2) PLACE 12" OF NATIVE TOPSOIL OVER TOP OF EXPOSED PIPE, WITHIN AREA, LINES AND GRADES SHOWN, APPROXIMATELY 80 CY FILL. PLACE WITH A MAX 3H:1V SLOPE.
- 3) REMOVE AND STOCKPILE TOPSOIL TO 12" IN DEPTH. EXCAVATE MATERIAL, WITHIN AREA SHOWN, TO REMOVE APPROXIMATELY 80 CY AND DISPOSE OF EXCESS EXCAVATED MATERIAL OFFSITE. PLACE STOCKPILED NATIVE SOIL OVER PIPE AREA TO THE LINES, AND GRADES SHOWN.
- 4) PLANT SITKA WILLOW (SALIX SITCHENSIS) WITHIN AREA SHOWN (APPROX 440 ST), PLUGS AT 5' MIN SPACING (APPROXIMATELY 475 PLUGS), TO LIMITS SHOWN.
- 5) INSTALL TEMPORARY SILT FENCE FOR EROSION CONTROL.
- 6) PAVED END SLOPE, REFER TO DETAIL.

VICINITY MAP

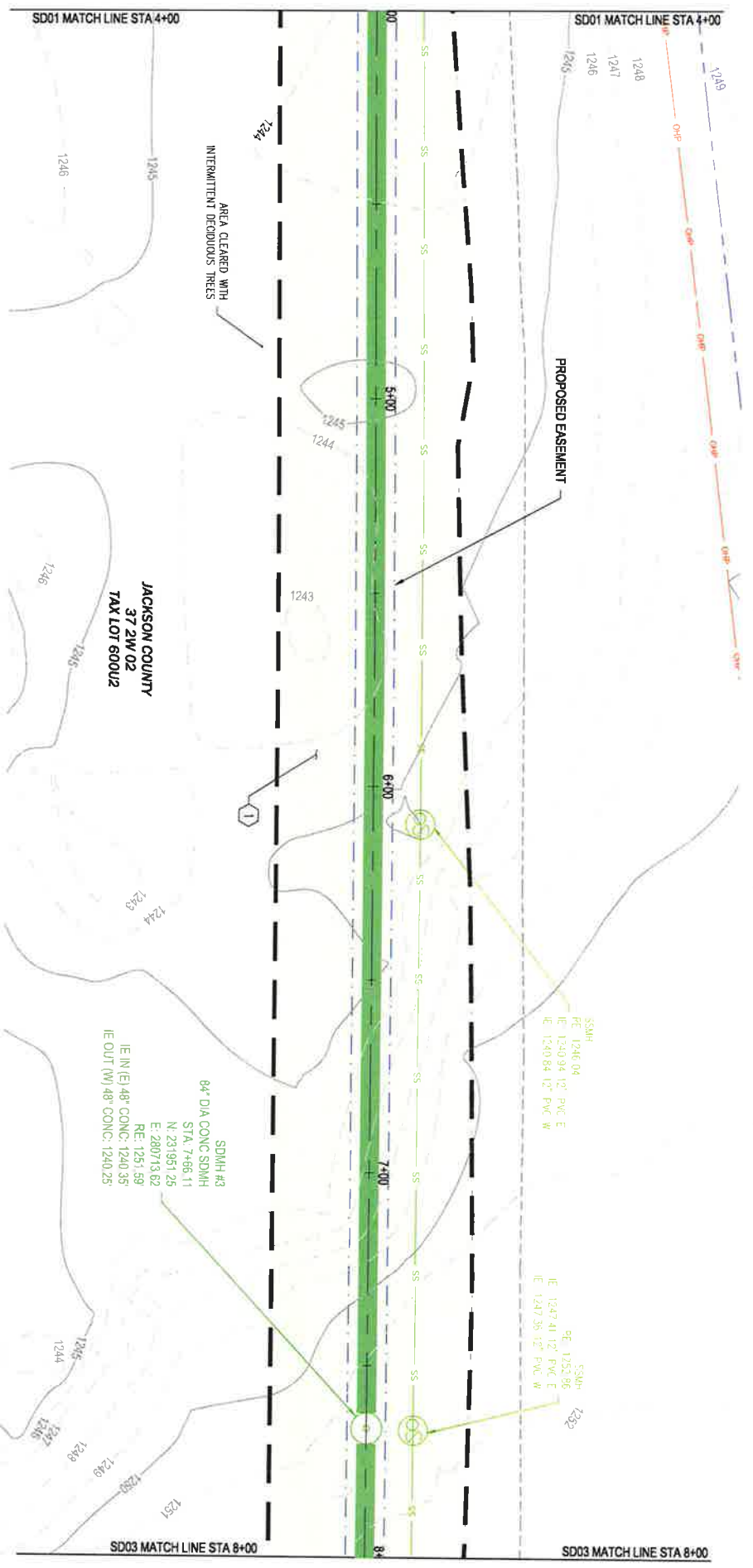


CITY OF CENTRAL POINT
GEBHARD ROAD STORMWATER OUTFALL
STORMWATER PLAN AND PROFILE -
SHEET 1 OF 3

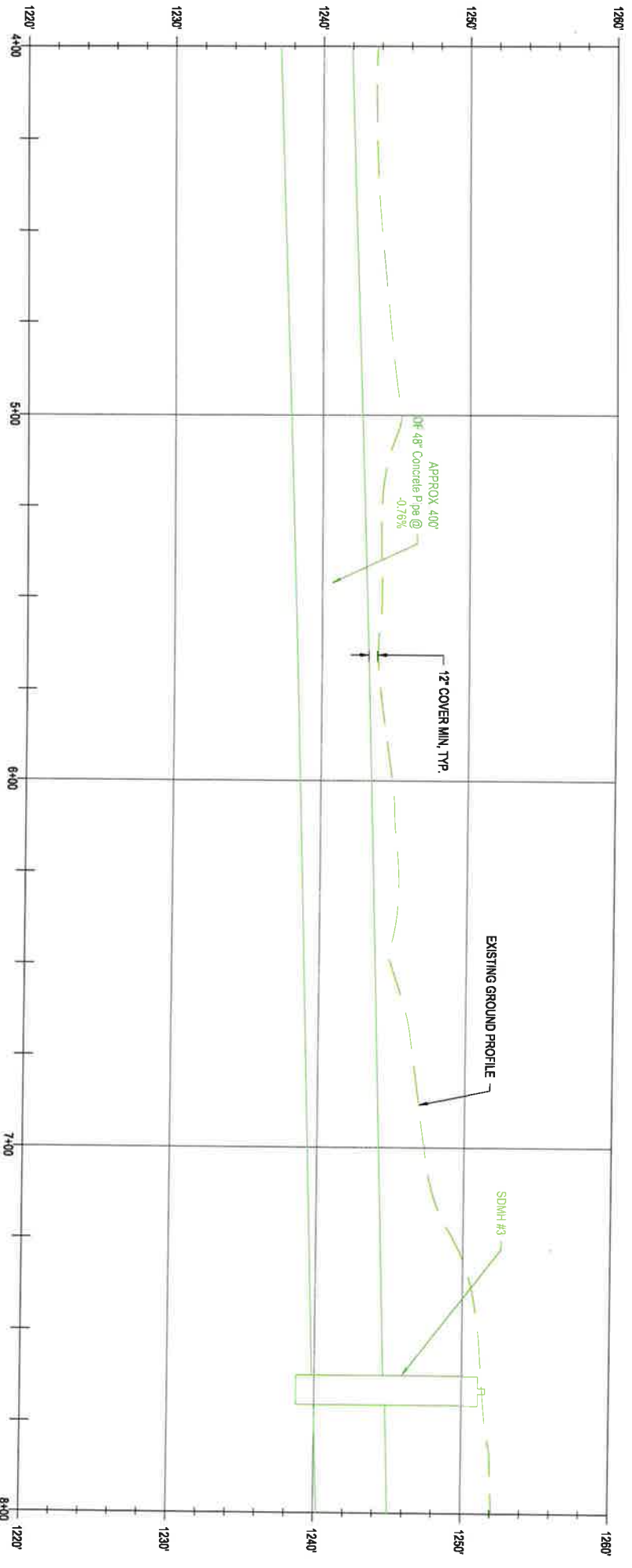
NO.	DATE	DESCRIPTION	BY	REVIEW

ENGINEER: TMD	SAVE DATE: Aug 6, 2021	CLIENT: CPT	JOB NO.: 1015-083-22
REVIEWED: JAB	PLOT DATE: Aug 6, 2021	FILENAME: GRSD-P-STORMP.DWG	

SCALE: SHOWN	
DRAWING IS FULL SCALE UNLESS OTHERWISE NOTED	
SHEET NO.: 03	TOTAL SHEETS: 05



PLAN VIEW
1" = 20'



STORM PROFILE
H: 1" = 20', V: 1" = 5'



GENERAL NOTES


- 1) LOCATION OF EX UTILITIES AND STRUCTURES ARE FROM INFORMATION AVAILABLE AT THE TIME OF DESIGN. EXACT LOCATION AND COMPLETENESS ARE NOT GUARANTEED SERVICES ALERT 811 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION AND POT HOLE FOR EXISTING UTILITY LOCATIONS.
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KEY NOTES

- 1) SEED BARE EARTH/DISTURBED AREAS WITH AN APPROVED GRASS SEED MIX.

VICINITY MAP



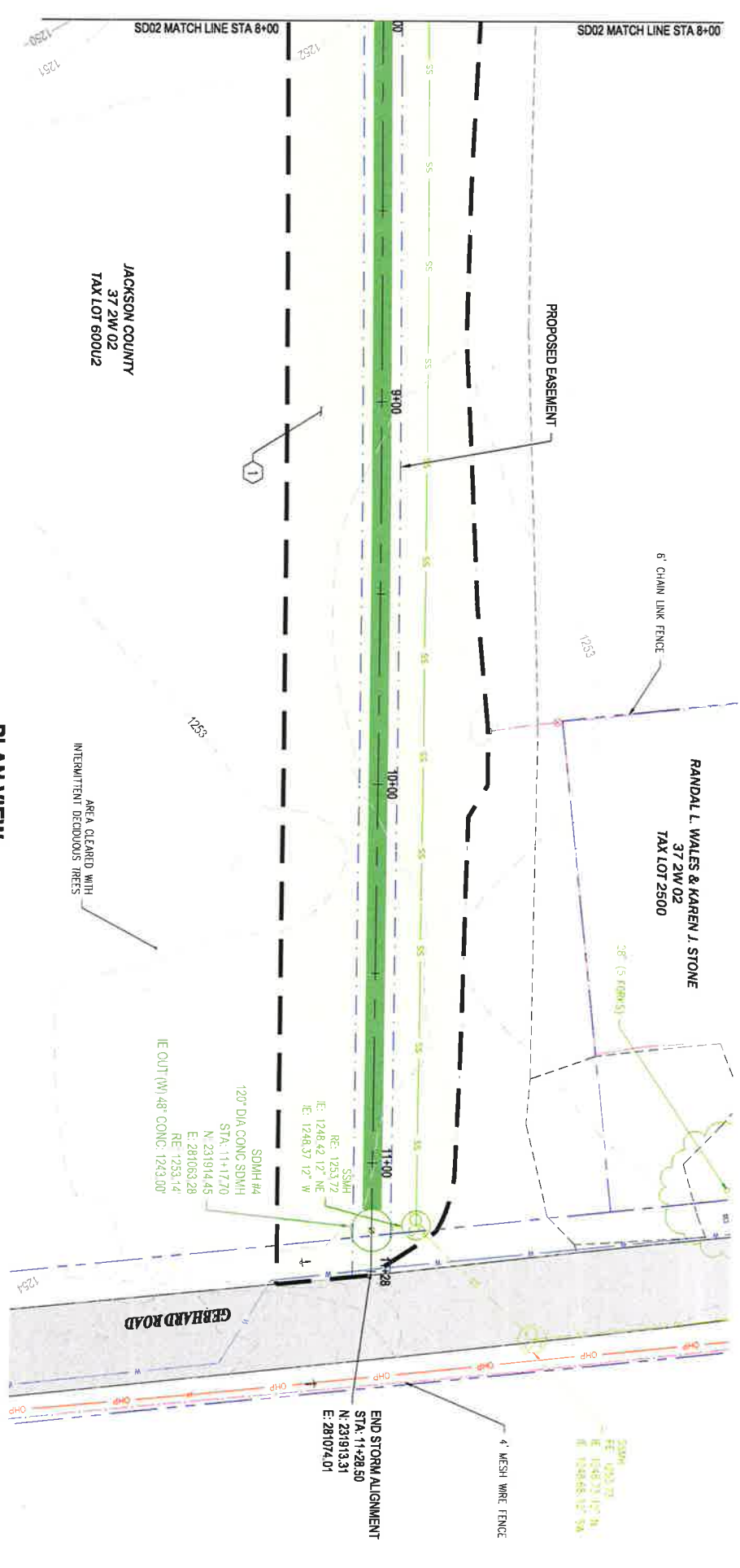

CITY OF CENTRAL POINT
GEHWARD ROAD STORMWATER OUTFALL
STORMWATER PLAN AND PROFILE -
SHEET 2 OF 3



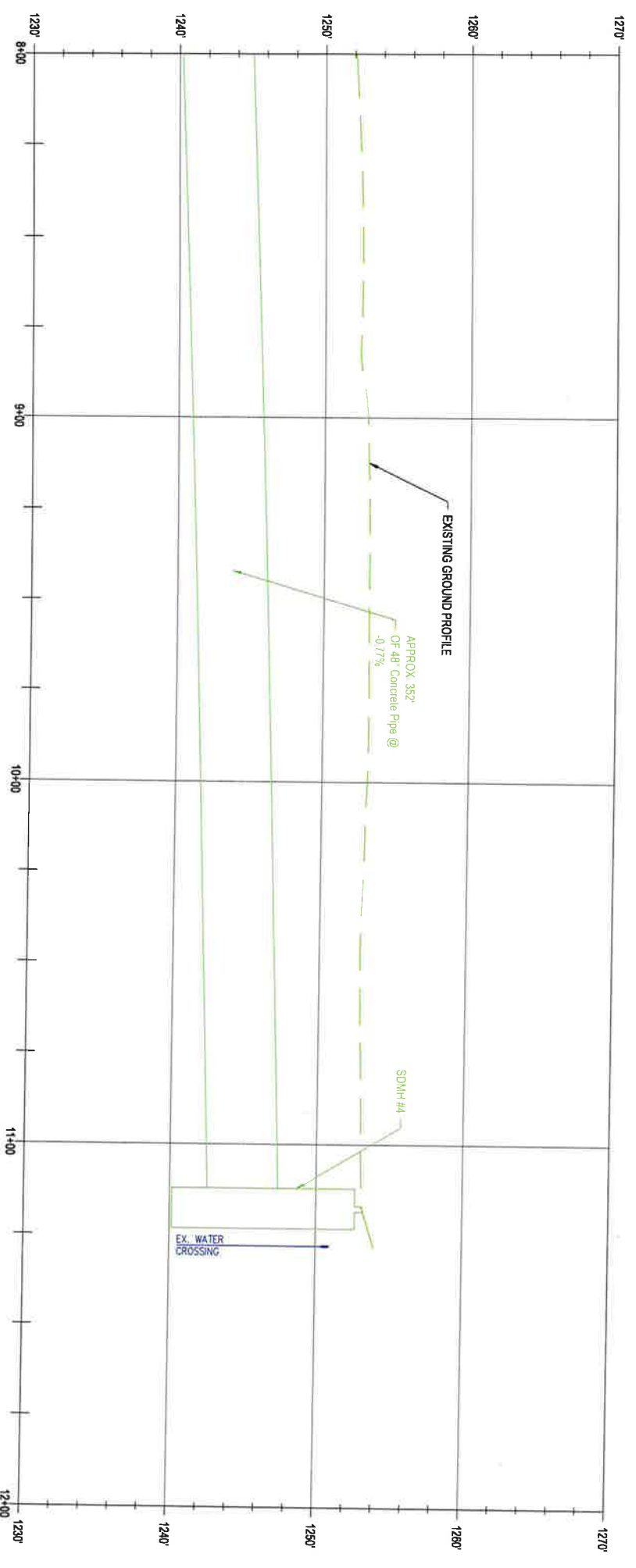
ENGINEER: TMD	SAVE DATE: Aug 6, 2021	CLIENT: CPT	JOB NO.: 1015-083-22
REVIEWED: JAB	PLOT DATE: Aug 6, 2021	FILENAME: GR50-P-STORMP.DWG	
REVISIONS			
NO.	DATE	DESCRIPTION	BY

DRAWING IS SOLE PROPERTY OF
 ENGINEER'S FIRM
 NO. 04 OF 06

Attachment: gebhard road sd project overview (1510 : Gebhard Road Storm Drain - Bid Opening)



PLAN VIEW
1" = 20'



STORM PROFILE
H: 1" = 20', V: 1" = 5'

GENERAL NOTES

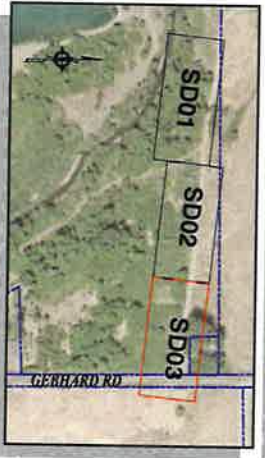
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KEY NOTES

- ① SEED BARE EARTH/DISTURBED AREAS WITH AN APPROVED GRASS SEED MIX.



VICINITY MAP



CITY OF CENTRAL POINT
GEHBARD ROAD STORMWATER OUTFALL
STORMWATER PLAN AND PROFILE -
SHEET 3 OF 3



ENGINEER: TMD	SAVE DATE: Aug 6, 2021	CLIENT: CPT	JOB NO.: 1015-083-22
REVIEWER: JAB	PLOT DATE: Aug 6, 2021	FILENAME: GRSC-P-STORMP.DWG	
REVISIONS			
NO.	DATE	DESCRIPTION	BY

SHEET NO. SD03
 DRAWING SCALE: 1" = 20'
 SHEET TOTAL: 05 OF 06

RESOLUTION NO. _____

A RESOLUTION APPROVING THE LOWEST QUALIFIED BID FOR THE CONSTRUCTION OF THE GEBHARD ROAD STORM DRAIN PROJECT TO CENTRAL PIPELINE, INC. AND AUTHORIZING THE CITY MANAGER TO EXECUTE A CONTRACT.

RECITALS:

A. WHEREAS, the City recently published a solicitation for lowest bids for the construction of the Gebhard Road Storm Drain Project.

B. WHEREAS, the City received six bids.

C. WHEREAS, the lowest bid was submitted by Central Pipeline, Inc. for \$488,000.

The City of Central Point resolves as follows:

Section 1. The City Council hereby accepts the lowest bid from Central Pipeline, Inc for the construction of the Gebhard Road Storm Drain Project.

Section 2. The City Manager is hereby authorized to execute a contract and any related documents necessary to effectuate the acceptance of this award in a form substantially the same as that included in the specifications.

Section 3. This Resolution shall take effect immediately from and after its passage and approval.

Passed by the Council and signed by me in authentication of its passage this _____ day of _____, 2022.

Mayor Hank Williams

ATTEST:

City Recorder

1 - Resolution No. _____ 2/17/2022 Council meeting)

Attachment: resol_gebhard road sd (1510 : Gebhard Road Storm Drain - Bid Opening)



City of Central Point **Staff Report to Council**

ISSUE SUMMARY

TO:	City Council	DEPARTMENT:	Parks and Recreation
FROM:	Dave Jacob, Park Planner		
MEETING DATE:	February 17, 2022		
SUBJECT:	Resolution No. _____, A Resolution Approving an Intergovernmental Agreement with the Rogue Valley Council of Governments for Temporary Use of the Senior Center Building		
ACTION REQUIRED:	Resolution	RECOMMENDATION:	Approval

BACKGROUND INFORMATION

The Rogue Valley Council of Governments (RVCOG) approached the City of Central Point last year to discuss the purchase of the Central Point Senior Center building. RVCOG manages the Food & Friends program in Jackson and Josephine Counties which includes the Meals on Wheels and the Senior Meals program. Currently, RVCOG uses an older facility in Central Point which only allows them to offer home delivered meals. Moving to the Central Point Senior Center site will permit RVCOG to expand their food services to include onsite meals. Since RVCOG will provide low cost or no cost expanded meal services and increased access to senior resources to city residents, the Central Point City Council approved the sale of the property.

The Senior Center has been operated for many years by the Central Point Area Senior Citizens, a nonprofit organization. They have provided daily lunches Monday through Friday along with limited activities for many years. The hope was that with RVCOG providing meal services, the nonprofit would be able to offer expanded social activities for residents. After meeting with both City and RVCOG staff, the nonprofit board of directors decided to disband and will not provide meals or services after February 28. They will vacate the facility on or before March 31.

The RVCOG Board has approved the purchase of the property, but arrangements for the loan and the finalized sale will not be completed prior to April 1. This intergovernmental agreement, if approved, will allow RVCOG to begin using the Senior Center Facility as of April 1, 2022 to provide for continuity of meal services for a period not to exceed 180 days. RVCOG's goal is to move their current meals on wheels program to the facility and to provide onsite meals as soon as possible after April 1. The existing kitchen will need to be updated to meet federal and state standards. The actual reopening date is dependent on completion of this work.

FINANCIAL ANALYSIS:

The facility will be provided to RVCOG without charge for a period not to exceed 180-days. The facility is currently leased to the Central Point Area Senior Citizens at no charge. RVCOG will be responsible for all associated expenses for operating the facility. The IGA will have no financial impact on the City.

LEGAL ANALYSIS:

Legal counsel drafted the resolution and IGA and has no objections to the agreement.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

Strategic Priority: Community Investment

GOAL 2 – Be a city filled with happy, healthy people who are thriving

STRATEGY 1 – Develop and maintain positive partnerships with public and private entities and the community to understand community needs and leverage resources to develop and deliver opportunities for personal, professional, and/or vocational development.

STAFF RECOMMENDATION:

Make a motion to approve the resolution.

RECOMMENDED MOTION:

I move to approve Resolution No. _____, a Resolution approving an intergovernmental agreement with RVCOG for temporary use of the Senior Center building and authorizing the City Manager to execute same.

ATTACHMENTS:

1. RESO IGA RVCOG Senior Center Use
2. IGA 2 - RVCOG - Senior Ctr Use Agt 2-9-2022 (2)

RESOLUTION NO. _____

A RESOLUTION APPROVING THAT INTERGOVERNMENTAL AGREEMENT WITH RVCOG FOR TEMPORARY USE OF SENIOR CENTER BUILDING AND AUTHORIZING CITY MANAGER TO EXECUTE SAME

Recitals:

- A. RVCOG and the City are in the process of negotiating a sale of the Central Point Senior Center commercial building located at 123 N. 2nd Street in Central Point;
- B. RVCOG intends to use the building for provision of senior services and programs including “Meals on Wheels” and “Food with Friends”;
- C. In order to allow continuity of such services pending completion of the sale of the property, RVCOG and City desire to enter into an Intergovernmental Agreement allowing RVCOG to use the building for provision of such services.

The City of Central Point resolves as follows:

Section 1. The Council approves the Intergovernmental Agreement attached hereto as Exhibit “A” and authorizes the City Manager or his designee to sign said Agreement on behalf of the City.

Passed by the Council and signed by me in authentication of its passage this _____ day of February, 2022.

Mayor Hank Williams

ATTEST:

City Recorder

INTERGOVERNMENTAL AGREEMENT
TEMPORARY USE OF SENIOR CENTER

This Intergovernmental Agreement is made and entered into in duplicate as of the later of the dates entered below, by and between the CITY OF CENTRAL POINT, a municipal corporation, hereinafter “CITY” and ROGUE VALLEY COUNCIL OF GOVERNMENTS, hereinafter “RVCOG”) regarding temporary use and operation of the Central Point Senior Center and related facilities for operation of senior services such as “Meals on Wheels” and “Food and Friends” program.

RECITALS

WHEREAS, the State of Oregon has declared it to be a matter of statewide concern to promote intergovernmental cooperation for the purposes of furthering economy and efficiency in local government; and

WHEREAS, Oregon Statutes grant general authority for intergovernmental agreements by units of local government pursuant to the provisions of ORS 190.010 et seq; and

WHEREAS, the Central Point Senior Center, located at 123 N. 2nd Street in Central Point (hereinafter the “Premises”) is currently the location in which multiple services to seniors in the community are offered; and

WHEREAS, RVCOG currently offers meal programs to seniors such as Meals on Wheels and Food & Friends and desires to purchase the Premises for their meal programs; and

WHEREAS, it is necessary and appropriate for RVCOG to secure space to continue providing senior services pending its negotiation and purchase of the Premises from the CITY; and

WHEREAS, CITY and RVCOG deem it to be in their mutual best interest to enter into this Intergovernmental Agreement for the purpose of allowing RVCOG to utilize the Premises and related facilities for continuation of senior center operations.

NOW THEREFORE, in consideration of the above recitals and the mutual covenants, terms and provisions set forth below, the parties agree as follows:

1. The CITY agrees to provide RVCOG use of the Premises for a period up to 180-days without charge for Senior Center operations, including but not limited to “Meals on Wheels” and “Food & Friends” subject to the provisions of section 3 below. The Premises consist of approximately 1800 square feet of commercial building space located at 123 N. 2nd Street, Central Point as well as non-exclusive reasonable use of the common area surrounding the building space for parking, ingress and egress, in common with CITY.
2. During the period of any such temporary use, RVCOG shall provide all necessary personnel, volunteers, equipment and supplies and pay all expenses related thereto in

connection with RVCOG operations. Should it become necessary for RVCOG to use CITY equipment on a temporary basis, the parties agree to meet and confer as to the terms of such use.

3. It is anticipated, at the time of this agreement, that the temporary use of the Premises shall occur Monday through Friday from 7:00 a.m. to 1:30 p.m. and is subject to the following restrictions:
 - 3.1 Any modifications to the Premises made by RVCOG shall be subject to prior approval of CITY. All such modifications shall be at RVCOG's sole expense. RVCOG shall be required to obtain all necessary permits for such work. RVCOG shall pay as due all claims for work done on and for services rendered or material furnished to the Premises and shall keep the Premises free from any liens. If RVCOG fails to pay any such claims or to discharge any lien, CITY may do so and collect the costs from RVCOG. Any amount so added shall bear interest at the rate of twelve percent (12%) per annum from the date expended by CITY and shall be payable on demand.
 - 3.2 RVCOG is responsible for its own set up and take down, cleanup, storage, and any damage done to CITY facilities by its groups or persons whom it may invite to attend.
 - 3.3 The Premises shall be used for the purpose of providing senior services and other civic activities.
 - 3.4 Upon termination of this Agreement, except where such termination is the result of RVCOG's taking title to the Premises, all fixtures placed upon the Premises during the term, other than RVCOG's trade fixtures, shall, at CITY's option, become the property of CITY. If CITY so elects, RVCOG shall remove any or all fixtures that would otherwise remain the property of CITY and shall repair any physical damage resulting from the removal. If RVCOG fails to remove such fixtures, CITY may do so and charge the cost to RVCOG with interest at the legal rate from the date of expenditure.
4. This Agreement shall continue in effect until the closing of the sale of the Premises, or until terminated by either party as provided in paragraph 5.
5. Each party to this Agreement reserves the right to terminate this Agreement by giving the other party 30-days prior written notice of its intent to terminate.
6. Each party agrees to indemnify and hold harmless the other from liability arising from the acts or omissions of the elected officials, officers, employees or agents of the party.
7. This Agreement shall be liberally construed to effect the purposes expressed herein.
8. This Agreement is effective when it has been executed by both parties.

IN WITNESS WHEREOF, the parties have executed this Agreement in triplicate on the dates set forth below.

CITY OF CENTRAL POINT

ROGUE VALLEY COUNCIL OF GOVERNMENTS

By:
Its:

By:
Its:

Attachment: IGA 2 - RVCOG - Senior Ctr Use Agt 2-9-2022 (2) (1506 : Resolution Approving an IGA with RVCOG for Temporary Use of the



City of Central Point Staff Report to Council

ISSUE SUMMARY

TO:	City Council	DEPARTMENT:	Community Development
FROM:	Stephanie Holtey, Planning Director		
MEETING DATE:	February 17, 2022		
SUBJECT:	Transit Oriented Development (TOD) Fence Code Update Discussion		
ACTION REQUIRED:	Information/Direction	RECOMMENDATION:	Not Applicable

BACKGROUND INFORMATION:

The City received a request from School District #6 to modify current fence standards in the Transit Oriented Development (TOD) District to allow fences higher than four (4) feet and to consider allowing powder coated chain-link. The existing fence at this location is a 4-ft galvanized chain-link fence. The current code standards prohibit the use of chain-link fencing with or without slats and goes further to specify the materials that are allowed (i.e. masonry, decorative metal, wood, plant material/hedges or other materials that are approved and complimentary to adjacent building types). It is also required that fences be setback 2-ft from adjacent public right-of-way and the 2-ft buffer be landscaped. It is the School District's objective to replace the existing fencing to provide a more secure facility for its elementary aged kids. Metal is the preferred building material; however, the cost of wrought iron is \$151 per lineal foot, which would result in a cost of about \$407,000. The City's Public Works facility is facing similar challenges in affording compliance with the TOD fencing standards. Although cost alone is not typically the basis for policy decisions, it is indicative of a broader situation that warrants evaluation to determine if the current standards continue to be appropriate or if modifications are needed to mitigate cost while keeping the look and feel desired in the TOD.

At the February 17th City Council meeting, staff will present an overview of the current code standards in the TOD, provide examples of existing fences for context, and code amendment options for discussion and direction from Council.

FINANCIAL ANALYSIS:

There is not cost other than in-kind staff contribution for any code amendments that may be needed to update the City's fence standards in the TOD.

LEGAL ANALYSIS:

Not applicable at this time.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

The discussion about fence standards is directly related to the Community Investment Strategic

Priority in Goal 4, “Manage growth to provide places that are timeless and loved by the community,” and specifically Strategy 2:

Goal 4, Strategy 2: Continually monitor and update the community’s land use to reflect the community’s preferences for how new growth areas will look, feel, and function. (As the City grows in response to market demand, this supports the creation of urban form and architectural character informed by the community’s vision for its future.)

Comment: The discussion about the TOD fence standards was initiated based on feedback and input from School District #6, the Public Works Department and recent staff experience and observation. From time to time, community preferences and/or construction or development practices may necessitate re-evaluation and adjustment of our local standards. This discussion and any subsequent code amendments reflect the City’s commitment to monitoring and adjusting standards when needed.

STAFF RECOMMENDATION:

Provide feedback and direction relative to the TOD fence standards concerning height and building materials.

RECOMMENDED MOTION:

This is a discussion item. No motion is necessary.

To meet the foregoing requirements, staff recommends an overall rate increase of 5% for 2022/2023 FY.

FINANCIAL ANALYSIS:

City/Public Works staff has prepared an update to the City Water Rates based upon the past three years of rate increases. The increase is split consistently between the base rate and the consumptive rate.

Current Rates:

Residential							
Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge	Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf	
5/8"	14.57	1.00	15.57	0.98	1.90	3.04	
1"	19.96	2.45	22.41	0.98	1.90	3.04	
1.5"	25.34	8.15	33.49	0.98	1.90	3.04	
2"	36.12	11.15	47.27	0.98	1.90	3.04	
3"	57.68	25.25	82.93	0.98	1.90	3.04	
4"	79.24	43.85	123.09	0.98	1.90	3.04	
6"	149.30	86.00	235.30	0.98	1.90	3.04	
8"	230.14	139.50	369.64	0.98	1.90	3.04	

Proposed Rates:

Residential							
Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge	Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf	
5/8"	15.30	1.00	16.30	1.03	2.00	3.19	
1"	20.96	2.45	23.41	1.03	2.00	3.19	
1.5"	26.61	8.15	34.76	1.03	2.00	3.19	
2"	37.93	11.15	49.08	1.03	2.00	3.19	
3"	60.56	25.25	85.81	1.03	2.00	3.19	
4"	83.20	43.85	127.05	1.03	2.00	3.19	
6"	156.77	86.00	242.77	1.03	2.00	3.19	
8"	241.65	139.50	381.15	1.03	2.00	3.19	

Based upon this methodology, the Base Rate will increase by \$0.73 and the consumptive rates by \$0.05 for Tier 1, \$0.10 for Tier 2, and \$0.15 for Tier 3. Attached is the full rate resolution reflecting residential, commercial, multi-family and irrigation rates based on the hybrid rate increase proposal.

LEGAL ANALYSIS: N/A

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

City of Central Point 2040 Strategic Plan – Strategic Priority – Responsible Governance

GOAL 1 - Maintain a strong financial position that balances the need for adequate service levels and capital requirements against the affordability that is desired by our citizens.

STRATEGY 1 – Continually update the City’s long-term financial plan/strategy.

STRATEGY 2 – Work with partner agencies and stakeholders to eliminate redundancy and maximize efficiency in all areas.

STRATEGY 3 – Solicit support from partner agencies in our effort to maintain a robust financial position.

STAFF RECOMMENDATION: Staff recommends approval of the Resolution.

RECOMMENDED MOTION: I move to approve Resolution. ___ a Resolution of the City of Central Point Setting a Water Rate Adjustment effective March 1, 2022.

ATTACHMENTS:

1. 2022 wate rate resolution
2. Water rates 2022
3. fcs rate model

RESOLUTION NO. _____

A RESOLUTION OF THE CITY OF CENTRAL POINT
SETTING A WATER RATE ADJUSTMENT EFFECTIVE MARCH 1, 2022

Recitals:

- A. The City has received information from the Medford Water Commission that March 1, 2022 the rate for bulk water purchases will increase by a minimum of 4% to a maximum of 8%, an estimated additional cost of at least \$44,000 annually.
- B. The City of Central Point conducted an update based on inflationary cost increases and determined that a 4% increase is needed to handle the increased supply costs and paying off the Vilas Reservoir and Pump Station bond two years early.
- C. The combined increases equate to a 5% increase which would be split between the base and tier rates.

The City of Central Point resolves as follows:

Section 1. Effective March 1, 2022, the City of Central Point Water Rates shall be adjusted as set forth on the Attachment A.

Passed by the Council and signed by me in authentication of its passage this _____ day of February 2022.

Mayor Hank Williams

ATTEST:

City Recorder

Attachment: 2022 wate rate resolution [Revision 1] (1509 : 2022 Water Rates)

2022/23 Utility Rates

Attachment A

Cost of Service - Water Rates (Per hundred cubic feet)

Res. No.

(Into effect March 21, 2022)

Residential

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	15.30	1.00	16.30
1"	20.96	2.45	23.41
1.5"	26.61	8.15	34.76
2"	37.93	11.15	49.08
3"	60.56	25.25	85.81
4"	83.20	43.85	127.05
6"	156.77	86.00	242.77
8"	241.65	139.50	381.15

Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19

Senior-Housing

Units	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5	22.71	1.25	23.96
10	45.42	2.50	47.92
15	68.13	3.75	71.88
20	90.85	5.00	95.85
25	113.56	6.25	119.81
30	136.27	7.50	143.77
35	158.98	8.75	167.73
40	181.69	10.00	191.69

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Iti-Family Residential

Units	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
3	24.66	4.50	29.16
4	32.89	6.00	38.89
5	41.11	7.50	48.61
6	49.33	9.00	58.33
7	57.55	10.50	68.05
8	65.77	12.00	77.77
9	73.98	13.50	87.48
10	82.20	15.00	97.20

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Commercial & Standby

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	13.60	1.00	14.60
1"	16.99	2.10	19.09
1.5"	22.65	4.00	26.65
2"	28.31	7.15	35.46
3"	45.29	14.10	59.39
4"	65.66	21.70	87.36
6"	117.16	44.00	161.16
8"	182.79	68.00	250.79

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Irrigation

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	15.30	1.50	16.80
1"	20.96	4.15	25.11
1.5"	26.61	8.65	35.26
2"	37.93	14.00	51.93
3"	60.56	28.00	88.56
4"	83.20	44.00	127.20
6"	156.77	88.00	244.77
8"	241.65	139.00	380.65

Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19

Hydrant	30.99	20.00	50.99
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1.81

2017/18 Street Fee

\$6.06

2017/18 Stormwater Fee

\$6.50

2017/18 Stormwater Quality Fee

\$1.00

Notes:

Backflow Fee

\$1.00 w/backflow
\$0.25 w/o backflow

Damage Recovery

hour of a PW employee, + 10% admin fee

Expo	0.00	0.00	0.00	1.81
Hardship Discount			50%	50%
Outside City Factor			200%	200%

Central Point
Utility Rate Study: Water Utility
Summary

Revenue Requirement	2023	2024	2025	2026	2027	2028	2029	2030
Revenues								
Rate Revenues Under Existing Rates	\$ 3,787,531	\$ 3,825,406	\$ 3,873,224	\$ 3,935,787	\$ 3,999,361	\$ 4,063,962	\$ 4,129,606	\$ 4,196,311
Non-Rate Revenues	<u>275,106</u>	<u>277,913</u>	<u>282,003</u>	<u>286,578</u>	<u>291,229</u>	<u>295,955</u>	<u>300,758</u>	<u>305,639</u>
Total Revenues	\$ 4,062,637	\$ 4,103,319	\$ 4,155,227	\$ 4,222,366	\$ 4,290,590	\$ 4,359,917	\$ 4,430,365	\$ 4,501,950
Expenses								
Cash Operating Expenses	\$ 3,313,813	\$ 3,450,685	\$ 3,518,233	\$ 3,587,445	\$ 3,658,118	\$ 3,730,285	\$ 3,803,980	\$ 3,879,238
Existing Debt Service	509,106	664,710	664,220	662,504	662,552	662,335	661,854	1,434,096
New Debt Service	-	57,862	57,862	57,862	57,862	57,862	57,862	57,862
System Reinvestment Funding	-	-	-	-	-	-	-	-
Additions Required to Meet Reserves	-	-	-	-	-	-	-	-
Total Expenses	\$ 3,822,919	\$ 4,173,257	\$ 4,240,314	\$ 4,307,811	\$ 4,378,531	\$ 4,450,482	\$ 4,523,697	\$ 5,371,196
Net Surplus (Deficiency)	\$ 239,718	\$ (69,938)	\$ (85,087)	\$ (85,446)	\$ (87,942)	\$ (90,565)	\$ (93,332)	\$ (869,246)
Additions to Meet Coverage	-	-	-	-	-	-	-	-
Total Surplus (Deficiency)	\$ 239,718	\$ (69,938)	\$ (85,087)	\$ (85,446)	\$ (87,942)	\$ (90,565)	\$ (93,332)	\$ (869,246)
Annual Rate Increase	4.00%	4.00%	4.00%	4.00%	3.00%	3.00%	3.00%	2.00%
Cumulative Rate Increase	9.20%	13.57%	18.11%	22.84%	26.52%	30.32%	34.23%	36.91%
Revenues After Rate Increases	\$ 4,016,676	\$ 4,219,117	\$ 4,442,730	\$ 4,695,072	\$ 4,949,466	\$ 5,180,296	\$ 5,421,891	\$ 5,660,673
Additional Taxes from Rate Increase	<u>3,900</u>	<u>6,700</u>	<u>9,692</u>	<u>12,922</u>	<u>16,169</u>	<u>18,998</u>	<u>21,992</u>	<u>24,920</u>
Net Cash Flow After Rate Increase	\$ 464,964	\$ 317,073	\$ 474,727	\$ 660,918	\$ 845,994	\$ 1,006,771	\$ 1,176,961	\$ 570,195
Coverage After Rate Increase: Bonded Debt	n/a	19.99	22.73	25.96	29.22	32.08	35.11	38.10
Coverage After Rate Increase: Total Debt	2.14	1.60	1.82	2.08	2.35	2.58	2.82	1.48
Sample Residential Bill (5/8" Meter, x 14 ccf)	\$36.89	\$38.37	\$39.90	\$41.50	\$42.74	\$44.02	\$45.34	\$46.25
Annual Increase (\$)	\$1.42	\$1.48	\$1.53	\$1.60	\$1.24	\$1.28	\$1.32	\$0.91

Attachment: fcs rate model (1509 : 2022 Water Rates)

CITY OF CENTRAL POINT

Oregon

City Council Meeting Minutes Thursday, January 27, 2022

I. REGULAR MEETING CALLED TO ORDER

The meeting was called to order at 7:00 PM by Mayor Hank Williams

II. PLEDGE OF ALLEGIANCE

III. ROLL CALL

Attendee Name	Title	Status	Arrived
Hank Williams	Mayor	Present	
Neil Olsen	Ward I	Present	
Kelley Johnson	Ward II	Present	
Melody Thueson	Ward III	Present	
Taneeea Browning	Ward IV	Present	
Rob Hernandez	At Large	Present	
Michael Parsons	At Large	Present	

Staff members present: City Manager Chris Clayton; City Attorney Sydnee Dreyer (Remote); Police Chief Kris Allison (Remote); Police Captain Dave Croft; Police Captain Scott Logue (Remote); Parks and Public Works Director Matt Samitore; Planning Director Stephanie Holtey; Finance Director Steve Weber; IT Director Jason Richmond (Remote); and Accounting Business Services Coordinator Rachel Neuenschwander.

IV. PUBLIC COMMENTS

Mr. Bogenoff spoke to the council regarding someone breaking windows of houses and cars along Freeman Rd, and someone parking a trailer and van in front of his and his neighbors house for an extended period of time.

V. CONSENT AGENDA

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Rob Hernandez, At Large
SECONDER:	Taneeea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

A. Approval of January 13, 2022 City Council Minutes

VI. ITEMS REMOVED FROM CONSENT AGENDA

VII. PUBLIC HEARING

A. Resolution Authorizing Exemption from Competitive Solicitation - RH2 Engineering, Inc.

Minutes Acceptance: Minutes of Jan 27, 2022 7:00 PM (CONSENT AGENDA)

City Attorney Sydnee Dreyer presented a resolution authorizing exemption from competitive solicitation - RH2 Engineering Inc. Under city code the contracting board is authorized to exempt a contract from competitive bidding, if certain factors in your code are met. In this case there would be substantial cost savings to grant the exemption from competitive bidding.

Exemption is sought for a Design and Engineering contract for the improvements to the Central Point Little League Property. Much of the work toward these improvements was previously completed by RH2, the City's engineer, on the basis of previously authorized contracts for a conceptual plan, detailed cost estimate, project schedule and summary of tasks to be performed to complete the project.

The estimated cost for the design and engineering work is \$275,858.00 (scope and project estimate attached). Exempting this project is likely to result in significant cost savings given that the initial conceptual plan, budget and detailed summary of tasks has already been prepared by RH2, which result in lower costs to finalize the design and engineering of the improvements. Additionally, this project is subject to significant time restraints given that a portion of the funding is subject to the American Rescue Plan Act (ARPA) guidance. Awarding the contract to RH2 will allow the design and engineering to be completed much more quickly as RH2 is already in possession of a portion of the information, analysis and survey it will need to finalize these plans.

Council was concerned on how the contingency is handled the \$25,000, Parks and Public Works Director Matt Samitore stated that they have to make a request to use the contingency. In the past RH2 has been really good at not needing to use the contingency.

Council asked if the project schedule completion date of September 2022 is accurate. Mr. Samitore stated that this is what they were wanting but in this day and time it is not realistic. Summer of 2023 construction, product will be the biggest issue. City Manager Chris Clayton stated the design will get done but as we work on the design if bid is completed in the first half of the summer will contractors work during the summer. They will be meeting with the little league in the next couple of weeks. Everyone is working towards doing this as soon as possible, however the most likely it will be done the following year. The Planning Dept. will be starting the land use component, this is EFU lands with their being an existing facility it should be fine.

Kelley Johnson moved to approve Resolution No. 1696 Adopting Findings Authorizing an Exemption from Competitive Solicitation and Awarding a Contract to RH2 Engineering, Inc. For Design and Engineering of Little League Fields.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Kelley Johnson, Ward II
SECONDER:	Taneea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

VIII. ORDINANCES, AND RESOLUTIONS

A. Ordinance No. _____, An Ordinance amending Central Point Municipal Code Chapter 13.20 in Part Regarding Backflow Prevention Devices

Mr. Samitore presented the second reading of an Ordinance amending the Central Point Municipal code regarding backflow prevention devices. No changes since the first reading.

Council wanted clarification that this matches what Medford Water Commission, Matt stated yes and the City of Ashland adopted something similar this last year, and Eagle Point has already updated there requirements.

Melody Thueson moved to approve Ordinance No 2083 an Ordinance Amending Central Point Municipal Code Chapter 13.20 in Part Regarding Backflow Prevention Devices.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Melody Thueson, Ward III
SECONDER:	Michael Parsons, At Large
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

B. Ordinance No. _____, An Ordinance Amending in Part Central Point Municipal Code Chapters 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers

City Attorney Sydnee Dreyer presented the second reading of ordinance amending in part CPMC 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers requires pawnbrokers and secondhand dealers to maintain a report of all property received on deposit, pledged or purchased and to submit that electronic report to the City. There were no changes since the first reading.

Council was concerned if the current businesses know this is coming. Captain Croft stated that other cities are doing this and that they will be notified of this change.

Council questioned secondhand clothing stores having to do this. Mrs. Dreyer stated that if they are not currently using the electronic system then they would not need to do this.

Mike Parsons moved to approve Ordinance No 2084 an Ordinance Amending in Part Central Point Municipal Code 5.34.010 and 5.34.020 Pawnbrokers and Secondhand Dealers

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Michael Parsons, At Large
SECONDER:	Rob Hernandez, At Large
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

C. Ordinance No. _____, An Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford for Use of the Public Right of Way Relating to Small Cell Wireless Facilities

Sydnee Dreyer presented the second reading of an Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford (“USCC”) for use of Public Right of Way Relating to small cell wireless facilities. No changes were made since the first reading.

Council was concerned if these towers interfere with the airport, Jeff Colantino with USCC answered that the proposed locations will not have any impact on air travel.

Rob Hernandez moved to approve Ordinance No. 2085 an Ordinance Establishing a Franchise Agreement Between United States Cellular Operating Company of Medford for use of the Public Right of Way Relating to Small Cell Wireless Facilities.

RESULT:	APPROVED [UNANIMOUS]
MOVER:	Rob Hernandez, At Large
SECONDER:	Kelley Johnson, Ward II
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

D. Public Hearing and First Reading - An Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230

Planning Director Stephanie Holtey presented the first reading of an Ordinance Amending the transportation system plan of Central Point Comprehensive plan to add road diet improvements to Project No 230 at the intersection of Scenic Avenue and Highway 99 when warrants are met. The project is identified in the TSP to address safety issues at the intersection. A traffic report performed by the Oregon Department of Transportation (ODOT) documented 36 accidents over a 10-year period that resulted in serious injuries and one (1) fatality. The traffic analysis shows that the current five (5) lane road configuration does not meet warrants and that adding road improvements reconfiguring Highway 99 to three (3) lanes allows signal warrants to be met as needed for the project to proceed.

There is no cost for the proposed amendment other than in-kind staff expenses associated with processing the application. It is important to note, however, that approval of the proposed amendment will allow ODOT to apply grant funds authorized through the All Roads Transportation Safety (ARTS) Program to construct Project No. 230 improvements. The total project cost is \$3M and the City, per a revised Intergovernmental Agreement, is contributing \$650K. The City was previously obligated to fund the project in its entirety as a condition for opening a new railroad crossing in Twin Creeks. Approval of the proposed TSP Amendment will allow grant fund disbursement allowing the City meet its obligation for the Twin

Creeks Rail Crossing at significant cost savings.

Council was concerned as to why it needs to be a three lane rd. Mr. Samitore stated that there is not enough traffic to install a light with five lanes but with three lanes this would warrant a light to be installed

Melody Thueson moved to approve the first reading of Ordinance Amending the Transportation System Plan (TSP) of the Central Point Comprehensive Plan to Add Road Diet Improvements to Project No. 230.

RESULT:	1ST READING [UNANIMOUS]
MOVER:	Melody Thueson, Ward III
SECONDER:	Taneea Browning, Ward IV
AYES:	Williams, Olsen, Johnson, Thueson, Browning, Hernandez, Parsons

IX. BUSINESS

A. ARPA Project Eligibility Update

Finance Director Steve Weber presented an update on the ARPA Project Eligibility and that at the September 9, 2021 City Council meeting, staff presented a revised spending plan based on the City’s updated allocation (\$4.184 million) of the American Rescue Plan Act (ARPA) stimulus package that was approved by the Federal Government. The spending plan was compiled based staff’s interpretation of the U.S. Treasury guidelines issued at the time. For two of the projects, staff sought input from legal counsel that the projects are permitted under the referenced categories in the guidelines.

The two projects are the Dennis Richardson Memorial Project and the Central Point Little League Project, the City Attorney reviewed these projects in categories that were identified and these two projects fit in the treasury guidelines. The American Rescue Plan Act (ARPA) award is included in the City’s 2021-23 biennial budget.

B. Discussion of 2022 Water Rates

Matt Samitore presented Water Rate Study - The City rehired the FCS group to determine what rate structure adjustments the City of Central Point would need for the next decade. The revised rate would be based upon four dynamics. 1) is the projected annual increases from the MWC. 2) is that we did not achieve our water fund revenue projections/targets for FY 2020-2021. 3) dynamic involves the increasing price for goods and services. 4), with the UGB expansion forthcoming (and the associated need for a new reservoir), we need to plan for retiring debt service from the Vilas Pump Station and Reservoir by 2031, two years before what was initially planned.

Current water rates are based more on consumption then the base rates. Tiered rates are causing the consumption to go down, other cities are increasing the base rate \$1 every year. Central Point now has the 2nd lowest rates in Southern Oregon.

Two options were presented, A) is to continue to increase rates based on annual review of MWC Rate, inflation, previous water season revenue. B) would be to

increase the base rate from \$15.57 to \$18.58, with the next potential increase in March of 2026.

Base on the math increasing the base rate is the best option, it is not popular for those who do not use much water and does not give them a chance to save. Right now the City's rates are 1/3 on base 2/3 on consumption. Consultants state you want 2/3 base and 1/3 consumption. Tiered rates encourages conservation, and certain amount of blight, base rates are regressive for residential customers on fixed incomes, we have a hardship program available. The state of Oregon has changed there metric of who is low income to 200% of the federal poverty level.

Council would like to see a stronger presentation on the Hardship Program to the seniors.

City Manager Chris Clayton stated that a phased approach to the base rate, if the council passed the plan and not implement it all at once, \$2 this year, \$2 next year, and \$1 the third year would still accomplish the city's goals.

Council shared that inflation is understandable, and cost of water. Council would like to see what a \$2 raise would do and what other options may look like.

City Manager Chris Clayton stated that this money is not eligible to be spent on anything outside of the water system, the only debt the water fund has is the water reservoir, and it will be taking on part of the corporation yard. Shortening the debt by 2yrs is to keep from having more debt on the water fund. Currently the city can only handle so much growth before we need another reservoir.

X. MAYOR'S REPORT

Mayor Hank Williams reported that he attended the Study Session.

XI. CITY MANAGER'S REPORT

City Manager Chris Clayton reported that:

- He received an email followed by a letter to the editor in the Mail Tribune about the City not displaying flags on Martin Luther King Day. Currently the flags are displayed on Presidents Day, Oregon's Birthday, Peace Officers Memorial Day, Armed Forces Day, Memorial Day, Flag Day, Independence Day, Labor Day, 9/11 Remembrance Day, Veterans Day and Pearl Harbor Day. With councils recommendation we can add Martin Luther King Day.

Discussion was made to amend the list and include MLK day.

- February Council Meeting changes, Council meeting will be on the February 17th and a Study Session on February 28th
- Changes made with opportunities for recycling electronic waste in the valley
- Dr. Bruce Dingler passed away.

- The RVCOG board approved the transition and the purchase of the senior center. Staff met with COG staff today and are working on a the plan for social and recreational opportunities for seniors.
- He has information on who is planning on purchasing the Banner Bank property
- MWC made an error in the cities favor in the amount of \$12,000 on the city's monthly statements.

XII. COUNCIL REPORTS

Council Member Kelley Johnson reported that she attended the study session, and appreciated seeing the public works building.

Council Member Neil Olsen reported that he attended the study session.

Council Member Tanea Browning reported that:

- Attended listening session in Vale, great attendance and participation from the region including Sen. Finley, several representatives from each city, including Nyssa, Burns, Ontario. Learned about the boarder board that addresses issues that arise specifically from being located near the border of Idaho and Oregon. The Board has additional tools available to these communities. Specifically, they had two onion sheds relocated to Idaho due to significant additional cost to rebuild in Oregon.
- March 10th listening session here in CP - much shorter car ride than when we all traveled to GP in my Subaru to elected essentials.
- She will be flying into DC next week for the State League Presidential fly in for NLC, this meeting along with the appointment to the state cabinet on infrastructure logistics is going to assist all of our Oregon cities in hopes to see the fed funding monies.
- Study session touring the new public works facility, very excited!

Council Member Rob Hernandez reported that:

- He attended by Zoom Jackson County/Central Point Community Center meeting.
- He attended the study session
- District 6 Bond Oversight Committee went and looked at new gym and Jewett and the new learning center.

Council Member Mike Parsons reported that:

- Attended the January Rogue Valley Services Board meeting. Housekeeping issues, personal policies updates, adjustments to the compensation plan were performed, and appointments to the budget committee were made.
- Attended the Study Session, tour of Public Works Building.

- Attended the Jackson County Public Safety Coordinating Council meeting. Funding mechanisms and collaborations to address behavior health housing needs were addressed by Julie Jackson with Jackson Care Connect.

Council Member Melody Thueson reported that she attended the study session.

XIII. DEPARTMENT REPORTS

Planning Director Stephanie reported that:

- The City's UGB Amendment is being reviewed by the State Department of Land Conservation and Development (DLCDC) and the Department of Justice. The deadline for UGB Acknowledgement is February 23, 2022. Due the fact an objection was filed, the UGB Amendment is subject to more rigorous review. However the objection had to with the State allowing prime farmland to be included in the City's urban reserves and not the City's application of criteria governing UGB Amendments. As such, no delay in the acknowledgement process is anticipated by the State. Once in the UGB, the newly added lands are not eligible to be annexed until the Transportation System Plan (TSP), Environmental Element and zoning code update and complete. All three projects are currently underway and scheduled to be completed concurrently within the next 18-24 months.
- All spaces at the new commercial building located at 98 Freeman Road are full except for one 1,100 square foot space that is seeing interest from a bagel shop and shipping center. Other spaces are leased to a dental office, physical therapy office, real estate office, Mazatlán Grill restaurant and Edward Jones office.
- Smith Crossing Phase 3 is scheduled for a Pre-Application Conference in mid-February. The development objective is to expand Smith Crossing into another phase East of Pear Valley. The site has several challenges associated with access, easements and floodplain impacts. The purpose of this meeting is to identify viable solutions to advance the project.

Police Captain Dave Croft reported that:

- A couple weeks ago the Police Department launched a pod cast called The Wide Blue Line. 1st episode was launched a couple of weeks ago, and Wednesday the 26th the 2nd episode was launched.
- In the last 2-3 weeks there was bank robbery and a robbery at the Purple Parrot, they have some good leads and good information on the vehicles and hope to have some conclusion to those soon.

Parks and Public Works Director Matt Samitore reported that:

- The restroom at Pfaff Park contractor is having some supply and demand issues, restroom not to be installed until the end of April. Temporary porta-potties will be setup.
- Meeting with ODOT and Jackson County regarding Beebe Rd extension UGB Expansion and Community Center.

- Attended a kickoff meeting on the Bear Creek Greenway plan.

Finance Director Steve Weber reported that the amended financing for the Public Works Operations Center closed

Jackson County Commissioner Dave Dotterer reported that:

- Greenway project was a great meeting.
- Special Session of the Oregon Legislature on the Marijuana and Hemp issues went well.

XIV. EXECUTIVE SESSION

XV. ADJOURNMENT

Rob Hernandez moved to adjourn. All said aye and the meeting was adjourned at 8:55 p.m.

The foregoing minutes of the January 27, 2022, Council meeting were approved by the City Council at its meeting of _____, 2022.

Dated:

Mayor Hank Williams

ATTEST:

City Recorder

Minutes Acceptance: Minutes of Jan 27, 2022 7:00 PM (CONSENT AGENDA)



City of Central Point **Staff Report to Council**

ISSUE SUMMARY

TO:	City Council	DEPARTMENT:	Parks and Recreation Commission
FROM:	Dave Jacob, Park Planner		
MEETING DATE:	February 17, 2022		
SUBJECT:	Proclamation - Arbor Week 2022		
ACTION REQUIRED:	Consent Agenda Item	RECOMMENDATION:	Approval

Purpose: The proclamation illustrates the value of tree awareness and of Arbor Week in Central Point.

Background: The Arbor Week proclamation is important to the City of Central Point in that it will assist in securing recertification for the eighth year of the Tree City USA designation for 2022. To qualify as a Tree City USA community, a city must meet the required standards established by The Arbor Day Foundation and the National Association of State Foresters. The standards help to ensure that every qualifying community, no matter the size, has a viable tree management plan which includes the celebration of Arbor Week. In addition to accomplishing Tree City USA goals, establishing an Arbor Week celebration will help to raise awareness of the importance of trees in Central Point, expand the tree canopy by planting additional trees, and enhance the parks and open spaces where the trees are planted.

Recommendation: Approve the Arbor Week proclamation and join Central Point Parks and Recreation Department in celebration of Arbor Week: April 3 - 9, 2022.

ATTACHMENTS:

1. ARBOR WEEK PROCLAMATION 2022.docx

ARBOR WEEK PROCLAMATION

WHEREAS, J. Sterling Morton proposed to the Nebraska Board of Agriculture in 1872 that a special day be set aside for the planting of trees; and

WHEREAS, Arbor Day was first observed with the planting of more than a million trees in Nebraska; and

WHEREAS, Arbor Day is now observed throughout the nation and the world; and

WHEREAS, trees can reduce the erosion of topsoil by wind and water, reduce heating and cooling costs, moderate the temperature, clean the air, produce oxygen, strengthen stream bank integrity, provide flood mitigation, and provide a habitat for wildlife; and

WHEREAS, trees are a renewable resource that provides paper, wood, fuel for our fires, and countless other wood products; and

WHEREAS, trees in our city increase property values, enhance the economic vitality of business areas, and beautify our community; and

WHEREAS, trees, wherever they are planted, are a source of joy and spiritual renewal,

NOW, THEREFORE, I _____ Mayor of the city of Central Point, do hereby proclaim April 3rd – 9th, 2022 as

Arbor Week

in the City of Central Point, and I urge all citizen to celebrate Arbor Week and to support efforts to protect our trees and woodland and

Futher, I urge all citizens to plant and care for trees to gladden the heart and promote the well-being of this and future generations.

Date this _____ Day of _____ in the Year _____

Mayor _____

Attachment: ARBOR WEEK PROCLAMATION 2022.docx (1507 : Proclamation - Arbor Week 2022)

City of Central Point, Oregon
140 S 3rd Street, Central Point, OR 97502
541.664.3321 Fax 541.664.6384
www.centralpointoregon.gov



Administration Department
Chris Clayton, City Manager
Deanna Casey, City Recorder
Elizabeth Simas, Human Resource Director

**APPLICATION FOR APPOINTMENT TO
CITY OF CENTRAL POINT COMMITTEE**

Name: Eden Foster Date: 2-3-2022

Address: [Redacted] Shaker Drive, Central Point, OR 97502

Home Phone: _____ Business Phone: _____ Cell Phone: [Redacted]

Fax: _____ E-mail: [Redacted]@gmail.com

Are you a registered voter with the State of Oregon? Yes X No _____

Are you a city resident? Yes X No _____ If Yes, How long: 2006

Which committee(s) would you like to be appointed to: *(Please make sure the dates below work with your schedule before applying. Council and Planning Commission members are required to file an Annual Statement of Economic Interest to the State of Oregon.)*

- Meeting Dates (All meeting dates are subject to change or additions, times vary for each committee):**
- Budget Committee: Meetings vary in April Bi-Annually
 - Citizens Advisory Committee: 2nd Tuesday of quarterly
 - Planning Commission: 1st Tuesday of each month
 - Parks and Recreation Committee/Foundation: Meeting dates vary

Employment, professional, and volunteer background:

Community affiliations and activities:

Previous City appointments, offices, or activities:

Attachment: Eden Foster Application_Redacted (1508 : Park Commission Appointment)

Central Point Committee Application
Page 2

To provide additional background for the Mayor and City Council, please answer the following questions.

1. Please explain why you are interested in the appointment and what you would offer to the community.

2. Please describe what you believe are the major concerns of the City residents and businesses that this committee should be concerned about.

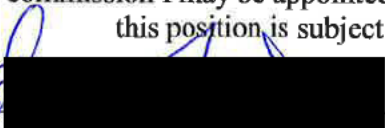
3. Please provide any additional information or comments which you believe will assist the City Council in considering your application.

4. Do you anticipate that any conflicts of interest will arise if you are appointed; and if so, how would you handle them?

Please feel free to use additional sheet if you have more information to help the Council make a final decision.

My signature affirms that the information in this application is true to the best of my knowledge. I understand that misrepresentation and/or omission of facts are cause for removal from any council, advisory committee, board or commission I may be appointed to. All information/documentation related to service for this position is subject to public record disclosure.

Signature: _____



Date: _____

2-3-2022

Attachment: Eden Foster Application_Redacted (1508 : Park Commission Appointment)

Application for Employment to City of Central Point

Parks and Recreation Committee

Eden Foster

February 3, 2022

Employment, professional and volunteer background:

I worked for 25 years in land conservation and public garden education programming and management for various public gardens, arboreta, and land conservation organizations around the country. During that period I also earned a master's degree in Urban Affairs and Planning from Boston University. I later became a full-time freelance writer for twelve years. After moving to Oregon, I worked as a receptionist for Twin Creeks Retirement from 2009-2011, and since then have led an active retired life.

Community affiliations and activities:

Co-founder, Plant a Thousand Daffodils, a program jointly sponsored by the Parks and Recreation Foundation and the Parks and Recreation Department. Recruited more than 100 volunteers to plant 1,700 daffodil bulbs in three public parks and two public areas in Central Point. Raised money to pay for the program and to benefit the Foundation. Am currently involved in designing a poetry contest for elementary students and art contest for middle and high school students that will take place this spring.

Have volunteered for special events sponsored by Parks and Recreation since approximately 2010 and have taught several classes on a voluntary basis.

Central Point Friends of the Library: serve as interim secretary, member of Book Buffs, steward for Little Free Library at Don Jones Park, 2020 to present

Delivered meals daily for six weeks to fire victims and did various miscellaneous service activities, fall, 2020

Travelled to Guatemala to deliver water filtration systems to villages with the Medford Rogue Rotary, annually 2014-2019

Worked with Rogue Climate to lobby State legislators on climate-related bills, various years

Served on the board of directors for Armadillo Technical Institute in Phoenix, OR from 2006-2011

Homeowner's Association Board of Directors, 2007-2011

Previous City appointments, offices or activities:

Parks and Recreation Foundation board member and current president, 2017-present

Central Point Budget Committee, 2021-present

1. Please explain why you are interested in the appointment and what you would offer to the community.

I believe that parks and recreational activities play an integral role in the physical, mental and emotional health of our community. My background in garden programing and management as well as my interest in city planning is easily translated into work with the Parks and Recreation Department. Over the years I have forged positive relationships with City staff in the department and with volunteers and businesses in the community. I believe these relationships will benefit my work with the Parks and Recreation Committee.

2. Please describe what you believe are the major concerns of the City residents and businesses that this committee should be concerned about.

Health in all its forms has become even more critically important since the advent of COVID-19. Parks and Recreation can provide safe, high-quality opportunities to participate in healthful and fun activities while building a strong sense of community. Central Point has long been admired for its numerous parks and the city is now a sought-after destination to both live and work. I believe that a long-range plan for keeping the current park's infrastructure sound and planning for the development of new open spaces is of critical importance to this committee so that we continue to attract families and businesses to our community. I also believe that an emphasis on diversity as it relates to programing and accessibility is also extremely important.

3. Please provide any additional information or comments which you believe will assist the City Council in considering your application.

I have experienced great joy in volunteering with the Parks and Recreation department for more than a decade. The staff has been generous with their time and ideas and has been warm and welcoming. We are so fortunate to have such high-quality professionals who have selected Central Point as their place to work.

4. Do you anticipate that any conflicts of interest will arise if you are appointed?

I do not anticipate any conflicts of interest.

\$650K. The City was previously obligated to fund the project in its entirety as a condition for opening a new railroad crossing in Twin Creeks. Approval of the proposed TSP Amendment will allow grant fund disbursement allowing the City meet its obligation for the Twin Creeks Rail Crossing at significant cost savings.

LEGAL ANALYSIS:

The proposed Comprehensive Plan Amendment to the TSP is subject to the approval criteria in CPMC 17.96.500 for Major Amendments. To approve a Major Amendment, the City Council must find that it is consistent with applicable Statewide Planning Goals, the City of Central Point Comprehensive Plan and the State Transportation Planning Rule. As demonstrated in the Findings of Fact and Conclusions of Law, the proposed amendment is consistent with the applicable state and local criteria.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

The proposed TSP Amendment is consistent with the following goals and strategies in the 2040 Strategic Plan:

- Community Investment, Goal 5 - Plan, design and construct modern and efficient infrastructure in all areas and systems.

The current configuration at the intersection of Highway 99 and Scenic Avenue is skewed and has not been upgraded to urban standards. The proposed urban upgrade improvements are generally consistent with Goal 5 and specifically Strategy 3, which has to do with seeking partnerships with the County and ODOT to eliminate infrastructure deficiencies and build for the future. This proposed amendment will allow the City to capitalize on partnerships with ODOT and the County to fund needed improvements to the intersection.

- Responsible Governance, Goal 1 – Maintain a strong financial position that balances the need for adequate service levels and capital requirements against the affordability that is desired by our citizens.

Strategies 2 and 3 address working with and soliciting support from partnering agencies for the purpose of maximizing efficiency and maintaining a robust financial position. The proposed amendment supports the City's ability to partner with ODOT and the County to design and build improvements that maximize cost savings to City will providing improvements needed to minimize traffic and hazards and provide connectivity for vehicle, bicycle, pedestrian and railroad transportation modes.

- Vibrant Economy, Goal 1. Manage growth to provide a timely and orderly provision of facilities and services.

This goal recognizes the connection between transportation, land use and the City's ability to support a vibrant economy. The proposed amendment will allow an identified and needed project in the TSP to be built for the benefit of existing and new development within the City's UGB.

STAFF RECOMMENDATION:

Consider the second reading of the Ordinance Approving a Major Amendment to the Comprehensive Plan Adding Road Diet Improvements to Project No. 230 in the Transportation System Plan and forward it to a second reading without changes.

RECOMMENDED MOTION:

I move to approve Ordinance No. _____, an Ordinance Approving a Major Amendment to the Comprehensive Plan Adding Road Diet Improvements to Project No. 230 in the Transportation System Plan.

ATTACHMENTS:

1. TSP Amendment Ordinance (First Reading)
2. Exhibit 1 to CC Ordinance

ORDINANCE NO. _____

AN ORDINANCE AMENDING THE TRANSPORTATION SYSTEM PLAN (TSP) OF THE CENTRAL POINT COMPREHENSIVE PLAN TO ADD ROAD DIET IMPROVEMENTS TO PROJECT NO. 230.

Recitals:

- A. Words ~~lined through~~ are to be deleted and words **in bold** are added.
- B. The City of Central Point (City) is authorized under Oregon Revised Statute (ORS) Chapter 197 to prepare, adopt and revise comprehensive plans and implementing ordinances consistent with the Statewide Land Use Planning Goals.
- C. The City has coordinated its planning efforts with the State in accordance with ORS 197.040(2)(e) and OAR 660-030-0060 to assure compliance with goals and compatibility with City Comprehensive Plans.
- D. Pursuant to the requirements set forth in CPMC Chapter 17.96.100 Comprehensive Plan and Urban Growth Boundary Amendments – Purpose and Chapter 17.05.500, Type IV Review Procedures, the City has initiated an application and conducted the following duly advertised public hearings to consider the proposed amendment:
 - a) Planning Commission hearing on January 11, 2022
 - b) City Council hearing on January 27, 2022.

THE PEOPLE OF THE CITY OF CENTRAL POINT DO ORDAIN AS FOLLOWS:

Section 1. Amendments to TSP Chapter 7 – Street System, 2008-2030 to read:

Table 7.4 Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.

Section 2. Amendments to TSP Chapter 12 – Transportation System Financing Program to read:

Table 12.5. Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.

Section 3. Evidence. Based on all the information received, the City Council adopts Planning Commission Resolution No. 892 and its attachments (Exhibit 1) as evidence that justifies adoption of the TSP Amendment as set forth in Sections 1 and 2 of this Ordinance.

Section 4. The City Manager is directed to conduct post acknowledgement procedures defined in ORS 197.610 et seq. upon adoption of the TSP Amendment.

Section 5. Effective Date. The Central Point City Charter states that an ordinance enacted by the council shall take effect on the thirtieth day after its enactment. The effective date of this ordinance will be the thirtieth day after the second reading.

Passed by the Council and signed by me in authentication of its passage this _____ day of _____, 20____.

Mayor Hank Williams

ATTEST:

City Recorder

PLANNING COMMISSION RESOLUTION NO. 892

A RESOLUTION OF THE PLANNING COMMISSION FORWARDING A FAVORABLE RECOMMENDATION TO THE CITY COUNCIL TO AMEND THE COMPREHENSIVE PLAN TO ADD ROAD DIET IMPROVEMENTS TO PROJECT NO. 230 IN THE TRANSPORTATION SYSTEM PLAN

(File No: CPA-20001)

WHEREAS, on January 11, 2022 the City of Central Point Planning Commission held a duly-noticed public hearing, reviewed staff reports, findings of fact and heard public testimony on a Major Revision to the Central Point Comprehensive Plan, Transportation System Plan; and

WHEREAS, the Planning Commissions determined that the revisions as proposed were in the public interest and that the general welfare of the public will benefit by the proposed revisions to improve safety at the Highway 99/Scenic Avenue intersection; and

WHEREAS, after reviewing the requested proposal and considering public testimony it is the determination of the Central Point Planning Commission that the proposed amendment as set forth in attached Exhibit "A" dated January 11, 2022 are adjustments that do not alter, or otherwise modify the uses and character of development and land use within the City of Central Point, and is therefore determined to be consistent with all of the goals, objectives, and policies of the City's Comprehensive Plan and State Planning Goals.

NOW, THEREFORE, BE IT RESOLVED that the City of Central Point Planning Commission by Resolution No. 892 does hereby accept, and forward to the City Council a recommendation that the City Council favorably consider amending the City of Central Point Comprehensive Plan, Transportation System Plan (TSP) as set forth in the attached Exhibit "A" including Attachments "A" through "E" provided therein.

PASSED by the Planning Commission and signed by me in authentication of its passage this 11th day of January, 2022.


Planning Commission Chair

ATTEST:


City Representative

Approved by me this 11th day of January, 2022.


Planning Commission Chair

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



Staff Report

Transportation System Plan (TSP) Amendment (Project No. 230)
File No. CPA-20001

January 11, 2022

Item Summary

Consideration of a Comprehensive Plan Text Amendment to the Transportation System Plan (TSP) concerning improvements at and around Scenic Avenue and Highway 99 (Project No. 230). Applicant: City of Central Point.

Staff Source

Stephanie Holtey, Planning Director

Background

The Oregon Department of Transportation applied and received funding through the All Road Transportation Safety (ARTS) Program to signalize the intersection at Highway 99 and Scenic Avenue and apply a road diet (i.e. 5 lanes to 3 lanes with a center turn lane from the Exit 35 interchange (MP 0.35) to MP 1.95 just south of Brookhaven Drive) (Attachment "A" and "C"). This project addresses known traffic safety issues by reducing the number and severity of traffic accidents at this location (Attachment "B"). The project is consistent with the City of Central Point TSP Project No. 230 with the exception of the road diet improvements. Based on current volumes and the five (5) lane configuration on Highway 99, signal warrants are not met. Adding the road diet will allow signal warrants to be met as needed for the project to proceed. At the request of ODOT, staff introduced a proposed revision to the TSP at the October 5th Planning Commission meeting to add the road diet to Project No. 230. After asking questions and expressing some concerns, the Planning Commission directed staff to schedule a public hearing to consider the proposed TSP Amendment.

At the January 11, 2022 Planning Commission meeting staff will present the proposed amendment to the TSP for a public hearing. The proposed revision is a Major Revision to the Central Point Comprehensive Plan. At the conclusion of the public hearing, the Planning Commission will consider the revision relative to the approval criteria in CPMC 17.96.500 and make a recommendation to the City Council for final decision.

Issues

At the October 5, 2021 Planning Commission meeting there was discussion and some concern that applying a road diet would worsen the ability of residents to evacuate the city in the event of wildfire. There were also concerns relating to heavy traffic volumes on Highway 99, particularly related to incoming traffic from Highway 62. Provided below is information related to the issues of evacuation safety and traffic volume on Highway 99.

Evacuation: The proposed TSP Amendment adding a road diet will reduce the number of lanes in the city limits but not the pavement width. Based on consultation with Public Works, the full pavement width can be used during times of emergency to evacuate residents if necessary. The City is in the process of updating its TSP. As part of that planning effort, the consultant will be looking at transportation needs and mitigation measures during times of emergency to look at community concerns related to wildfire evacuations. In the meantime, the City has created Neighborhood Evacuation Zones to phase evacuations based on location to minimize congestion and facilitate more orderly traffic movements during emergencies.

Traffic Volume: ODOT prepared a Traffic Report that looks that traffic volumes on Highway 99 now and in 2040 (Attachment "D"). The report found that there are 6,800 Average Daily Trips currently with 18.5% being heavy vehicle traffic. In 2040 this is forecast to increase to 8,870 Average Daily Trips. This is much less than other arterials in the City. Public Works reported that Pine Street has 21,000; Beall Lane has 15,000 and Twin Creeks Crossing has 5,000 Average Daily Trips. Based on this data, the impacts from traffic volume are not likely to negatively impact this transportation facility in the next 20-years. However, if the conditions change, the Highway 99 can be reconfigured back to five (5) lanes.

Findings of Fact & Conclusions of Law

The Major Comprehensive Plan Amendment to the TSP has been evaluated against the applicable criteria set forth in CPMC 17.96 and found to comply as evidenced in the Planning Department Findings of Fact and Conclusions of Law (Attachments "D").

Attachments

- Attachment "A" – Proposed TSP Amendment (Tables 7.4 and 12.5)
- Attachment "B" – Project Location Map
- Attachment "C" – Proposed Project Plans (Sheets A01, QB01-QB10)
- Attachment "D" – ODOT Traffic Report
- Attachment "E" – Planning Department Findings of Fact and Conclusions of Law
- Attachment "F" – Draft Planning Commission Resolution No. 892

Action

Consider the proposed Major Comprehensive Plan Amendment to the TSP and 1) approve; 2) approve with revisions; or 3) deny the application.

Recommendation

Approve Resolution No. 892 recommending the City Council approve the TSP Amendment without revisions per the Staff Report dated January 11, 2022 including all attachments.

ATTACHMENT "A" – Proposed Transportation System Plan Revisions

Table 7.4, Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Year	ODOT	County	Central Point	Medford	Other
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	•	•	•					√			√	2012	♦	♦	♦		

Table 12.5, Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Tier	ODOT	County	Central Point	Medford	Other	Total Project Cost
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	■	■	■					√			√	Tier 1, Long	♦	♦	♦			\$2,737,300

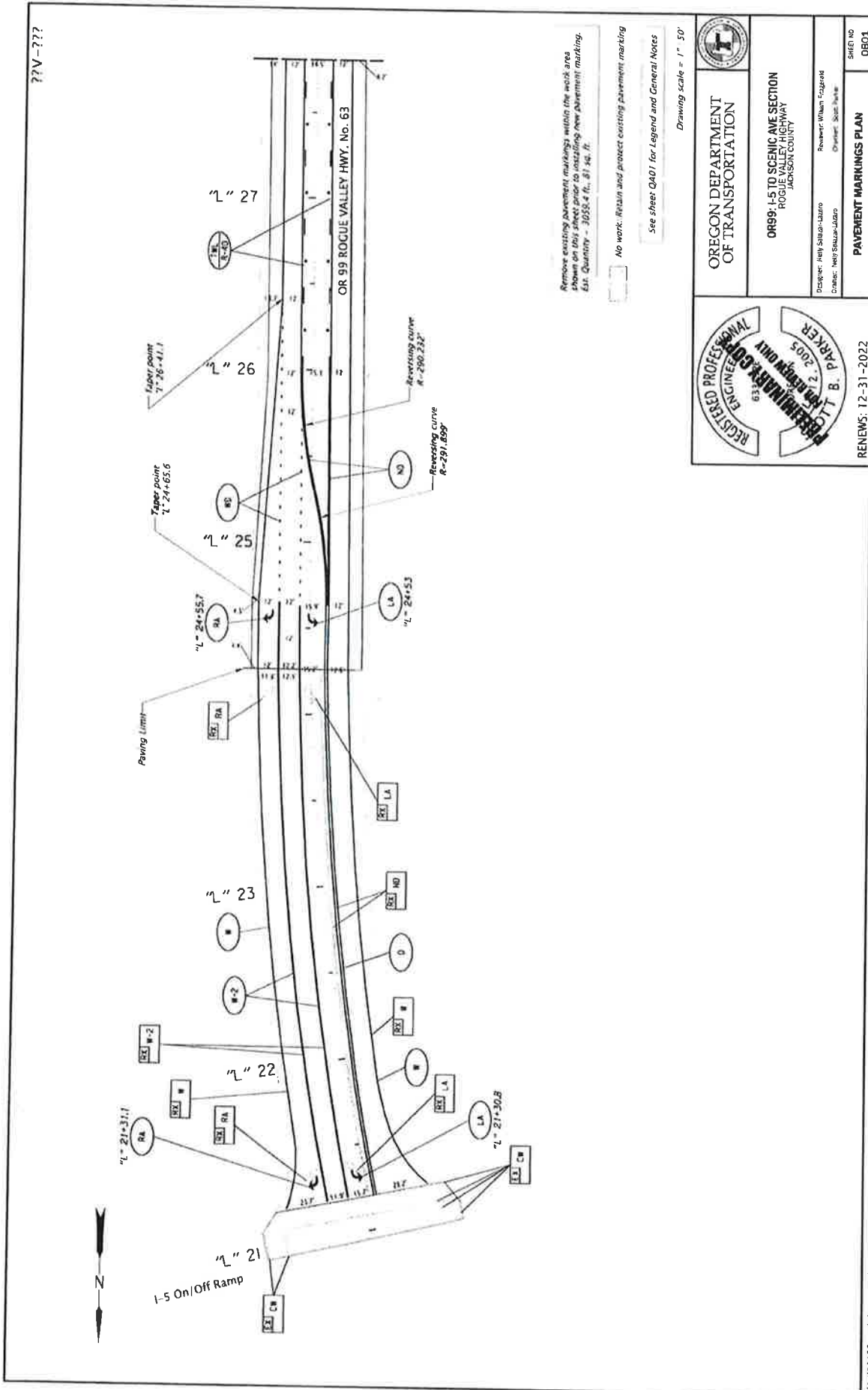


TSP Amendment: Project No. 230 (Highway 99/Scenic Avenue)

ATTACHMENT "B"

Project Location Map (Approximate)
CPA-20001

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



OREGON DEPARTMENT OF TRANSPORTATION

0899: I-5 TO SCENIC AVE SECTION
ROGUE VALLEY HIGHWAY
JACKSON COUNTY

DESIGNER: Amy Salomon-Laurio
CHECKER: Amy Salomon-Laurio
REVIEWER: William Fitzgerald
DRAWN BY: Amy Salomon-Laurio

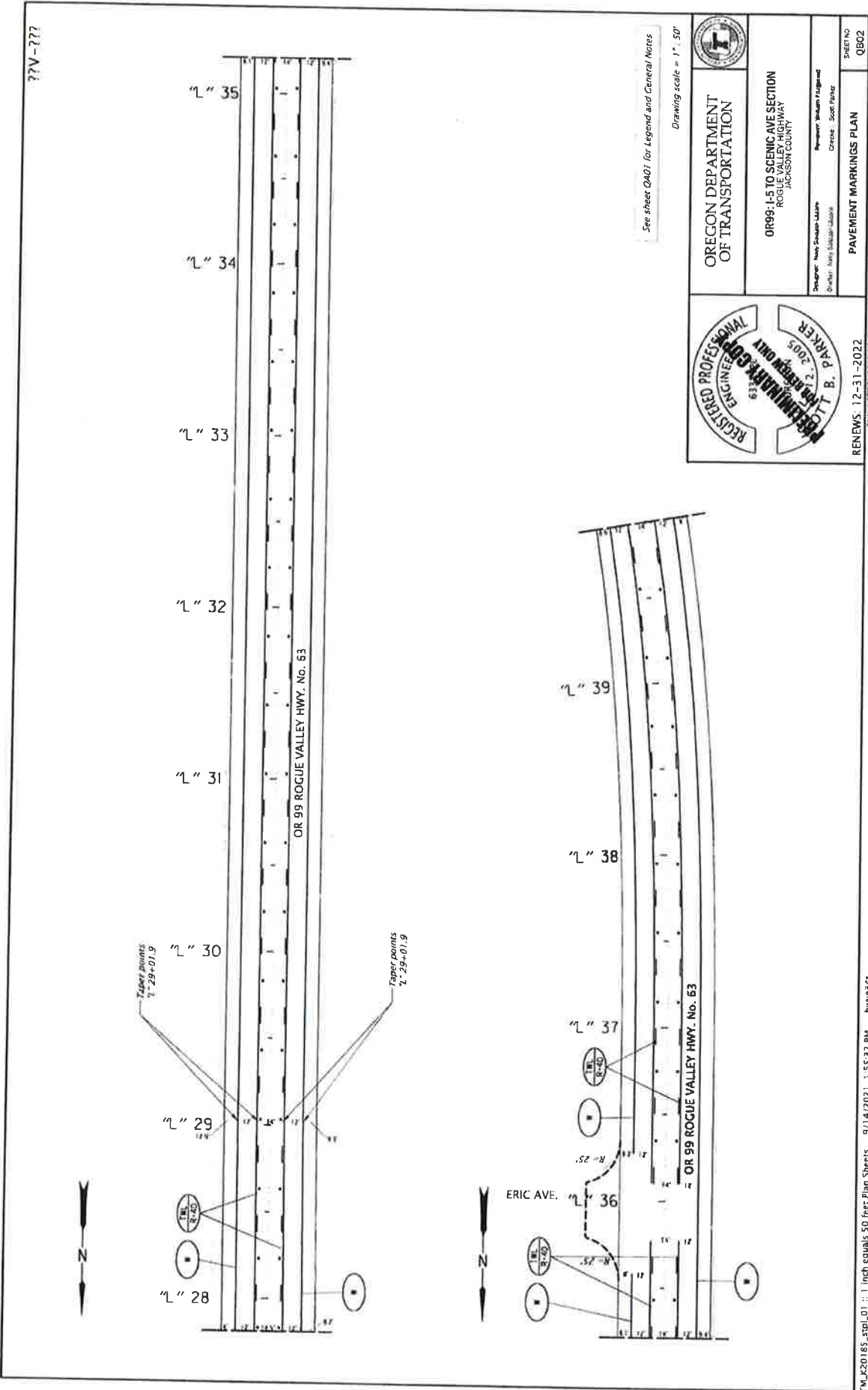
REGISTERED PROFESSIONAL ENGINEER
No. 11877
EXPIRES 12-31-2022
PARKER ENGINEERING, INC.
1000 ELECTRONIC DRIVE
JACKSONVILLE, FLORIDA 32218

PAVEMENT MARKINGS PLAN
SHEET NO. Q801

Scale: 1" = 50'
Rotation: 0°

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Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



See sheet Q407 for Legend and General Notes

Drawing scale = 1" = 50'

REGISTERED PROFESSIONAL ENGINEER
 LICENSE NO. 12-31-2022
 B. PARKER
 2005

RENEWALS: 12-31-2022

OREGON DEPARTMENT OF TRANSPORTATION

OR99: L5 TO SCENIC AVE SECTION
 ROGUE VALLEY HIGHWAY
 JACKSON COUNTY

Prepared: William Polglund
 Checked: Scott Parker

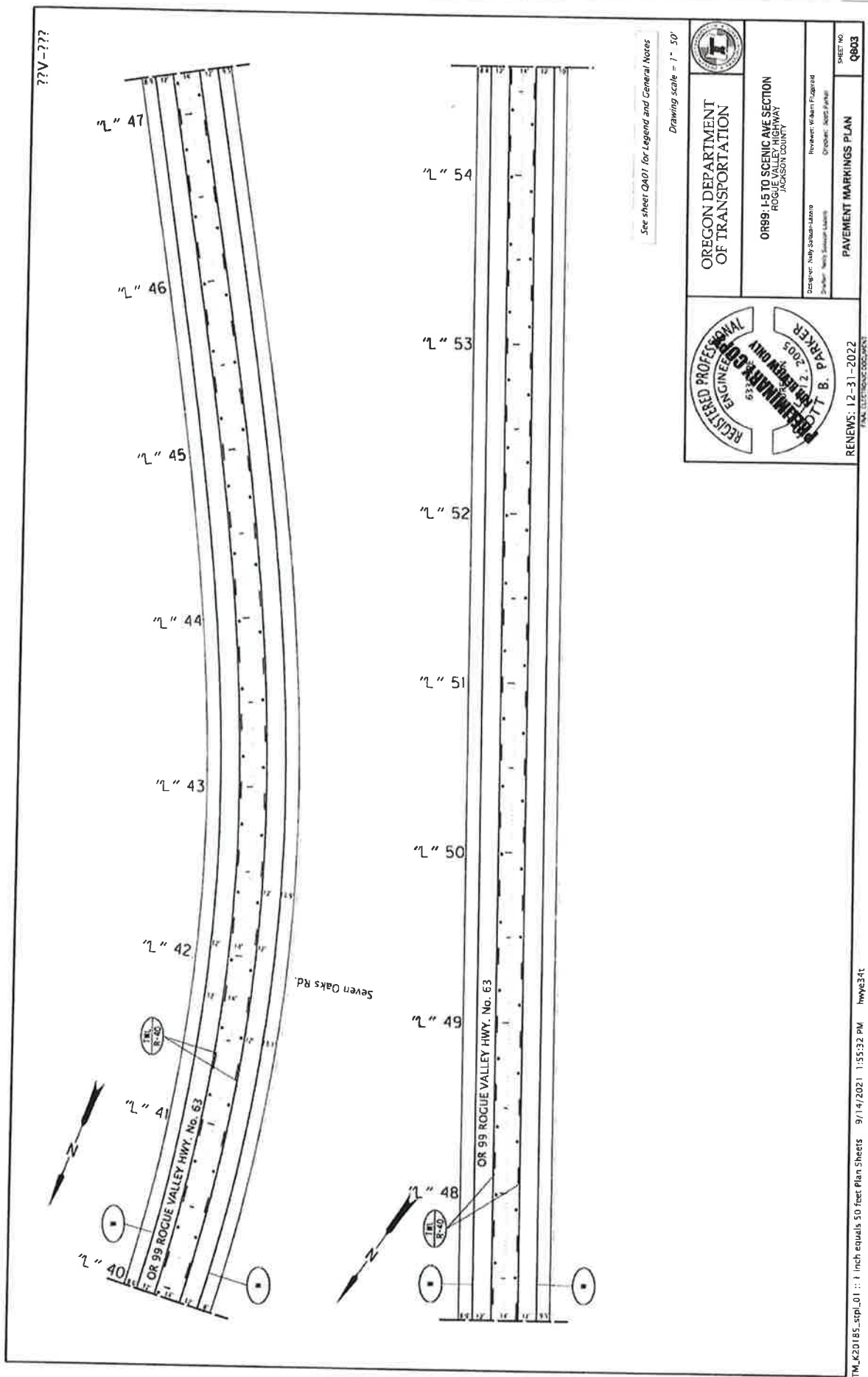
PAVEMENT MARKINGS PLAN

SHEET NO. Q802

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Rotation: 0°

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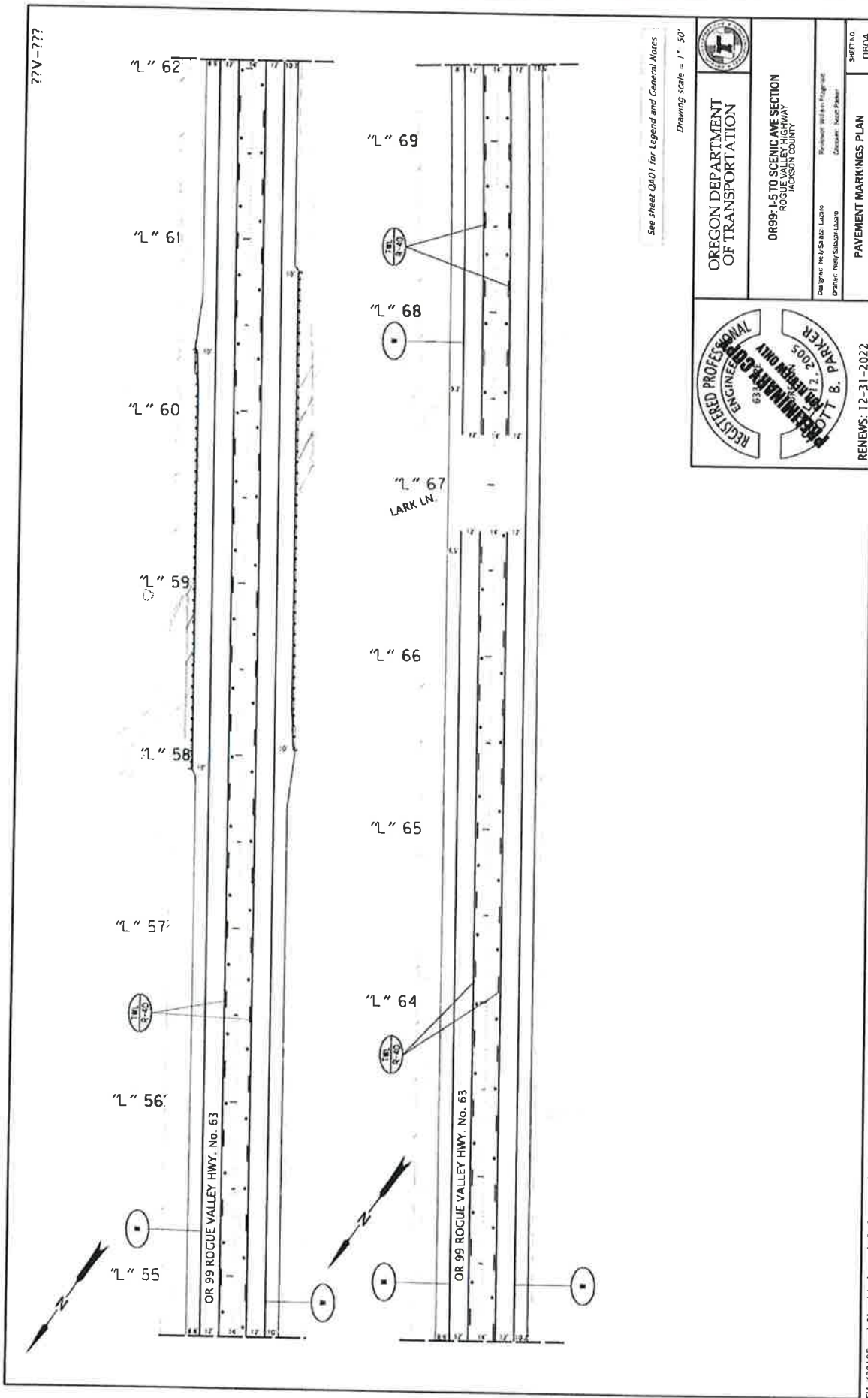


See sheet Q407 for Legend and General Notes
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	REGISTERED PROFESSIONAL ENGINEER WILLIAM J. B. PARKER No. 63222 STATE OF OREGON	OREGON DEPARTMENT OF TRANSPORTATION OR 99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JOHNSON COUNTY	SHEET NO. Q803
	RENEWS: 12-31-2022 <small>FINAL DESIGN DOCUMENT APPROVED FOR RELEASE</small>	Designer: Nelly Salazar-Luonno Drafter: Nelly Salazar-Luonno	Reviewer: William J. Parker Checker: JACOB PARKER

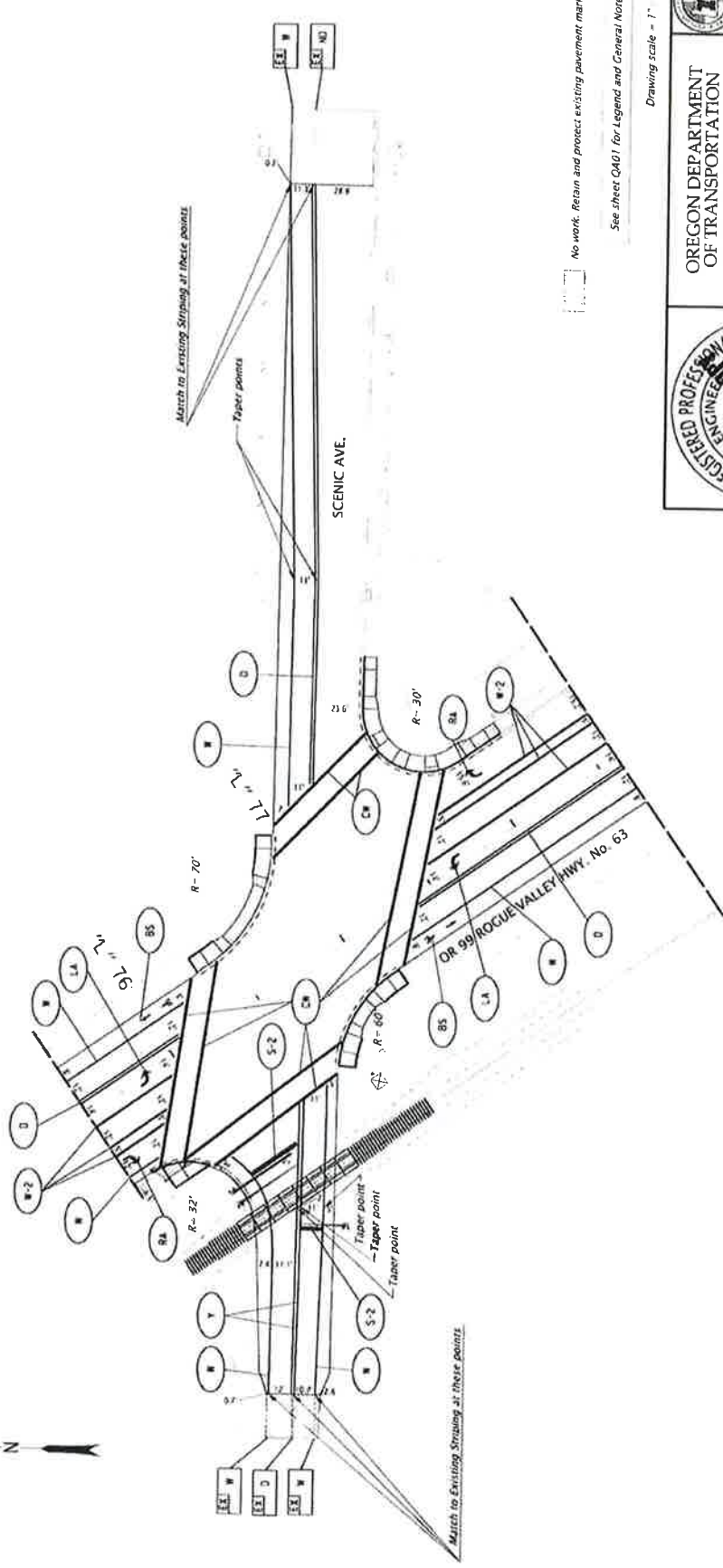
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Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))



Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

??V-???



See sheet Q401 for Legend and General Notes

Drawing scale = 1" = 50'

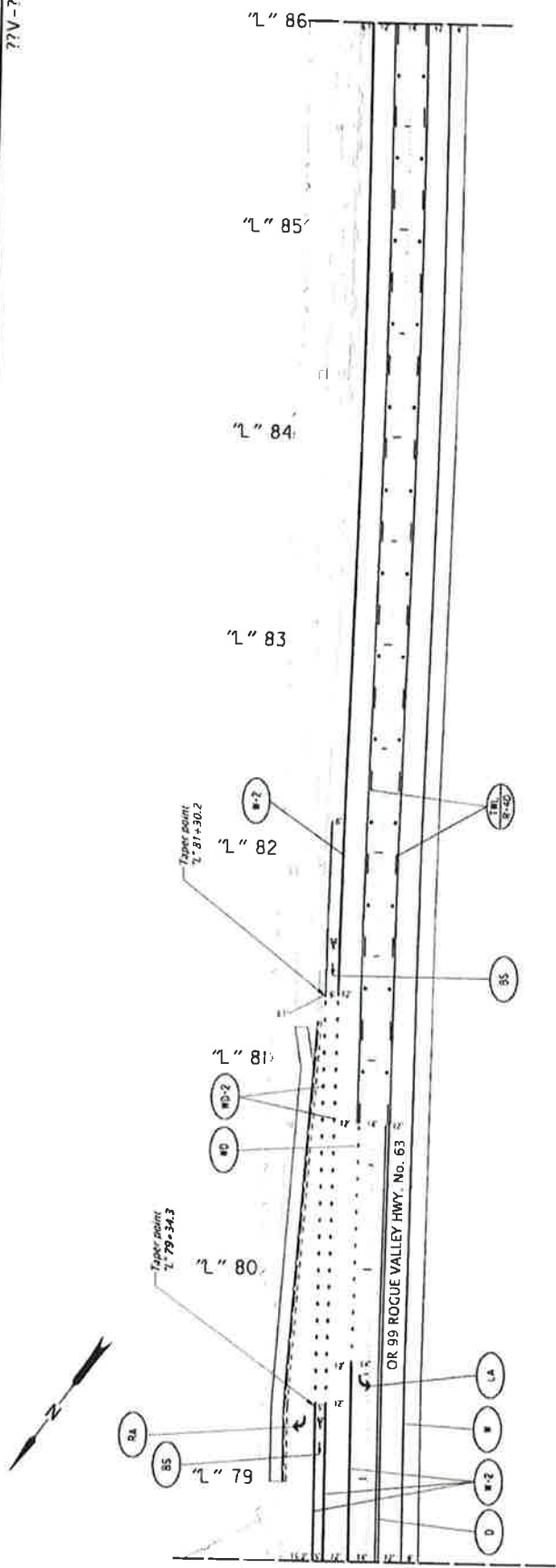
	OREGON DEPARTMENT OF TRANSPORTATION
	OR99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY
Designer: Nelly Salazar-Lopez Drafter: Leahy Salazar-Lopez	Reviewer: William Fitzgerald Checker: Scott Phelan
RENEWS: 12-31-2022 <small>ALL LICENSES EXPIRE ON 12/31/2022</small>	SHEET NO. Q405 PAVEMENT MARKINGS PLAN

Rotation: 0° Scale: 1" = 50'

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Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

??V-???



See sheet Q401 for Legend and General Notes

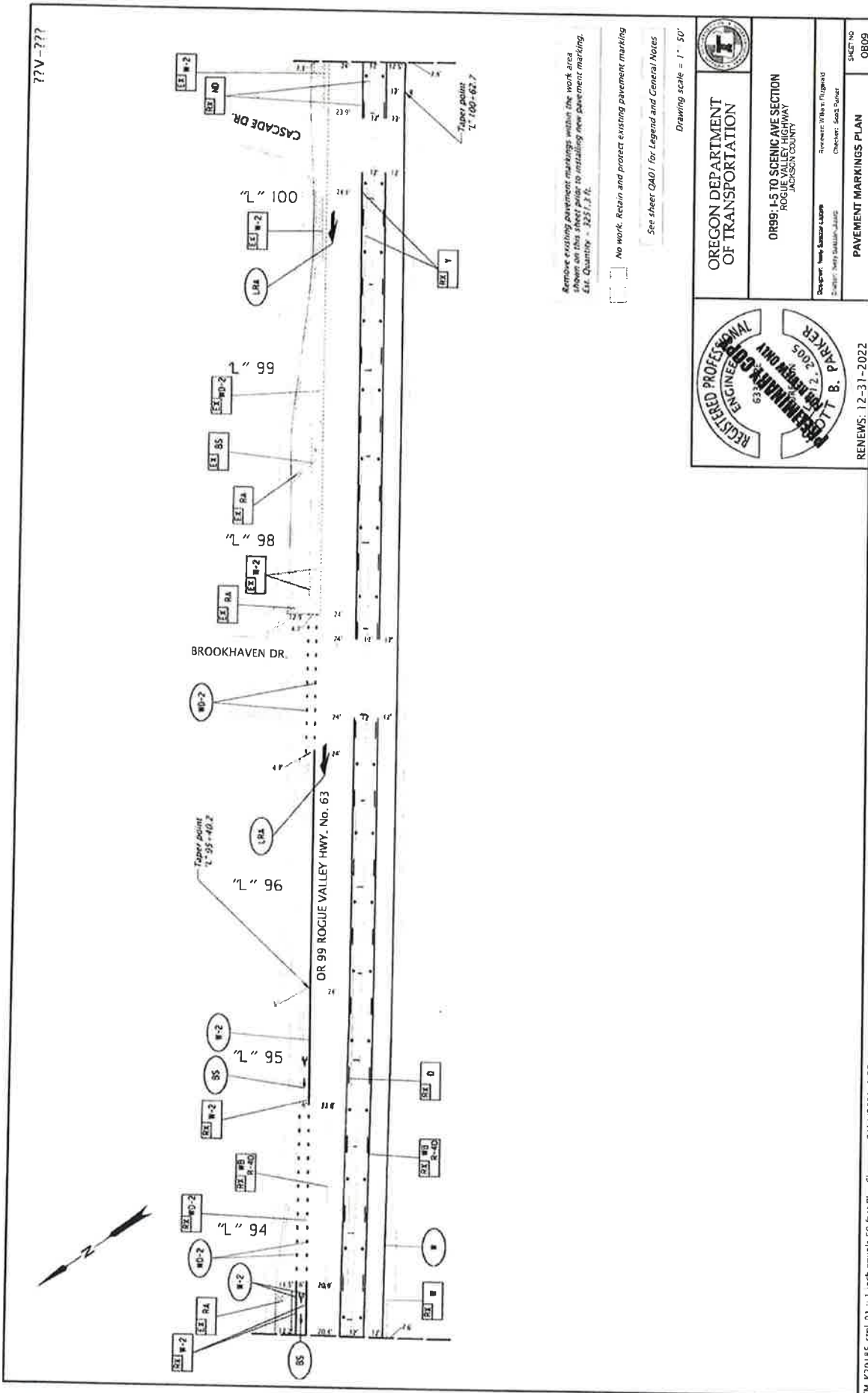
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	OR99 - LE TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY
Designer: Brett Parker Checker: Scott Pinner	Designer: Alan Ferguson Checker: Scott Pinner
PAVEMENT MARKINGS PLAN	
SHEET NO. Q807	

Rotation: 0° Scale: 1" = 50'

REVISIONS: 12-31-2022
FINAL ELECTRONIC SUBMITTAL
DATEABLE PDF REQUIRED

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Remove existing pavement markings within the work area shown on this sheet prior to installing new pavement marking. Est. Quantity - 3251.3 ft.

No work. Retain and protect existing pavement marking

See sheet Q401 for Legend and General Notes

Drawing scale = 1" = 50'

<p>OREGON DEPARTMENT OF TRANSPORTATION</p>	<p>DR99: I-5 TO SCENIC AVE SECTION ROGUE VALLEY HIGHWAY JACKSON COUNTY</p> <p>Designer: New, Sussler Labore Reviewer: William Fitzgerald Checker: Scott Parker</p>		<p>PAVEMENT MARKINGS PLAN</p>	<p>SHEET NO QB09</p>
			<p>RENEWS: 12-31-2022</p>	<p>Scale: 1"=50'</p>

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Oregon

Kate Brown, Governor

Department of Transportation

**Region 3 Traffic
District # 8**

100 Antelope Rd
White City, OR 97502
Phone 541-774-6359

Traffic Report

**OR 99: I-5 to Scenic
Rogue Valley Highway No. 63 M.P. 0.42 – 1.64
KN 20185
Jackson**

Prepared By:

William Fitzgerald, PE
Traffic Operation Engineer



Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

Traffic Data

On OR 99, there are two lanes in each direction of travel between MP 0.42 and 1.64. Average Annual Daily Traffic (AADT) in this section is 6800 vehicles with approximately 18.5% heavy vehicles. Future (2040) AADT is projected to be 8870 vehicles. The project limits are shown in the figure below.



Figure 1 Area Map.

Introduction

This project will install a road diet along OR 99 throughout this section of highway. There will be a grind inlay to facilitate the road diet. A signal will be installed at the intersection of OR 99 @ Scenic Ave. This project is a safety project due to the number of severe crashes that have been occurring along this section of highway. This is an ARTS funded safety project. Traffic section control The Example Project brief scoping level description including type of work (paving, etc.), reason for project and other current known details. Indicate funding source, and especially emphasize if project is a safety related project and the Traffic section's involvement/intentions.

Safety Analysis

In the past 10 years there have been 37 recorded crashes within the project limits, a summary of crash types and injury severity are shown in the table below. Leading crash causes included: *Almost all of the crashes were due to failure to yield ROW.*

Crash Severity	
Fatal	1
Inj A	3
Inj B	15
Inj C	6
No<5	0
PDO	12

Crash Types	
Fixed Object	6
Sideswipe-Meeting	1
Rear-End	1
Non Collision	0
Other	0
Angle	19
Turning Movement	10
Head-On	0
Parking Maneuver	0
Pedestrian	0

The majority of these crashes occurred at the Scenic @ OR 99 intersection. The failure to yield ROW cause of the crashes occur when drivers take inadequate gaps in traffic. Crashes were occurred from 5 am to 10 pm with spikes at 8 am and 1pm to 5 pm. These spikes are likely due to peak hour traffic and peak school traffic. Scenic Middle school is very close by which leads to peak queuing in the morning and afternoon. The signal portion of this project will reduce the angle type crashes by 67%. All but one of the Injury A crashes are angle type and the fatal is angle type, so the installation of the signal should reduce the severity of crashes as well. The road diet will help reduce all crashes on this section of roadway by about 29%. Considering the improvements being installed this project should help reduce the number of crashes on this section of roadway.

Traffic Operations Analysis

Existing year traffic operates at a Level of Service (LOS) A on the highway straight away sections and LOS F and a V/C of 1.42 for the Scenic Ave intersection during the peak hours. The LOS F for the Scenic Ave intersection is due traffic peaking on Scenic Ave during the AM, School and PM peak hours. Traffic has a difficult time existing Scenic onto OR 99 due to the low number of gaps, high speed of the highway and number of lanes. This problem has led to the inclusion of a traffic signal in this project. An analysis of the intersection shows that a traffic signal is not warranted with the existing 5 lane section. Due to the road diet being included in the project the warrant analysis can be completed using the proposed three lane section. A traffic signal is warranted with the proposed three lane section. With a traffic signal installed at the intersection of OR 99 and Scenic Ave the LOS improves to B and with a V/C ratio of 0.70. The LOS of the straight away section is reduced from LOS A to B with the road diet. Future year (2040) traffic is expected to operate at a LOS B with a B/C ratio of 0.77

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

at the intersection of Scenic Ave and OR 99. The addition of the traffic signal will reduce the LOS and V/C ratio for both existing and future conditions at the intersection of Scenic Ave and OR 99. The existing straight away section will have slightly lowered LOS with a slightly higher V/C but still within acceptable 20 year design life. Table 1 below provides the V/C, LOS, and 95th queue length for build, no build conditions and provides the same data for the future year (2040) scenario.

Table 1						
No Build						
Direction	2020			2040		
	V/C	LOS	95th Queuing	V/C	LOS	95th Queuing
WBL	1.54	F	580	4.8	F	840
WBT						
WBR						
EBL	0.77	F	100	1.64	F	170
EBT						
EBR						
NBL	0.06	A	10	0.08	A	20
NBT	~		~	~		~
NBR	~		~	~		~
SBL	0.08	A	10	0.11	A	20
SBT	~		~	~		~
SBR	~		~	~		~
Road Diet w/Signal						
Direction	2020			2040		
	V/C	LOS	95th Queuing	V/C	LOS	95th Queuing
WBL	0.55	B	80	0.66	B	210
WBT						
WBR						
EBL	0.26	B	45	0.31	B	100
EBT						
EBR						
NBL	0.14	B	10	0.19	B	35
NBT	0.70		100	0.77		160
NBR	0.27		40	0.30		45
SBL	0.21	B	30	0.30	B	40
SBT	0.50		90	0.56		120
SBR	0.05		25	0.06		25

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

Traffic Control

Traffic volumes allow for the roadway to be reduced to a single lane with alternating bidirectional traffic during the day, recommended lane restrictions are shown below.

Recommended Lane Restrictions – 00220.40(e)(1)

1. Closed Lanes – One traffic lane may be closed in each direction when allowed, shown, or directed during the following periods of time except as indicated in 00220.40(e-2).

- 7:00 p.m. Sunday through 3:00 p.m. Friday

Estimated Delay:

- *<5 minutes of average additional travel time is expected when following the closure recommendations.*

Conclusion

There are safety and capacity concerns at the intersection of OR 99 and Scenic Ave. Crash data revealed there are many angle and turning crashes that are occurring on this section of highway. The addition of a traffic signal and road diet will reduce the number of those types of crashes. The side street traffic entering the highway from Scenic Ave has a difficult time finding a gap. A traffic signal will allow safe gaps in highway traffic to allow traffic to exit Scenic Ave. The mainline traffic on OR 99 will not have queuing, LOS or capacity concerns with the addition of a traffic signal. With the addition of the road diet work may occur during day hours, weekly.

ATTACHMENT " E "

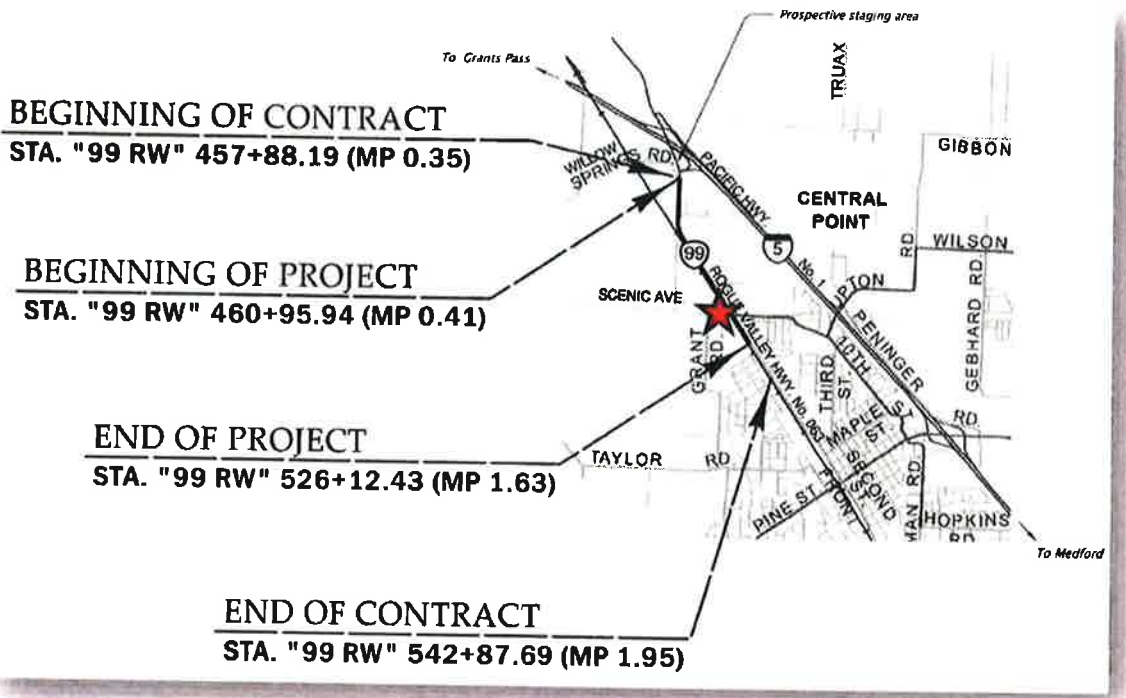


Findings of Fact & Conclusions of Law
 Major Comprehensive Plan Amendment: TSP Project No. 230
 Applicant: City of Central Point
 File No. CPA-20001

I. Introduction

The City of Central Point ("City") is proposing a Major Revision to the Transportation System Plan (TSP), an Element of the Comprehensive Plan ("TSP Amendment"). The proposed revision adds road diet improvements to Highway 99 as part of Project No. 230. As currently written, Project No 230 is to install a traffic signal at the Scenic Avenue intersection with Highway 99 when warrants are met.

Figure 1 - Project Location



The purpose of this project is to meet signal warrants as needed to install a traffic signal at the intersection of Scenic Avenue and Highway 99 to reduce the number and severity of traffic accidents at that location. The road diet, including tapering improvements, will extend from the Interstate 5 Exit 35 (Mile Post (MP) 0.35 to just south of Cascade Drive in the city limits (MP

Attachment: Exhibit 1 to CC Ordinance (2022-7 : Transportation System Plan Amendment (Project No. 230 - Scenic Ave/Hwy 99))

1.95). Most of the project is along portions of Highway 99 that are owned and operated by the Oregon Department of Transportation (ODOT).

The proposed TSP Amendment is 1.6 miles in length and crosses jurisdictional boundaries. Based on the scope of the project and the potential impacts beyond the Scenic Avenue intersection, it is considered a Major Revision in accordance with CPMC 17.96.300. The TSP Amendment has been processed using Type IV (Legislative) procedures set forth in CPMC 17.05.500.

These Findings of Fact and Conclusions of Law are presented in six (6) parts as follows:

1. Introduction
2. Statewide Planning Goals
3. Transportation Planning Rule
4. Central Point Comprehensive Plan
5. CPMC 17.96.500
6. Summary Conclusion

II. Statewide Planning Goals

Major Comprehensive Plan Amendments must address compliance with the Statewide Planning Goals. Due to Central Point's geographic location outside the Willamette Valley and coastal areas, Goals 15-19 do not apply and are not addressed further. Findings relative to the TSP Amendment are set forth below.

Goal 1 – Citizen Involvement

To develop a citizen involvement program that insures the opportunity for citizens to be involved in all phases of the planning process.

Finding, Goal 1: The proposed TSP Amendment was developed and processed in accordance with the City's citizen involvement program. The Citizen's Advisory Committee (CAC) considered a possible TSP Amendment on August 11, 2020 early in the planning process. At that time, the CAC unanimously forwarded a recommendation to the Planning Commission to consider a TSP Amendment necessary to improve safety along this corridor and at the Scenic Avenue and Highway 99 intersection. On October 5, 2021, staff presented a proposed TSP Amendment to the Planning Commission at which time the Planning Commission voted to direct staff to schedule a public hearing. A duly noticed legislative hearing was held on January 11, 2022 and January 27, 2022. Notice was published in the Mail Tribune and mailed to property owners within 250-feet of the intersection.

Conclusion, Goal 1: Consistent.

Goal 2 – Land Use

To establish a land use planning process and policy framework as a basis for all decision and actions related to use of land and to assure an adequate factual basis for such decisions and actions.

Finding, Goal 2: The TSP amendment follows the land use review procedures outlined in Central Point Municipal Code and these findings provide an adequate factual basis for action. The TSP amendment therefore conforms to the established land use planning process and framework consistent with Goal 2.

Conclusion, Goal 2: Consistent.

Goal 3 – Agricultural Lands

To preserve and maintain agricultural lands.

Finding, Goal 3: The proposed TSP amendment does not involve or otherwise affect lands designated for agricultural use.

Conclusion, Goal 3: Not applicable.

Goal 4 – Forest Lands

To conserve forest lands by maintaining the forest land base and to protect the state's forest economy by making possible economically efficient forest practices that assure the continuous growing and harvesting of forest tree species as the leading use on forest land consistent with sound management of soil, air, water, and fish and wildlife resources and to provide for recreational opportunities and agriculture.

Finding, Goal 4: The proposed TSP amendment does not involve or otherwise affect lands designated for forest use.

Conclusion, Goal 4: Not applicable.

Goal 5 – Open Space, Scenic and Historic Areas, and Natural Resources

To protect natural resources and conserve scenic and historic areas and open spaces.

Finding, Goal 5: Public road projects are required to comply with provisions under the Endangered Species Act (ESA) for aquatic species that exceed the Goal 5 and associated Central Point Municipal Code requirements for riparian protection. As such, by complying with the ESA, such public road projects also will adequately address riparian protections otherwise required by regulations applicable to riparian area development. The treatment of other resources regulated under Goal 5 will not change because of the TSP amendment, and therefore the goal is otherwise not relevant to this transportation system plan amendment.

Conclusion, Goal 5: Consistent.

Goal 6 – Air, Water and Land Resources Quality

To maintain and improve the quality of the air, water and land resources of the state.

Finding, Goal 6: The proposed TSP amendment does not involve or otherwise affect local, state, or federal regulations managing the quality of air, water and land resources.

Conclusion, Goal 6: Not applicable.

Goal 7 – Areas Subject to natural Hazards and Disasters.

To protect people and property from natural hazards.

Finding, Goal 7: The proposed TSP amendment does not involve or otherwise affect regulations protecting the citizens of Central Point from natural hazards.

Conclusion, Goal 7: Not applicable.

Goal 8 – Recreation Needs

To satisfy the recreational needs of the citizens of the state and visitors and, where appropriate, to provide for the siting of necessary recreational facilities.

Finding, Goal 8: The proposed TSP amendment does not involve or otherwise affect the City's provision of necessary recreational facilities.

Conclusion, Goal 8: Not applicable.

Goal 9 – Economy of the State

To provide adequate opportunities throughout the state for a variety of economic activities vital to the health, welfare and prosperity of Oregon's citizens.

Finding, Goal 9: The proposed TSP amendment will add a lane reconfiguration project to an already listed signal project and will provide for the continued orderly development of the City's road network, which is a vital infrastructure component necessary to support continued economic development.

Conclusion, Goal 9: Consistent.

Goal 10 – Housing

To provide for the housing needs of citizens of the state.

Finding, Goal 10: The proposed TSP amendment does not involve or otherwise affect the City's ability to provide needed housing to the citizens of Central Point or the state of Oregon.

Conclusion, Goal 10: Not applicable.

Goal 11 – Public Facilities and Services

To plan and develop a timely, orderly and efficient arrangement of public facilities and services to serve as a framework for urban and rural development.

Finding, Goal 11: Transportation facilities are identified as public facilities under this goal. It is acknowledged by the TSP, by reference, is part of the City's Comprehensive Plan Public Facilities Element. The addition of the lane reconfiguration project allows for orderly and timely provision of needed transportation facilities to provide for safe and reliable transportation along Highway 99 and in particular at the intersection with Scenic Avenue.

Conclusion, Goal 11: Consistent.

Goal 12 – Transportation

To provide and encourage a safe, convenient and economic transportation system.

Finding, Goal 12: Transportation, directs state and local jurisdictions "to provide and encourage a safe, convenient and economic transportation system." It establishes that a transportation plan consider all modes of transportation, be based upon an inventory of local, regional and state transportation needs, consider the differences in social consequences that would result from utilizing differing combinations of transportation

modes, avoid principal reliance upon any one mode of transportation, minimize adverse social, economic and environmental impacts and costs, conserve energy, meet the needs of the transportation disadvantaged by improving transportation services, facilitate the flow of goods and services so as to strengthen the local and regional economy, and conform with local and regional comprehensive land use plans.

The City's TSP (2008) currently lists the signal at OR 99 and Scenic Avenue as Project No. 230, when signal warrants are met. The amendment to the TSP is to include the road diet from MP .42 to MP 1.64. The Traffic Report indicated that the traffic signal is not warranted with the existing 5-lane section of roadway. However, with the road diet a traffic signal is warranted. The functional classification of OR 99 through this section remains as an Urban Principal Arterial. ODOT completed a Traffic Report that supports the road diet. The Traffic Report concluded that the addition of a traffic signal and road diet would reduce the number of angle and turning crashes that are occurring on this section of OR 99. The Report concluded that the mainline traffic on OR 99 would not have queuing (LOS or capacity) concerns with the addition of the traffic signal or the road diet.

Conclusion, Goal 12: Consistent.

Goal 13 – Energy

To conserve energy.

Finding, Goal 13: The proposed TSP amendment does not involve, or otherwise affect development standards or regulations that address conservation of energy.

Conclusion, Goal 13: Not applicable.

Goal 14 – Urbanization

To provide for an orderly and efficient transition from rural to urban land use, to accommodate urban population and urban employment inside urban growth boundaries, to ensure efficient use of land, and to provide for livable communities.

Finding, Goal 13: The proposed TSP amendment addresses the multi-modal transportation needs for a project that is already listed in the TSP. The proposed project will address transportation for all modes by addressing the lack of bicycle lanes in the urban area.

Conclusion, Goal 13: Consistent.

III. Transportation Planning Rule (OAR 660-012)

The purpose of the Transportation Planning Rule (TPR) is to facilitate implementation of Statewide Planning Goal 12 (Transportation). It sets forth the requirements for preparation of local TSPs.

Finding OAR 660-012: The City's adopted TSP (Ordinance No. 1922) is based on and complies with the TPR. The proposed amendment is limited to the addition of a lane reconfiguration improvements as part of Project No. 230, which includes installing a signal at the intersection of Highway 99 and Scenic Avenue when warrants are met. The proposed amendment is in response to a Traffic Report that found significant safety concerns at the intersection of Highway 99 and Scenic Avenue and a finding that the signal installation is not warranted with the existing five (5) lane configuration of Highway 99. The proposed addition of a

road diet will allow signal warrants to be met as needed to address a project identified in the TSP. The Traffic Report found that the impacted transportation facilities will not experience any change in functional classification. The proposed amendment will improve intersection performance and reduce angle and turning movement collisions that have resulted in 1 fatality and several injuries in the past ten (10) years. Finally the proposed revision implements Project KN 20185 in the State Transportation Improvement Plan (STIP) and is consistent with the Regional Transportation Plan and Jackson County Transportation Plan. As such, the City finds that there are no changes the TSP's compliance with OAR 660-012 and that no further discussion of TPR compliance is necessary.

Conclusion: OAR 660-012. The proposed TSP Amendment adding the road diet to Project No. 230 does not alter the TSPs compliance with OAR 660-012 and the proposed amendment is based on technical data that resolves a safety issue and allows an existing TSP project (230, intersection signal installation) to be implemented.

IV. Central Point Comprehensive Plan

The City's Comprehensive Plan addresses compliance with Statewide Planning Goals and establishes the goals and polices to carry out the City's mission and vision as articulated in the City's Strategic Plan. The adopted TSP (Ordinance No. 1922) was prepared based on the land use classification and distribution in the City's Land Use Element of the Comprehensive Plan. The proposed amendment addresses transportation facilities in the TSP. Applicable policies are limited to 5.1.1 and 7.1.11 in the TSP, which is part of the Public Facilities Element. These are addressed below:

TSP Policy 5.1.1

The City shall make every effort to maintain mobility standards that result in a minimal level of service (LOS) "D." The City defines LOS D as the equivalent to a volume to capacity ratio of 0.9.

Finding, TSP Policy 5.1.1: According the Traffic Report prepared by the Oregon Department of Transportation (ODOT) for STIP Project No. KN 20185, the Scenic Avenue intersection with Highway 99 operates at a LOS F and a volume to capacity ratio of 1.42 during peak hours. The report states that these conditions are due to school related traffic and this has caused a signal to be included in the STIP Project KN 20185 as well as the City of Central Point Project No. 230 in the adopted TSP. The traffic signal must meet warrants to be installed. Per the traffic report this occurs when the Highway 99 street section is reconfigured to three (3) lanes. Analysis shows that implementation of the revised project improves the intersection operation to a LOS B and a volume to capacity ration of 0.70, which brings this facility into compliance with the City's mobility standards. Without the road reconfiguration, the intersection would continue to operate at an unacceptable LOS and volume to capacity ratio.

Conclusion, TSP Policy 5.1.1: The proposed amendment is necessary to install a traffic signal and meet the City's mobility standards at the intersection of Scenic Avenue and Highway 99 consistent with this policy.

TSP Policy 7.1.11

The City shall place a higher priority on funding and constructing street projects that address identified vehicular, bicycle and pedestrian safety problems than those projects that solely respond to automotive capacity deficiencies in the street system. Exceptions are those capacity improvements that are designed to also resolve identified safety problems.

Finding, TSP Policy 7.1.11: Based on the ODOT Traffic Report, there have been 37 crashes recorded within the project limits during the past 10 year period. The proposed revision adding a road diet is in response to the safety concerns. By adding the road diet, signal warrants can be met as needed to reduce the number and severity of collisions at this intersection. Funding has been obtained by ODOT through the All Road Transportation Safety (ARTS) Program to implement the project. The City finds that this TSP Amendment is consistent with prioritizing projects that address safety concerns.

Conclusion, TSP Policy 7.1.11: Consistent.

V. CPMC 17.96, Comprehensive Plan and Urban Growth Boundary Amendments.

This chapter establishes the procedures and approval criteria for amending the Comprehensive Plan. The type of amendment and procedures applied are addressed in Section I of these findings. The approval criteria set forth in CPMC 17.96.500 are addressed below:

CPMC 17.96.500(A)

Approval of the request is consistent with the applicable statewide planning goals;

Finding, CPMC 17.96.500(A): See Section II Findings.

Conclusion, CPMC 17.96.500(A): Consistent.

CPMC 17.96.500(B)

Approval of the request is consistent with the Central Point comprehensive plan;

Finding, CPMC 17.96.500(1): See Section IV Findings.

Conclusion, CPMC 17.96.500(1): Consistent.

CPMC 17.96.500(C)

For urban growth boundary amendments findings demonstrate that adequate public services and transportation networks to serve the property are either available, or identified for construction in the city's public facilities master plans (major and minor amendments); and

Finding, CPMC 17.96.500(C): The proposed TSP Amendment does not involve a UGB Amendment.

Conclusion, CPMC 17.96.500(1): Not applicable.

CPMC 17.96.500(D)

The amendment complies with OAR 660-012-0060 of the Transportation Planning Rule.

Finding, CPMC 17.96.500(1): See Section III Findings.

Conclusion, CPMC 17.96.500(1): Consistent.

VI. Summary Conclusion

The TSP Amendment as presented in Exhibit 1 is consistent with all applicable approval criteria as demonstrated herein.

EXHIBIT 1 - Transportation System Plan Revisions

Table 7.4, Transportation Projects, 2008-2030

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Year	ODOT	County	Central Point	Medford	Other
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	•	•	•					√			√	2012	♦	♦	♦		

Table 12.5, Tier 1 – Long Term Projects

Ref. No.	Project Location	Improv. Category	Project Description	Vehicle	Bicycle	Pedestrian	Transit	Freight	Access	Economic	Safety	Operations	Truck Traffic	Urban Upgrade	Tier	ODOT	County	Central Point	Medford	Other	Total Project Cost
230	Hwy. 99 & Scenic Av. Intersection	major	Install a traffic signal when signal warrants are met and reconfigure from 5 to 3 lanes from MP 1.95 to the Exit 35 Interchange at MP 0.35.	■	■	■					√			√	Tier 1, Long	♦	♦	♦			\$2,737,300



City of Central Point **Staff Report to Council**

ISSUE SUMMARY

TO:	City Council	DEPARTMENT: Public Works
FROM:	Matt Samitore, Parks and Public Works Director	
MEETING DATE:	February 17, 2022	
SUBJECT:	Resolution No. _____, Accepting the Lowest Responsible Bid from Central Pipeline Inc. for the Gebhard Road Storm Drain Project and Authorizing the City Manager to Execute a Contract	
ACTION REQUIRED: Resolution		RECOMMENDATION: Approval

BACKGROUND INFORMATION:

The City conducted a bid letting procedure for the Gebhard Road Storm Drain project. This joint project will provide drainage for the remaining undeveloped areas of East Central Point as well as future growth areas as part of the current Urban Reserve. The project will include the installation of 1800 feet of storm drain from Gebhard Road to Bear Creek through land currently owned by Jackson County and managed by the Expo.

The engineer's estimate for the project was \$521,000. The City received six qualified bids for the project. The lowest responsible bidder for the project is from Central Pipeline Inc. for \$488,515.00. The project will be paid for from the storm drain and street funds.

FINANCIAL ANALYSIS:

The project was initially budgeted in the 2021/23 Storm Drain Fund for \$488,515. The additional monies will be funded by the Street Fund for the work occurring within the limits of Gebhard Road.

LEGAL ANALYSIS:

The City of Central Point public contracts/bidding is governed by Oregon Revised Statute (ORS) Chapter 279 and Central Point Municipal Code (CPMC) Chapter 2.40.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

Community Investment; Goal 5 – Plan, design, and construct modern and efficient infrastructure in all areas and systems. Strategies 3 and 4.

STAFF RECOMMENDATION:

Approve a resolution awarding the Gebhard Road Stormdrain project to Central Pipeline, Inc.

RECOMMENDED MOTION:

I move to approve Resolution No. ____ accepting the lowest responsible bid from Central Pipeline Inc. for \$488,515 for the Gebhard Road Storm Drain project and authorizing the City Manager to execute a contract.

ATTACHMENTS:

1. Gebhard Rd SD Outfall - Engineers Estimate_20210803
2. gebhard road sd bid ad
3. gebhard road sd project overview
4. resol_gebhard road sd

RH2		ENGINEER'S OPINION OF PROBABLE COST																		
Gebhard Road Stormwater Outfall		AGENCY City of Central Point		Probable Cost		Central Pipeline		Pilot Rock		Ledford		Jesse Rodriguez		Knife River Materials		JRT		Upper Rogue		
KIND OF WORK Storm Drainage Improvements		DATE 2/10/22																		
ITEM #	SPEC #	ITEM DESCRIPTION	UNIT	QUANTITY	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL	UNIT COST	TOTAL
200 TEMPORARY FEATURES AND APPURTENANCES																				
10	210	MOBILIZATION	LS	1	\$60,000.00	\$60,000.00	\$41,218.00	\$41,218.00	\$12,000.00	\$12,000.00	\$25,000.00	\$25,000.00	\$55,000.00	\$55,000.00	\$13,450.00	\$13,450.00	\$75,000.00	\$75,000.00	\$40,000.00	\$40,000.00
20	225	TEMPORARY WORK ZONE TRAFFIC CONTROL, COMPLETE	LS	1	\$1,500.00	\$1,500.00	\$1,225.00	\$1,225.00	\$1,500.00	\$1,500.00	\$2,000.00	\$2,000.00	\$4,567.00	\$4,567.00	\$2,000.00	\$2,000.00	\$5,000.00	\$5,000.00	\$12,000.00	\$12,000.00
30	280	EROSION AND SEDIMENT CONTROL	LS	1	\$2,500.00	\$2,500.00	\$7,000.00	\$7,000.00	\$6,500.00	\$6,500.00	\$7,300.00	\$7,300.00	\$3,000.00	\$3,000.00	\$6,300.00	\$6,300.00	\$7,000.00	\$7,000.00	\$18,000.00	\$18,000.00
300 ROADWORK																				
40	305	CONSTRUCTION SURVEY WORK	LS	1	\$2,500.00	\$2,500.00	\$3,500.00	\$3,500.00	\$1,700.00	\$1,700.00	\$2,000.00	\$2,000.00	\$5,975.00	\$5,975.00	\$1,620.00	\$1,620.00	\$2,000.00	\$2,000.00	\$10,000.00	\$10,000.00
50	330	GENERAL EXCAVATION	CY	160	\$30.00	\$4,800.00	\$18.00	\$2,880.00	\$25.00	\$4,000.00	\$15.00	\$2,400.00	\$27.00	\$4,320.00	\$26.00	\$4,160.00	\$51.00	\$8,160.00	\$60.00	\$9,600.00
400 DRAINAGE AND SEWERS																				
60	445	48 INCH CONC PIPE	LF	1118	\$350.00	\$391,300.00	\$335.00	\$374,530.00	\$360.00	\$402,480.00	\$350.00	\$391,300.00	\$441.00	\$493,038.00	\$436.00	\$487,448.00	\$502.00	\$561,236.00	\$360.00	\$402,480.00
70	445	SLOPED END SECTIONS, 48 INCH	EA	1	\$1,000.00	\$1,000.00	\$1,100.00	\$1,100.00	\$1,500.00	\$1,500.00	\$1,000.00	\$1,000.00	\$2,500.00	\$2,500.00	\$1,700.00	\$1,700.00	\$700.00	\$700.00	\$16,000.00	\$16,000.00
80	460	PAVED CULVERT END SLOPES	SF	98	\$45.00	\$4,410.00	\$15.00	\$1,470.00	\$40.00	\$3,920.00	\$70.00	\$6,860.00	\$42.00	\$4,116.00	\$46.00	\$4,508.00	\$50.00	\$4,900.00	\$87.00	\$8,526.00
90	470	CONCRETE STORM SEWER MANHOLES	EA	4	\$12,000.00	\$48,000.00	\$12,478.00	\$49,912.00	\$15,500.00	\$62,000.00	\$13,000.00	\$52,000.00	\$15,575.00	\$62,300.00	\$18,000.00	\$72,000.00	\$21,700.00	\$86,800.00	\$29,500.00	\$118,000.00
1000 RIGHT OF WAY DEVELOPMENT AND CONTROL																				
100	1040	PLANT MATERIALS - WILLOW PLUGS	EACH	475.00	\$10.00	\$4,750.00	\$12.80	\$6,080.00	\$8.00	\$3,800.00	\$14.00	\$6,650.00	\$9.00	\$4,275.00	\$8.00	\$3,800.00	\$10.00	\$4,750.00	\$23.00	\$10,925.00
SUBTOTAL:					\$521,000.00		\$488,915.00		\$499,400.00		\$496,510.00		\$639,091.00		\$596,986.00		\$755,546.00		\$645,531.00	
PROJECT TOTAL					\$521,000.00		\$488,915.00		\$499,400.00		\$496,510.00		\$639,091.00		\$596,986.00		\$755,546.00		\$645,531.00	

Attachment: Gebhard Rd SD Outfall - Engineers Estimate_20210803 (1510 : Gebhard Road Storm Drain - Bid Opening)

AFFIDAVIT OF PUBLICATION

**** THIS IS NOT A BILL ****

On Behalf of:
CITY OF CENTRAL POINT-Legal
140 S THIRD ST
CENTRAL POINT, OR 97502

STATE OF OREGON COUNTY OF JACKSON:

I, Kristy Gallon, being first duly sworn, depose and say that I am the principal clerk of Mail Tribune, a newspaper of general circulation, as defined by ORS 193.010 and 193.020; printed at Medford in the aforesaid county and state; that the public notice, a printed copy of which is hereto annexed, was published in the entire issue of said newspaper for 2 successive and consecutive insertion(s) in the following issues 1/9/2022, 1/16/2022 (HERE SET FORTH DATES OF ISSUE).

Kristy Gallon
Affiant

Subscribed and sworn to before me this
3rd day of Feb., 2022.

Terrie Rogers
Notary State of Oregon County of Jackson

My commission expires Sept 25, 2022 ^{24th}



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NOTICE TO CONTRACTORS
PUBLIC IMPROVEMENT PROJECT:
Gebhard Road Stormwater Outfall
PROJECT #8097070

Sealed bids will be opened and publicly read at the City of Central Point, City Hall, 140 S. 3rd Street, Central Point, OR 97502 at 2:00 PDT on Thursday, February 8, 2022 for the above referenced project. Bids must be submitted to Matt Samlors, Parks and Public Works Director, at the same address prior to 2:00 PDT on the above date. Subcontractor Disclosure forms must be submitted prior to 4:00 PDT on the same date.

Bidders must be prequalified in order to be eligible for award. Pre-qualification may be with the City of Central Point, City of Medford or the Oregon Department of Transportation. If the bidder is disqualified in any of these jurisdictions, the bidder will be ineligible for this contract.

Award of contract will not be final until the later of: 1) three business days after the City of Central Point announces Notice of Intent to Award, or 2) the City of Central Point provides a written response to each timely protest, denying the protest and affirming the award.

On all projects, work performed by the Contractor's own organization must be at least 30% of the awarded contract amount.

Plans and specifications will be available online only www.questodr.com beginning January 16th, 2022. Any addenda issued will be posted on the above websites.

COST ESTIMATE: \$300,000 - \$425,000

This project consists of storm drainage improvements from the east side of Gebhard Road to Bear Creek. This is a City funded project.

Work shall begin no earlier than February 22, 2022 and must be completed no later than July 15th, 2022. Please direct all questions to Greg Graves at 541-664-3321 (x225) or greg.graves@centralpoint.gov. The deadline for questions is February 3, 2022.

No bid will be received or considered by the City of Central Point unless bidder signs the bid statement.

The contract is for public work subject to ORS 279C.800 to 279.870. This project is subject to Oregon prevailing wage rates.

The City of Central Point may reject any bid not in compliance with all public bidding procedures and requirements and may reject for good cause any or all bids upon a finding by the City of Central Point that it is in the public interest to do so.

CITY OF CENTRAL POINT

Matt Samlors, Parks and Public Works Director

PUBLISHED:
Mail Tribune
PUBLISH DATE:
January 9th, 2022 and January 16, 2022

January 9 and 16, 2022

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

INVOICE

DJC Oregon

a division of BridgeTower Media
 PO Box 745929
 Atlanta, GA 30374-5929
 1 (612) 333-4244

City of Central Point
Cyndi Weeks
 140 S 3rd St
 Central Point, OR 97502-2216

Account #	8.B.b
10032589	
Invoice Date	1/14/2022
Invoice #	745289209
Order #	12066906
Terms	NET 30
PO/Case #	
Salesrep	

Days/Inserts	Description	Size/Qty	Unit Price	Amount
2	<p align="center">GEBHARD ROAD STORMWATER OUTFALL</p> <p>Daily Journal of Commerce (OR) Bids - Other / Construction City of Central Point; Bid Location Central Point, OR, Jackson County; Due 02/08/2022 at 02:00 PM 01/07/2022, 1/14 -Base Charge</p> <p align="center"><i>\$670.50</i></p>	1 col x 8.10in 416 wrd / 76 ln		311.60
TOTAL DUE				311.60

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

Acceptable Payment Methods:

To Pay by Check:
 BridgeTower OpCo, LLC
 PO Box 745929
 Atlanta, GA 30374-5929

To Pay by ACH:
 Bank: Bank of America
 Contact Linda Burnette
 Account Number: 237025443017
 Routing Number: 053000196

To Pay by Credit Card:
 Contact Accounts Receivable:
 866-802-8214
 Please have your Invoice Number
 and Credit Card Number Ready

AFFIDAVIT OF PUBLICATION



DJCOREGON

11 NE Martin Luther King Jr. Blvd. Suite 201 / Portland, OR 97232-3579
(503) 226-1311

STATE OF OREGON, COUNTY OF MULTNOMAH--ss.

I, **Nick Bjork**, being first duly sworn, depose and say that I am a **Publisher** of the **Daily Journal of Commerce**, a newspaper of general circulation in the counties of CLACKAMAS, MULTNOMAH, and WASHINGTON as defined by ORS 193.010 and 193.020; published at Portland in the aforesaid County and State; that I know from my personal knowledge that the Construction notice described as

Case Number: NOT PROVIDED

GEBHARD ROAD STORMWATER OUTFALL

City of Central Point; Bid Location Central Point, OR, Jackson County; Due 02/08/2022 at 02:00 PM

a printed copy of which is hereto annexed, was published in the entire issue of said newspaper for 2 time(s) in the following issues:

1/7/2022

1/14/2022

State of Oregon
County of Multnomah

SIGNED OR ATTESTED BEFORE ME
ON THE 14th DAY OF January, 2022

Nick Bjork

**SEE
EXHIBIT A**

Notary Public-State of Oregon



Cyndi Weeks
City of Central Point
140 S 3rd St
Central Point, OR 97502-2216

Order No.: 12066906
Client Reference No:

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)

**CITY OF CENTRAL POINT
DEPARTMENT OF PUBLIC
WORKS**

**PUBLIC IMPROVEMENT PROJECT:
GEBHARD ROAD STORMWATER
OUTFALL**

**Bids due 2:00 pm, February 8, 2022
ADVERTISEMENT FOR BIDS
PROJECT #8097070**

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Bidders must be prequalified in order to be eligible for award. Pre-qualification may be with the City of Central Point, City of Medford or the Oregon Department of Transportation. If the bidder is disqualified in any of these jurisdictions, the bidder will be ineligible for this contract.

Award of contract will not be final until the later of: 1) three business days after the City of Central Point announces Notice of Intent to Award, or 2) the City of Central Point provides a written response to each timely protest, denying the protest and affirming the award.

On all projects, work performed by the Contractor's own organization must be at least 30% of the awarded contract amount.

Plans and specifications will be available online only www.questcdn.com beginning January 15th, 2022. Any addenda issued will be posted on the above websites.

COST ESTIMATE: \$300,000 - \$425,000

This project consists of storm drainage improvements from the east side of Gebhard Road to Bear Creek. This is a City funded project.

Work shall begin no earlier than **February 22, 2022** and must be **completed no later than July 15th, 2022**. Please direct all questions to Greg Graves at 541-664-3321 (x225) or greg.graves@centralpoint.gov. The deadline for questions is **February 3, 2022**.

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The contract is for public work subject to ORS 279C.800 to 279.870. This project is subject to Oregon prevailing wage rates.

The City of Central Point may reject any bid not in compliance with all public bidding procedures and requirements and may reject for good cause any or all bids upon a finding by the City of Central Point that it is in the public interest to do so.

CITY OF CENTRAL POINT
Matt Samitore, Parks and
Public Works Director

Published Jan. 7 & 14, 2022.

12066906

Attachment: gebhard road sd bid ad (1510 : Gebhard Road Storm Drain - Bid Opening)



CITY OF CENTRAL POINT

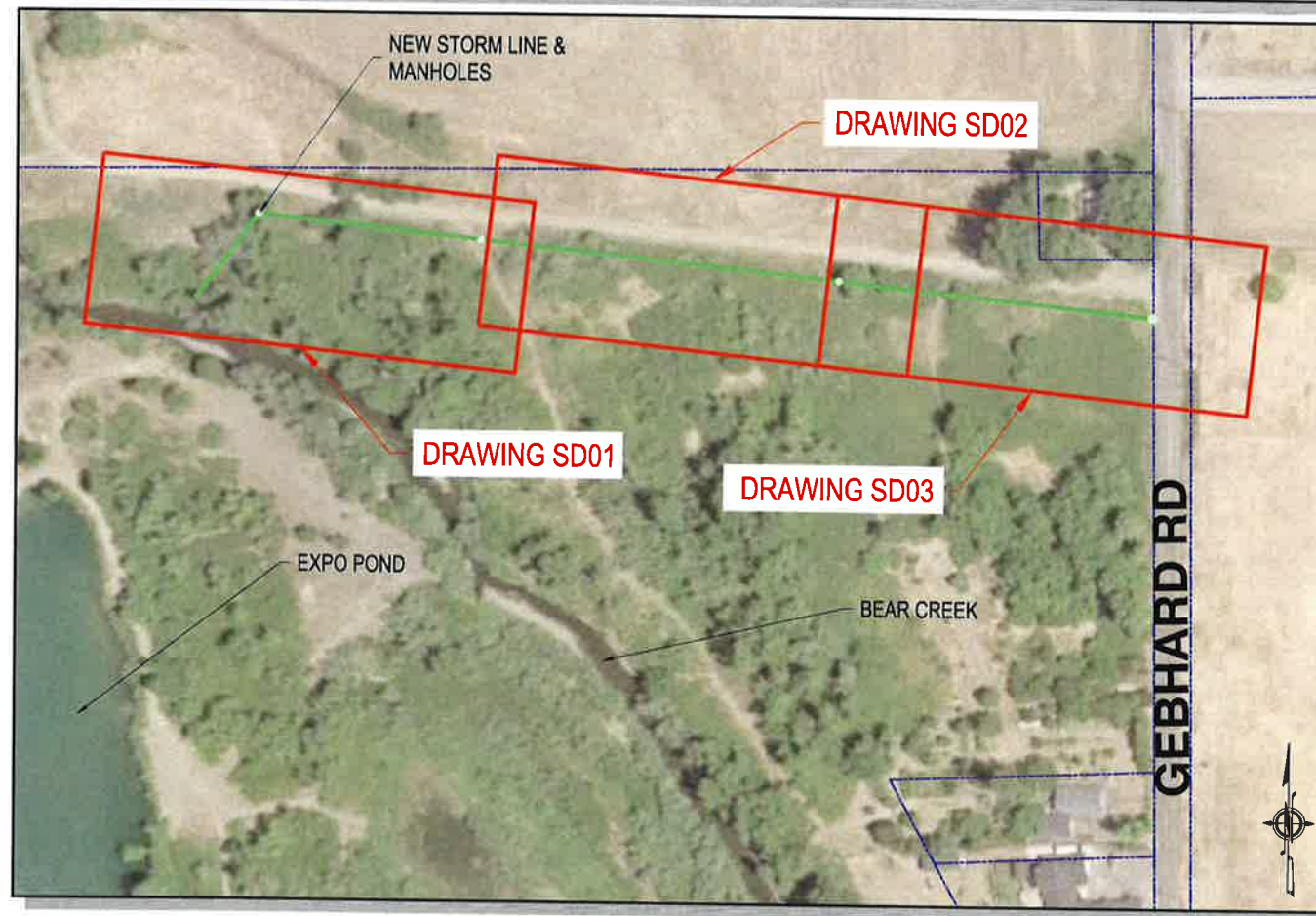
GEBHARD ROAD STORMWATER OUTFALL

SUMMER 2021

PROJECT VICINITY MAP



PROJECT LOCATION MAP



DRAWING INDEX

Sheet Number	Sheet Title	Dwg No
01	COVER	COV
02	GENERAL NOTES	G01
03	STORMWATER PLAN AND PROFILE - SHEET 1 OF 3	SD01
04	STORMWATER PLAN AND PROFILE - SHEET 2 OF 3	SD02
05	STORMWATER PLAN AND PROFILE - SHEET 3 OF 3	SD03
06	STORMWATER DETAILS	SD04

CALL 48 HOURS BEFORE YOU DIG ONE CALL 811

ATTENTION: OREGON LAW REQUIRES YOU TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER. THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090. YOU MAY OBTAIN COPIES OF THE RULES BY CALLING THE CENTER AT 503-232-1978

REPORT SPILLS

ATTENTION: OREGON LAW REQUIRES THAT SPILLS BE REPORTED TO THE FOLLOWING ENTITIES:
 OREGON EMERGENCY RESPONSE SYSTEM: 1-800-452-0311
 THE NATIONAL RESPONSE CENTER: 1-800-424-8802

CONTACT PERSONNEL

CONTACT	AGENCY	PHONE
TYLER DUNCAN, P.E. (PROJECT MANAGER)	RH2 ENGINEERING	(541) 326-4448
GREG GRAVES	CITY OF CENTRAL POINT	(541) 664-3321 x225
NICK BAKKE	RVSS	(541) 665-6300
RYLAN WOOD	PACIFIC POWER	(541) 858-3204

SECTION AND DETAIL REFERENCES

THE FOLLOWING CONVENTIONS HAVE BEEN USED WITHIN THESE DRAWINGS TO REFER THE READER BETWEEN THE SECTION/DETAIL AND THE PLAN FROM WHICH IT IS REFERENCED.

REFERENCE BUBBLES



PLAN REFERENCE BUBBLE - REFERS READER BACK TO THE PLAN FROM WHICH THE DETAIL OR SECTION ORIGINATED.



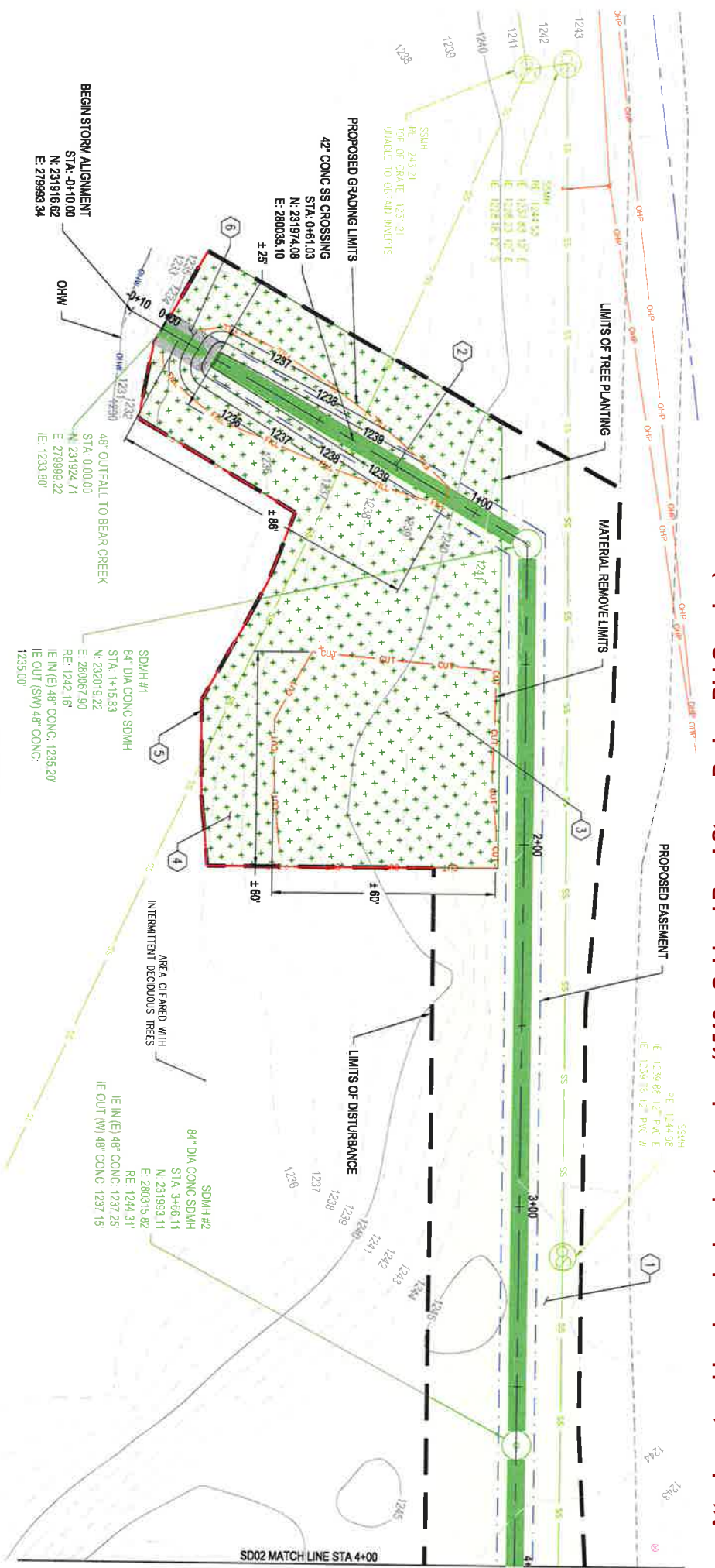
DETAIL/SECTION REFERENCE BUBBLE - REFERS READER TO THE DRAWING ON WHICH THE DETAIL OR SECTION IS LOCATED.

WHERE, ID = SECTION/DETAIL REFERENCE NUMBER
 ## = DRAWING NUMBER ON WHICH DETAIL ORIGINATED OR RESIDES.

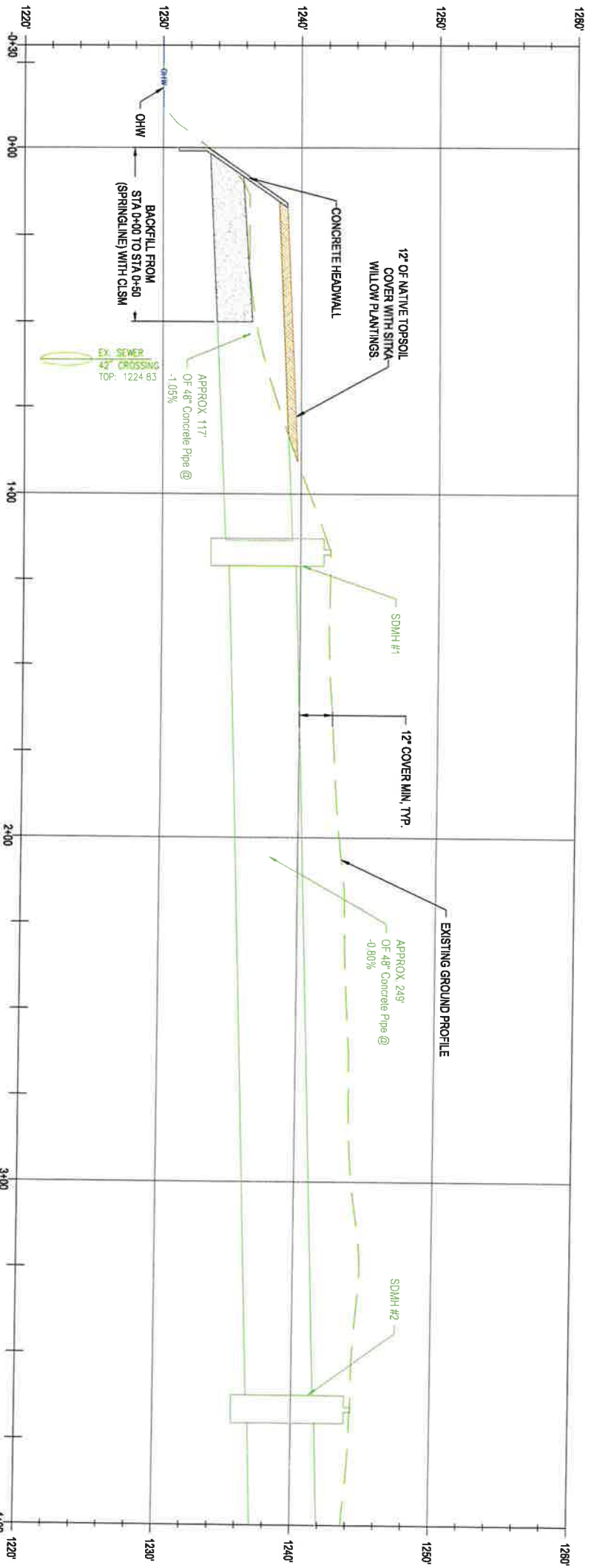
SECTION/DETAIL REFERENCE NUMBER CONVENTIONS:
 SECTIONS OR ELEVATIONS SHOULD HAVE A LETTER REFERENCE NUMBER (A THROUGH ZZ).



Attachment: gebhard road sd project overview (1510 : Gebhard Road Storm Drain - Bid Opening)



PLAN VIEW
1" = 20'



STORM PROFILE
H: 1" = 20', V: 1" = 5'

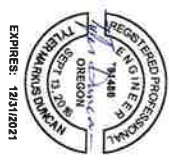
GENERAL NOTES

- 1) LOCATION OF EX UTILITIES AND STRUCTURES ARE FROM INFORMATION AVAILABLE AT THE TIME OF DESIGN. EXACT LOCATION AND COMPLETENESS ARE NOT GUARANTEED. CALL SERVICES ALERT 911 A MINIMUM OF 48 HOURS PRIOR TO ANY EXCAVATION AND POT HOLE FOR EXISTING UTILITY LOCATIONS.
- 2) WHEREVER TOPSOIL IS REMOVED FOR TRENCHING OR OTHER EXCAVATIONS, STOCKPILE TOPSOIL SEPARATELY AND REINSTALL STOCKPILED NATIVE TOPSOIL FOR UPPER 12" MINIMUM OF TRENCH BACKFILL AND IN ALL AREAS WHERE BACKFILL OR FILL IS REQUIRED.
- 3) PIPE SLOPES ARE REFERENCING ACTUAL LAYING SLOPE OF PIPE BETWEEN EDGE OF STRUCTURES. PIPE LENGTHS ARE REFERENCED TO CENTER OF STRUCTURES PER PAYMENT LENGTH. REFER TO DETAIL.
- 4) ALL WORK FROM STA 0+00 TO STA 1+1+5 (FROM BEAR CREEK TO SDMH #1) SHALL OCCUR WITHIN THE IN-WATER WORK PERIOD JUNE 15TH THROUGH SEPTEMBER 15TH.

KEY NOTES

- 1) SEED BARE EARTH/DISTURBED AREAS WITH AN APPROVED GRASS SEED MIX.
- 2) PLACE 12" OF NATIVE TOPSOIL OVER TOP OF EXPOSED PIPE, WITHIN AREA, LINES AND GRADES SHOWN, APPROXIMATELY 80 CY FILL. PLACE WITH A MAX 3H:1V SLOPE.
- 3) REMOVE AND STOCKPILE TOPSOIL TO 12" IN DEPTH. EXCAVATE MATERIAL, WITHIN AREA SHOWN, TO REMOVE APPROXIMATELY 80 CY AND DISPOSE OF EXCESS EXCAVATED MATERIAL OFFSITE. PLACE STOCKPILED NATIVE SOIL OVER PIPE AREA TO THE LINES, AND GRADES SHOWN.
- 4) PLANT SITKA WILLOW (SALIX SITCHENSIS) WITHIN AREA SHOWN (APPROX 440 ST), PLUGS AT 5' MIN SPACING (APPROXIMATELY 475 PLUGS), TO LIMITS SHOWN.
- 5) INSTALL TEMPORARY SILT FENCE FOR EROSION CONTROL.
- 6) PAVED END SLOPE, REFER TO DETAIL.

VICINITY MAP



CITY OF CENTRAL POINT
GEBHARD ROAD STORMWATER OUTFALL
STORMWATER PLAN AND PROFILE -
SHEET 1 OF 3

NO.	DATE	DESCRIPTION	BY	REVIEW

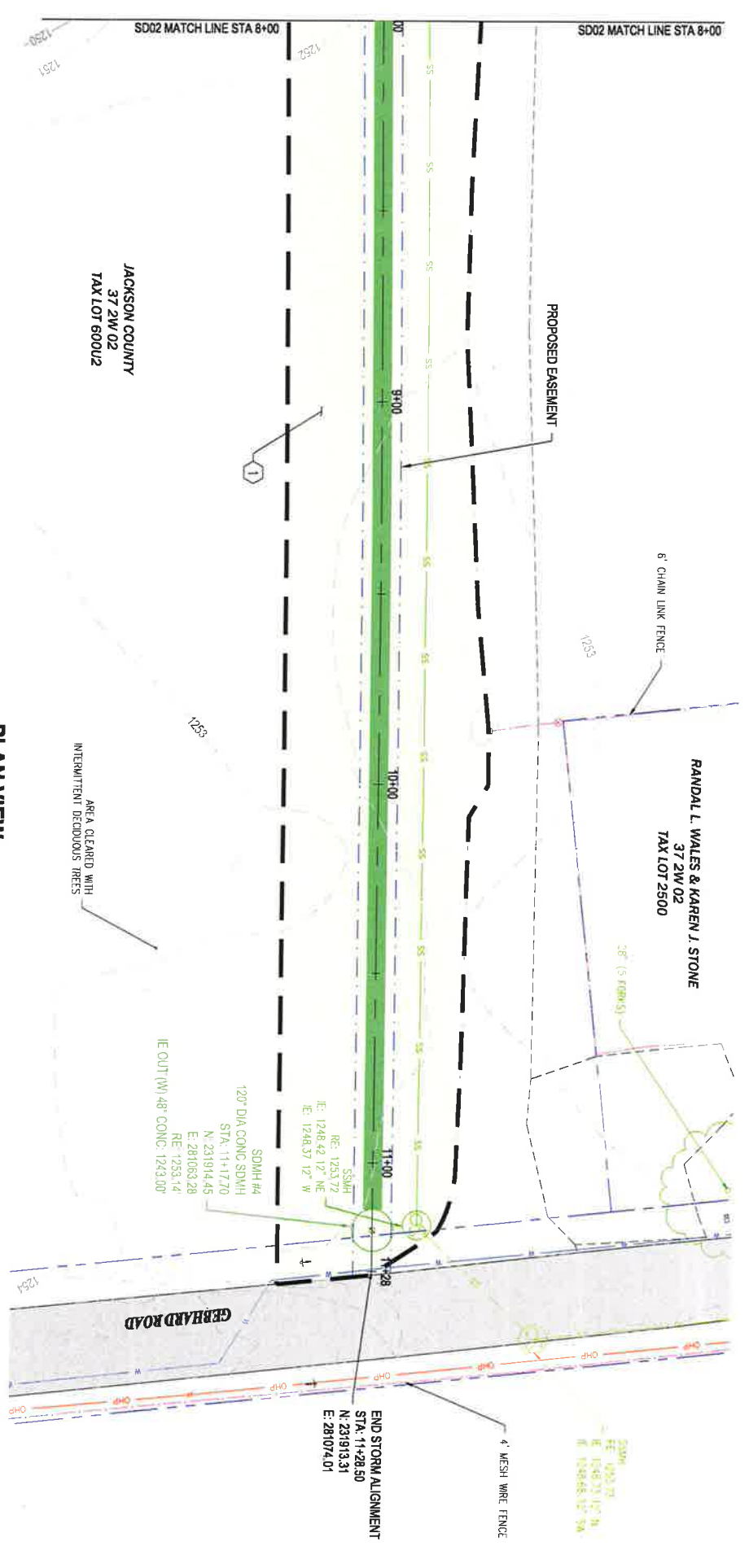
REVISIONS

ENGINEER: TMD SAVE DATE: Aug 6, 2021 CLIENT: CPT JOB NO.: 1015-083-22
 REVIEWED: JAB PLOT DATE: Aug 6, 2021 FILENAME: GRSD-P-STORMP DWG

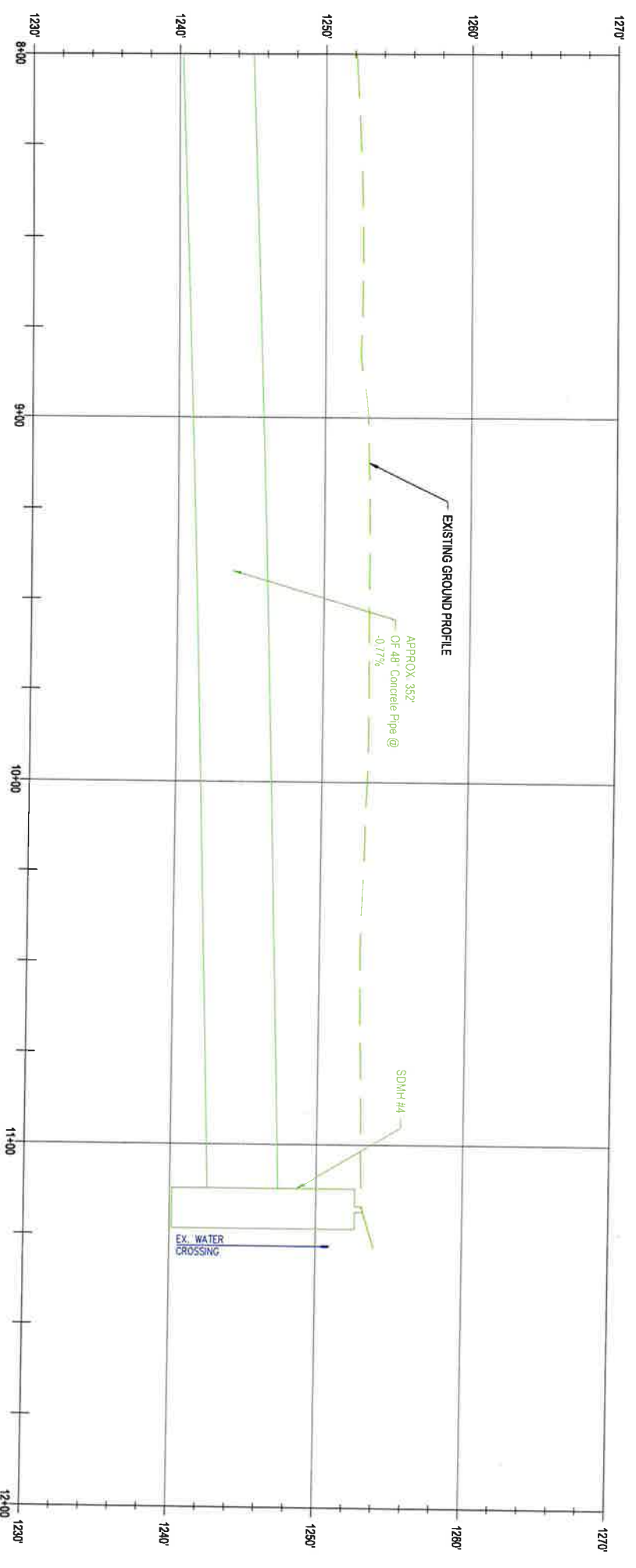
SCALE: SHOWN

DRAWING IS FULL SCALE UNLESS OTHERWISE NOTED

SHEET NO: 03 OF 05



PLAN VIEW
1" = 20'



STORM PROFILE
H: 1" = 20', V: 1" = 5'

Attachment: gebhard road sd project overview (1510 : Gebhard Road Storm Drain - Bid Opening)

GENERAL NOTES

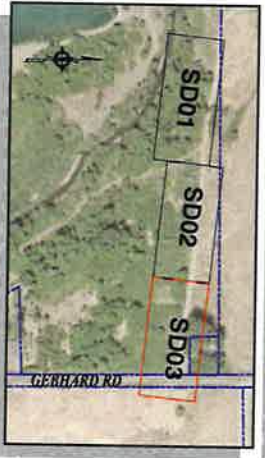
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KEY NOTES

- ① SEED BARE EARTH/DISTURBED AREAS WITH AN APPROVED GRASS SEED MIX.



VICINITY MAP



CITY OF CENTRAL POINT
GEHBARD ROAD STORMWATER OUTFALL
STORMWATER PLAN AND PROFILE -
SHEET 3 OF 3



ENGINEER: TMD	SAVE DATE: Aug 6, 2021	CLIENT: CPT	JOB NO.: 1015-083-22
REVIEWED: JAB	PLOT DATE: Aug 6, 2021	FILENAME: GRSC-P-STORMP.DWG	
REVISIONS			
NO.	DATE	DESCRIPTION	BY

DRAWING SCALE: 1" = 20'
 SHEET NO.: SD03
 OF 05

RESOLUTION NO. _____

A RESOLUTION APPROVING THE LOWEST QUALIFIED BID FOR THE CONSTRUCTION OF THE GEBHARD ROAD STORM DRAIN PROJECT TO CENTRAL PIPELINE, INC. AND AUTHORIZING THE CITY MANAGER TO EXECUTE A CONTRACT.

RECITALS:

A. WHEREAS, the City recently published a solicitation for lowest bids for the construction of the Gebhard Road Storm Drain Project.

B. WHEREAS, the City received six bids.

C. WHEREAS, the lowest bid was submitted by Central Pipeline, Inc. for \$488,000.

The City of Central Point resolves as follows:

Section 1. The City Council hereby accepts the lowest bid from Central Pipeline, Inc for the construction of the Gebhard Road Storm Drain Project.

Section 2. The City Manager is hereby authorized to execute a contract and any related documents necessary to effectuate the acceptance of this award in a form substantially the same as that included in the specifications.

Section 3. This Resolution shall take effect immediately from and after its passage and approval.

Passed by the Council and signed by me in authentication of its passage this _____ day of _____, 2022.

Mayor Hank Williams

ATTEST:

City Recorder

1 - Resolution No. _____ 2/17/2022 Council meeting)

Attachment: resol_gebhard road sd (1510 : Gebhard Road Storm Drain - Bid Opening)

The facility will be provided to RVCOG without charge for a period not to exceed 180-days. The facility is currently leased to the Central Point Area Senior Citizens at no charge. RVCOG will be responsible for all associated expenses for operating the facility. The IGA will have no financial impact on the City.

LEGAL ANALYSIS:

Legal counsel drafted the resolution and IGA and has no objections to the agreement.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

Strategic Priority: Community Investment

GOAL 2 – Be a city filled with happy, healthy people who are thriving

STRATEGY 1 – Develop and maintain positive partnerships with public and private entities and the community to understand community needs and leverage resources to develop and deliver opportunities for personal, professional, and/or vocational development.

STAFF RECOMMENDATION:

Make a motion to approve the resolution.

RECOMMENDED MOTION:

I move to approve Resolution No. _____, a Resolution approving an intergovernmental agreement with RVCOG for temporary use of the Senior Center building and authorizing the City Manager to execute same.

ATTACHMENTS:

1. RESO IGA RVCOG Senior Center Use
2. IGA 2 - RVCOG - Senior Ctr Use Agt 2-9-2022 (2)

RESOLUTION NO. _____

A RESOLUTION APPROVING THAT INTERGOVERNMENTAL AGREEMENT WITH RVCOG FOR TEMPORARY USE OF SENIOR CENTER BUILDING AND AUTHORIZING CITY MANAGER TO EXECUTE SAME

Recitals:

- A. RVCOG and the City are in the process of negotiating a sale of the Central Point Senior Center commercial building located at 123 N. 2nd Street in Central Point;
- B. RVCOG intends to use the building for provision of senior services and programs including “Meals on Wheels” and “Food with Friends”;
- C. In order to allow continuity of such services pending completion of the sale of the property, RVCOG and City desire to enter into an Intergovernmental Agreement allowing RVCOG to use the building for provision of such services.

The City of Central Point resolves as follows:

Section 1. The Council approves the Intergovernmental Agreement attached hereto as Exhibit “A” and authorizes the City Manager or his designee to sign said Agreement on behalf of the City.

Passed by the Council and signed by me in authentication of its passage this _____ day of February, 2022.

Mayor Hank Williams

ATTEST:

City Recorder

INTERGOVERNMENTAL AGREEMENT
TEMPORARY USE OF SENIOR CENTER

This Intergovernmental Agreement is made and entered into in duplicate as of the later of the dates entered below, by and between the CITY OF CENTRAL POINT, a municipal corporation, hereinafter “CITY” and ROGUE VALLEY COUNCIL OF GOVERNMENTS, hereinafter “RVCOG”) regarding temporary use and operation of the Central Point Senior Center and related facilities for operation of senior services such as “Meals on Wheels” and “Food and Friends” program.

RECITALS

WHEREAS, the State of Oregon has declared it to be a matter of statewide concern to promote intergovernmental cooperation for the purposes of furthering economy and efficiency in local government; and

WHEREAS, Oregon Statutes grant general authority for intergovernmental agreements by units of local government pursuant to the provisions of ORS 190.010 et seq; and

WHEREAS, the Central Point Senior Center, located at 123 N. 2nd Street in Central Point (hereinafter the “Premises”) is currently the location in which multiple services to seniors in the community are offered; and

WHEREAS, RVCOG currently offers meal programs to seniors such as Meals on Wheels and Food & Friends and desires to purchase the Premises for their meal programs; and

WHEREAS, it is necessary and appropriate for RVCOG to secure space to continue providing senior services pending its negotiation and purchase of the Premises from the CITY; and

WHEREAS, CITY and RVCOG deem it to be in their mutual best interest to enter into this Intergovernmental Agreement for the purpose of allowing RVCOG to utilize the Premises and related facilities for continuation of senior center operations.

NOW THEREFORE, in consideration of the above recitals and the mutual covenants, terms and provisions set forth below, the parties agree as follows:

1. The CITY agrees to provide RVCOG use of the Premises for a period up to 180-days without charge for Senior Center operations, including but not limited to “Meals on Wheels” and “Food & Friends” subject to the provisions of section 3 below. The Premises consist of approximately 1800 square feet of commercial building space located at 123 N. 2nd Street, Central Point as well as non-exclusive reasonable use of the common area surrounding the building space for parking, ingress and egress, in common with CITY.
2. During the period of any such temporary use, RVCOG shall provide all necessary personnel, volunteers, equipment and supplies and pay all expenses related thereto in

connection with RVCOG operations. Should it become necessary for RVCOG to use CITY equipment on a temporary basis, the parties agree to meet and confer as to the terms of such use.

3. It is anticipated, at the time of this agreement, that the temporary use of the Premises shall occur Monday through Friday from 7:00 a.m. to 1:30 p.m. and is subject to the following restrictions:
 - 3.1 Any modifications to the Premises made by RVCOG shall be subject to prior approval of CITY. All such modifications shall be at RVCOG's sole expense. RVCOG shall be required to obtain all necessary permits for such work. RVCOG shall pay as due all claims for work done on and for services rendered or material furnished to the Premises and shall keep the Premises free from any liens. If RVCOG fails to pay any such claims or to discharge any lien, CITY may do so and collect the costs from RVCOG. Any amount so added shall bear interest at the rate of twelve percent (12%) per annum from the date expended by CITY and shall be payable on demand.
 - 3.2 RVCOG is responsible for its own set up and take down, cleanup, storage, and any damage done to CITY facilities by its groups or persons whom it may invite to attend.
 - 3.3 The Premises shall be used for the purpose of providing senior services and other civic activities.
 - 3.4 Upon termination of this Agreement, except where such termination is the result of RVCOG's taking title to the Premises, all fixtures placed upon the Premises during the term, other than RVCOG's trade fixtures, shall, at CITY's option, become the property of CITY. If CITY so elects, RVCOG shall remove any or all fixtures that would otherwise remain the property of CITY and shall repair any physical damage resulting from the removal. If RVCOG fails to remove such fixtures, CITY may do so and charge the cost to RVCOG with interest at the legal rate from the date of expenditure.
4. This Agreement shall continue in effect until the closing of the sale of the Premises, or until terminated by either party as provided in paragraph 5.
5. Each party to this Agreement reserves the right to terminate this Agreement by giving the other party 30-days prior written notice of its intent to terminate.
6. Each party agrees to indemnify and hold harmless the other from liability arising from the acts or omissions of the elected officials, officers, employees or agents of the party.
7. This Agreement shall be liberally construed to effect the purposes expressed herein.
8. This Agreement is effective when it has been executed by both parties.

IN WITNESS WHEREOF, the parties have executed this Agreement in triplicate on the dates set forth below.

CITY OF CENTRAL POINT

ROGUE VALLEY COUNCIL OF GOVERNMENTS

By:
Its:

By:
Its:

Attachment: IGA 2 - RVCOG - Senior Ctr Use Agt 2-9-2022 (2) (1506 : Resolution Approving an IGA with RVCOG for Temporary Use of the

To meet the foregoing requirements, staff recommends an overall rate increase of 5% for 2022/2023 FY.

FINANCIAL ANALYSIS:

City/Public Works staff has prepared an update to the City Water Rates based upon the past three years of rate increases. The increase is split consistently between the base rate and the consumptive rate.

Current Rates:

Residential							
Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge	Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf	
5/8"	14.57	1.00	15.57	0.98	1.90	3.04	
1"	19.96	2.45	22.41	0.98	1.90	3.04	
1.5"	25.34	8.15	33.49	0.98	1.90	3.04	
2"	36.12	11.15	47.27	0.98	1.90	3.04	
3"	57.68	25.25	82.93	0.98	1.90	3.04	
4"	79.24	43.85	123.09	0.98	1.90	3.04	
6"	149.30	86.00	235.30	0.98	1.90	3.04	
8"	230.14	139.50	369.64	0.98	1.90	3.04	

Proposed Rates:

Residential							
Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge	Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf	
5/8"	15.30	1.00	16.30	1.03	2.00	3.19	
1"	20.96	2.45	23.41	1.03	2.00	3.19	
1.5"	26.61	8.15	34.76	1.03	2.00	3.19	
2"	37.93	11.15	49.08	1.03	2.00	3.19	
3"	60.56	25.25	85.81	1.03	2.00	3.19	
4"	83.20	43.85	127.05	1.03	2.00	3.19	
6"	156.77	86.00	242.77	1.03	2.00	3.19	
8"	241.65	139.50	381.15	1.03	2.00	3.19	

Based upon this methodology, the Base Rate will increase by \$0.73 and the consumptive rates by \$0.05 for Tier 1, \$0.10 for Tier 2, and \$0.15 for Tier 3. Attached is the full rate resolution reflecting residential, commercial, multi-family and irrigation rates based on the hybrid rate increase proposal.

LEGAL ANALYSIS: N/A

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

City of Central Point 2040 Strategic Plan – Strategic Priority – Responsible Governance

GOAL 1 - Maintain a strong financial position that balances the need for adequate service levels and capital requirements against the affordability that is desired by our citizens.

STRATEGY 1 – Continually update the City’s long-term financial plan/strategy.

STRATEGY 2 – Work with partner agencies and stakeholders to eliminate redundancy and maximize efficiency in all areas.

STRATEGY 3 – Solicit support from partner agencies in our effort to maintain a robust financial position.

STAFF RECOMMENDATION: Staff recommends approval of the Resolution.

RECOMMENDED MOTION: I move to approve Resolution. ___ a Resolution of the City of Central Point Setting a Water Rate Adjustment effective March 1, 2022.

ATTACHMENTS:

1. 2022 wate rate resolution
2. Water rates 2022
3. fcs rate model

RESOLUTION NO. _____

A RESOLUTION OF THE CITY OF CENTRAL POINT
SETTING A WATER RATE ADJUSTMENT EFFECTIVE MARCH 1, 2022

Recitals:

- A. The City has received information from the Medford Water Commission that March 1, 2022 the rate for bulk water purchases will increase by a minimum of 4% to a maximum of 8%, an estimated additional cost of at least \$44,000 annually.
- B. The City of Central Point conducted an update based on inflationary cost increases and determined that a 4% increase is needed to handle the increased supply costs and paying off the Vilas Reservoir and Pump Station bond two years early.
- C. The combined increases equate to a 5% increase which would be split between the base and tier rates.

The City of Central Point resolves as follows:

Section 1. Effective March 1, 2022, the City of Central Point Water Rates shall be adjusted as set forth on the Attachment A.

Passed by the Council and signed by me in authentication of its passage this _____ day of February 2022.

Mayor Hank Williams

ATTEST:

City Recorder

Attachment: 2022 wate rate resolution [Revision 1] (1509 : 2022 Water Rates)

2022/23 Utility Rates

Attachment A

Cost of Service - Water Rates (Per hundred cubic feet)

Res. No.

(Into effect March 21, 2022)

Residential

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	15.30	1.00	16.30
1"	20.96	2.45	23.41
1.5"	26.61	8.15	34.76
2"	37.93	11.15	49.08
3"	60.56	25.25	85.81
4"	83.20	43.85	127.05
6"	156.77	86.00	242.77
8"	241.65	139.50	381.15

Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19

Senior-Housing

Units	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5	22.71	1.25	23.96
10	45.42	2.50	47.92
15	68.13	3.75	71.88
20	90.85	5.00	95.85
25	113.56	6.25	119.81
30	136.27	7.50	143.77
35	158.98	8.75	167.73
40	181.69	10.00	191.69

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Iti-Family Residential

Units	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
3	24.66	4.50	29.16
4	32.89	6.00	38.89
5	41.11	7.50	48.61
6	49.33	9.00	58.33
7	57.55	10.50	68.05
8	65.77	12.00	77.77
9	73.98	13.50	87.48
10	82.20	15.00	97.20

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Commercial & Standby

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	13.60	1.00	14.60
1"	16.99	2.10	19.09
1.5"	22.65	4.00	26.65
2"	28.31	7.15	35.46
3"	45.29	14.10	59.39
4"	65.66	21.70	87.36
6"	117.16	44.00	161.16
8"	182.79	68.00	250.79

Volume Charge First 8 ccf
1.81
1.81
1.81
1.81
1.81
1.81
1.81
1.81

Irrigation

Meter Size	Monthly Base Charge	Monthly R & R Charge	Total Monthly Fixed Charge
5/8"	15.30	1.50	16.80
1"	20.96	4.15	25.11
1.5"	26.61	8.65	35.26
2"	37.93	14.00	51.93
3"	60.56	28.00	88.56
4"	83.20	44.00	127.20
6"	156.77	88.00	244.77
8"	241.65	139.00	380.65

Volume Charge First 8 ccf	Volume Charge 8 - 22 ccf	Volume Charge Over 22 ccf
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19
1.03	2.00	3.19

Hydrant	30.99	20.00	50.99
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1.81

2017/18 Street Fee

\$6.06

2017/18 Stormwater Fee

\$6.50

2017/18 Stormwater Quality Fee

\$1.00

Notes:

Backflow Fee

\$1.00 w/backflow
\$0.25 w/o backflow

Damage Recovery

hour of a PW employee, + 10% admin fee

Expo	0.00	0.00	0.00	1.81
Hardship Discount			50%	50%
Outside City Factor			200%	200%

Central Point
Utility Rate Study: Water Utility
Summary

Revenue Requirement	2023	2024	2025	2026	2027	2028	2029	2030
Revenues								
Rate Revenues Under Existing Rates	\$ 3,787,531	\$ 3,825,406	\$ 3,873,224	\$ 3,935,787	\$ 3,999,361	\$ 4,063,962	\$ 4,129,606	\$ 4,196,311
Non-Rate Revenues	<u>275,106</u>	<u>277,913</u>	<u>282,003</u>	<u>286,578</u>	<u>291,229</u>	<u>295,955</u>	<u>300,758</u>	<u>305,639</u>
Total Revenues	\$ 4,062,637	\$ 4,103,319	\$ 4,155,227	\$ 4,222,366	\$ 4,290,590	\$ 4,359,917	\$ 4,430,365	\$ 4,501,950
Expenses								
Cash Operating Expenses	\$ 3,313,813	\$ 3,450,685	\$ 3,518,233	\$ 3,587,445	\$ 3,658,118	\$ 3,730,285	\$ 3,803,980	\$ 3,879,238
Existing Debt Service	509,106	664,710	664,220	662,504	662,552	662,335	661,854	1,434,096
New Debt Service	-	57,862	57,862	57,862	57,862	57,862	57,862	57,862
System Reinvestment Funding	-	-	-	-	-	-	-	-
Additions Required to Meet Reserves	-	-	-	-	-	-	-	-
Total Expenses	\$ 3,822,919	\$ 4,173,257	\$ 4,240,314	\$ 4,307,811	\$ 4,378,531	\$ 4,450,482	\$ 4,523,697	\$ 5,371,196
Net Surplus (Deficiency)	\$ 239,718	\$ (69,938)	\$ (85,087)	\$ (85,446)	\$ (87,942)	\$ (90,565)	\$ (93,332)	\$ (869,246)
Additions to Meet Coverage	-	-	-	-	-	-	-	-
Total Surplus (Deficiency)	\$ 239,718	\$ (69,938)	\$ (85,087)	\$ (85,446)	\$ (87,942)	\$ (90,565)	\$ (93,332)	\$ (869,246)
Annual Rate Increase	4.00%	4.00%	4.00%	4.00%	3.00%	3.00%	3.00%	2.00%
Cumulative Rate Increase	9.20%	13.57%	18.11%	22.84%	26.52%	30.32%	34.23%	36.91%
Revenues After Rate Increases	\$ 4,016,676	\$ 4,219,117	\$ 4,442,730	\$ 4,695,072	\$ 4,949,466	\$ 5,180,296	\$ 5,421,891	\$ 5,660,673
Additional Taxes from Rate Increase	<u>3,900</u>	<u>6,700</u>	<u>9,692</u>	<u>12,922</u>	<u>16,169</u>	<u>18,998</u>	<u>21,992</u>	<u>24,920</u>
Net Cash Flow After Rate Increase	\$ 464,964	\$ 317,073	\$ 474,727	\$ 660,918	\$ 845,994	\$ 1,006,771	\$ 1,176,961	\$ 570,195
Coverage After Rate Increase: Bonded Debt	n/a	19.99	22.73	25.96	29.22	32.08	35.11	38.10
Coverage After Rate Increase: Total Debt	2.14	1.60	1.82	2.08	2.35	2.58	2.82	1.48
Sample Residential Bill (5/8" Meter, x 14 ccf)	\$36.89	\$38.37	\$39.90	\$41.50	\$42.74	\$44.02	\$45.34	\$46.25
Annual Increase (\$)	\$1.42	\$1.48	\$1.53	\$1.60	\$1.24	\$1.28	\$1.32	\$0.91

Attachment: fcs rate model (1509 : 2022 Water Rates)



City of Central Point Staff Report to Council

ISSUE SUMMARY

TO:	City Council	DEPARTMENT:	Community Development
FROM:	Stephanie Holtey, Planning Director		
MEETING DATE:	February 17, 2022		
SUBJECT:	Transit Oriented Development (TOD) Fence Code Update Discussion		
ACTION REQUIRED:	Information/Direction	RECOMMENDATION:	Not Applicable

BACKGROUND INFORMATION:

The City received a request from School District #6 to modify current fence standards in the Transit Oriented Development (TOD) District to allow fences higher than four (4) feet and to consider allowing powder coated chain-link. The existing fence at this location is a 4-ft galvanized chain-link fence. The current code standards prohibit the use of chain-link fencing with or without slats and goes further to specify the materials that are allowed (i.e. masonry, decorative metal, wood, plant material/hedges or other materials that are approved and complimentary to adjacent building types). It is also required that fences be setback 2-ft from adjacent public right-of-way and the 2-ft buffer be landscaped. It is the School District's objective to replace the existing fencing to provide a more secure facility for its elementary aged kids. Metal is the preferred building material; however, the cost of wrought iron is \$151 per lineal foot, which would result in a cost of about \$407,000. The City's Public Works facility is facing similar challenges in affording compliance with the TOD fencing standards. Although cost alone is not typically the basis for policy decisions, it is indicative of a broader situation that warrants evaluation to determine if the current standards continue to be appropriate or if modifications are needed to mitigate cost while keeping the look and feel desired in the TOD.

At the February 17th City Council meeting, staff will present an overview of the current code standards in the TOD, provide examples of existing fences for context, and code amendment options for discussion and direction from Council.

FINANCIAL ANALYSIS:

There is not cost other than in-kind staff contribution for any code amendments that may be needed to update the City's fence standards in the TOD.

LEGAL ANALYSIS:

Not applicable at this time.

COUNCIL GOALS/STRATEGIC PLAN ANALYSIS:

The discussion about fence standards is directly related to the Community Investment Strategic

Priority in Goal 4, “Manage growth to provide places that are timeless and loved by the community,” and specifically Strategy 2:

Goal 4, Strategy 2: Continually monitor and update the community’s land use to reflect the community’s preferences for how new growth areas will look, feel, and function. (As the City grows in response to market demand, this supports the creation of urban form and architectural character informed by the community’s vision for its future.)

Comment: The discussion about the TOD fence standards was initiated based on feedback and input from School District #6, the Public Works Department and recent staff experience and observation. From time to time, community preferences and/or construction or development practices may necessitate re-evaluation and adjustment of our local standards. This discussion and any subsequent code amendments reflect the City’s commitment to monitoring and adjusting standards when needed.

STAFF RECOMMENDATION:

Provide feedback and direction relative to the TOD fence standards concerning height and building materials.

RECOMMENDED MOTION:

This is a discussion item. No motion is necessary.
