
**Central Point
City Hall
541-664-3321**

City Council

Mayor

Hank Williams

Ward I

Bruce Dinger

Ward II

Michael Quilty

Ward III

Brandon Thueson

Ward IV

Allen Broderick

At Large

Rick Samuelson
Tanea Browning

Administration

Chris Clayton, City
Manager
Deanna Casey, City
Recorder

**Community
Development**

Tom Humphrey,
Director

Finance

Bev Adams, Director

Human Resources

Barb Robson, Director

**Parks and Public
Works**

Matt Samitore,
Director
Jennifer Boardman,
Manager

Police

Kris Allison Chief

**CITY OF CENTRAL POINT
Study Session
September 21, 2015**

I. REGULAR MEETING CALLED TO ORDER – 6:00 P.M.

II. DISCUSSION ITEMS

A. Discussion of the E. Pine Street Corridor Refinement Plan.

III. ADJOURNMENT

WORK SESSIONS ARE INTENDED FOR DISCUSSION. NO ACTION WILL BE TAKEN ON THE AGENDA ITEMS AND NO DECISIONS WILL BE MADE. NO ORAL OR WRITTEN TESTIMONY WILL BE HEARD OR RECEIVED FROM THE PUBLIC.

RESOLUTION NO. 1371

**A RESOLUTION ADOPTING THE EAST PINE STREET CORRIDOR REFINEMENT
PLAN DATED JANUARY 2013 WITH THE PREFERRED ALTERNATIVE: ENHANCED FOUR-LANE
EAST PINE STREET IN THE CITY OF CENTRAL POINT**

RECITALS:

- A. The Central Point City Council (the "Council") embarked on this corridor refinement plan to objectively evaluate the multimodal performance of alternative design options and to develop consensus on a preferred plan for East Pine Street that is consistent with the community's vision and policies; and
- B. It is the intent of the Council to use the outcome of the analysis and conclusions of the East Pine Street Corridor Refinement Plan to provide direction for the City's Urban Renewal efforts in the future; and
- C. The Council's action to adopt the above described plan is the first of several steps to institutionalize the East Pine Street Corridor Refinement Plan for use in Urban Renewal and Transportation planning; and

The City of Central Point resolves as follows:

Section 1: The City Council of the City of Central Point, Oregon adopts the East Pine Street Corridor Refinement Plan of January 2013 with the Preferred Alternative, Enhanced Four-Lane Street.

Passed by the Council and signed by me in authentication of its passage this 27th day of July, 2013.

Bruce Dingler
Mayor Hank Williams
Council President Bruce Dingler

ATTEST: Deanna Casey
City Recorder

Resolution No. 1371
06272013

CITY OF CENTRAL POINT
City Council Meeting Minutes
June 27, 2013

I. REGULAR MEETING CALLED TO ORDER

Council President Bruce Dingle called the meeting to order at 7:00 p.m.

II. PLEDGE OF ALLEGIANCE

III. ROLL CALL: Mayor: Hank Williams, Excused.

Council Members: Bruce Dingle, Kelly Geiger, Rick Samuelson, David Douglas, and Ellie George were present. Allen Broderick was excused.

City Manager Chris Clayton; City Attorney Paul Nolte; Police Chief Kris Allison; Community Development Director Tom Humphrey; Human Resource Director Barb Robson; Parks and Public Works Director Matt Samitore; and City Recorder Deanna Casey were also present.

IV. PUBLIC APPEARANCES

Kay Harrison, City Representative to Jackson County Fair Board Mrs. Harrison presented the Fair Board report. They were pleased with the attendance at the rodeo this year. There was a good turn out and this continues to be a good event for the Expo and Central Point. They are currently working on the Red, White, and Boom for the 4th of July and then on to the Jackson County Fair. There will be changes this year for the fair trying to make it successful. She enjoys being the city representative but they have changed their meeting times and can no longer attend. Kelly Geiger stated that he is the Council Representative.

V. SPECIAL PRESENTATION

City Manager Chris Clayton recognized City Attorney Paul Nolte. Mr. Nolte is retiring from municipal service and this is his last meeting as our City Attorney. He was presented a special City of Central Point glass plate and thanked for all his service to the City.

V. CONSENT AGENDA

- A. Approval of June 13, 2013 City Council Minutes
- B. Cancellation of July 11, 2013 Council Meeting
- C. Acceptance of the Annual Safety Committee Report

Kelly Geiger made a motion to approve the consent agenda as presented. David Douglas seconded. Roll call: Bruce Dingle, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

VI. ITEMS REMOVED FROM CONSENT AGENDA - None

VII. PUBLIC HEARINGS, ORDINANCES AND RESOLUTIONS

A. Ordinance No. 1973, An Ordinance Updating and Adopting the Central Point Comprehensive Plan Economic Element

Community Development Director Tom Humphrey stated that this is a second reading of an ordinance updating the Comprehensive Plan. This is a pre-requisite to expanding the UGB and demonstrating the need for additional industrial lands.

The City Council and Planning Commission have conducted public hearings on this topic. There have been no recommended changes to the proposed plan. Comments from the public meetings have been incorporated into the final draft of the Economic Element.

Ellie George made a motion to approve Ordinance No. 1973, Updating and Adopting the Central Point Comprehensive Plan Economic Element. Rick Samuelson seconded. Roll call: Bruce Dingler, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

B. Resolution No. 1368, Adopting General Procedures for Fiscal Year 2013-2014.

City Manager Chris Clayton explained that the proposed resolution is an annual item brought before the Council as a house keeping issue. The General Procedures resolution appoints specific individuals or firms to represent the city in the capacity of City Attorney, City Engineer, City Auditor, Municipal Judge, and Insurance Agent of Record. This resolution also establishes and regulates council expenses; designation of depository; authority to keep, invest, transfer and expend funds; accounting principles; interfund and overhead fees; outstanding checks; capital acquisition; summons, complaints, and tort claim notices.

The retirement of City Attorney Paul Nottle has prompted the appointment of Sydnee Dreyer, and the Municipal Court consolidation with Jackson County designates Joe Charter as Municipal Court Judge. The city insurance agent has been bought by Brown and Brown; and the City Engineer has changed to Jeff Ballard dba RH2.

Rick Samuelson made a motion to approve Resolution No. 1368, Adopting General Procedures for Fiscal Year 2013-2014 with the recommended changes. Ellie George seconded. Roll call: Bruce Dingler, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

C. Resolution No. 1369, Revising the Classification Pay Plan

Human Resources Director Barb Robson presented the revised Classification Pay Plan. Staff is currently in negotiations with the Police Bargaining unit, the pay plan does not include any changes to the Police information.

Part A of the pay plan has been increased by a 1.6% cost of living increase pursuant to the contract ratified in September 2012. The list of classification titles has been updated to reflect current positions.

Part B of the plan is unchanged. The city will return with a revised pay plan when negotiations are settled if necessary.

Part C of the plan is for exempt, non-bargaining unit, management positions. There are no changes in salary range amounts. The plan does reflect changes in positions by adding the Building Division Manager, removing the Assistant City Manager, and changing the IT Manager position to IT Director.

Council asked for more clarification on the change for the IT Manager to Director. Mrs. Robson stated that the changes are to better clarify the position. The IT Manager originally reported to the Assistant City Manager, he now is a direct report to the City Manager and supervises another employee.

David Douglas made a motion to approve Resolution No. 1369, Revising the Classification Pay Plan. Ellie George seconded. Roll call: Bruce Dinger, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

D. Resolution No. 1370, Extending Workers' Compensation Coverage to Volunteers for the City of Central Point

Mrs. Robson stated this continues the current practice of providing workers' compensation coverage for city volunteers, with the exception of special event volunteers who are covered under a separate policy.

Rick Samuelson made a motion to approve Resolution No. 1370, Extending Workers' Compensation Coverage to Volunteers for the City of Central Point. Ellie George seconded. Roll call: Bruce Dinger, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

E. Resolution No. 1371, A Resolution Adopting the East Pine Street Corridor Refinement Plan Dated January 2013 with the preferred alternative: Enhanced Four-Lane East Pine Street in the City of Central Point.

Council President Dinger recommended postponing this item until the August 8, 2013 Council meeting when all members should be present. Several Council Members objected to removing the item from the agenda and would like to discuss it tonight. There is no guarantee that all council members will be present at the August meeting.

Community Development Director Tom Humphrey stated that the city hired transportation consultants to evaluate ways to make Central Point's Downtown more safe and attractive while preserving Pine Street's transportation function. Streetscape Design Alternatives allowed decision makers to visualize what the changes could look like without actually making the improvements. An estimate of the cost associated with the improvements was also provided for the discussion.

The intention has been to use the outcome of this analysis to provide direction for the City's Urban Renewal efforts. Street improvements will be a component of a larger plan that is tied to economic revitalization involving public investment in infrastructure, private investment in property, the right combination of businesses, more effective market, etc.

The plan recommends 4 lanes and wider sidewalks in order to meet minimum conditions for a Main Street design which is a combination of safety features and amenities. Parking will be eliminated along the business district only where a bus stop would be located, a Second Street Plaza, and dedicated turn lanes on side streets. Bulb outs are recommended at 3rd Street, 5th Street, and 6th Street to improve pedestrian visibility and crossing. Ornamental street lights are recommended to match those already in place between Front Street and 1st Street.

Mr. Clayton stated that even if the resolution is approved tonight there will be plenty of opportunity for Council and public input at the design phase. There will be a shared bike/car lane in each direction.

Mr. Dingler stated that he would like the resolution to be voted on in August. Mr. Clayton explained if the resolution was approved tonight it only adopts the overall plan and the design aspect could still be discussed at a later date.

Ellie George made a motion to approve Resolution No. 1371, A Resolution Adopting the East Pine Street Corridor Refinement Plan Dated January 2013 with the preferred alternative: Enhanced Four-Lane East Pine Street in the City of Central Point. Kelly Geiger seconded. Roll call: Bruce Dingler, no; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion passed.

VIII. BUSINESS

A. Clarification of 332 N. Second Street

Parks and Public Works Director Matt Samitore explained that in May the City Council asked the Municipal Court to dismiss the derelict property issue associated with 332 N. Second Street as the property was listed with a realtor and a qualifying offer to purchase the property was accepted. The intent of the buyer at that time was to demolish the house and rebuild. That buyer has backed out of the purchase but there is another offer on the table. The potential buyer would like to refurbish the house and not demolish. Staff was concerned that the

Council intent was to have the house demolished and this offer would be in violation of the dismissed derelict structure resolution.

Patricia Curtin, John L Scott Realtor

Ms. Curtin is representing the buyer. They would like to work with the city on the property clean up. They plan to start with the outside of the building to improve the property. The first improvements would be to paint and make the property acceptable. They do intend to work with the building department on permits. She will also recommend that the City Building Inspector be allowed to do an initial inspection and report back to the city.

The Council was in agreement that refurbishing the house would be acceptable as long as there were improvements are done in a timely manner. Council directed staff to have the Building Official do a full inspection and provide a report to the Council at the July 25, 2013 meeting. At that time the Council will decide if the derelict structure process should start over in order to encourage the new owners to fix the property.

IX. MAYOR'S REPORT – No Mayor's Report

X. CITY MANAGER'S REPORT

City Manager Chris Clayton reported that

- Senate Bill 822 addressing PERS rates has passed. Our rates will be reduced for a couple of years helping the general fund carry over.
- He has received word from the County Administrator that the outlying Libraries will be closing in 2014 with the Medford branch closing in 2015 because of lack of available funding. He is in discussion with the County regarding optional uses for the vacant library.
- The July 11, 2013 Council meeting was cancelled with the approval of the consent agenda. There will be a Study Session on July 15, a Development Commission meeting and Council Meeting on July 25th.
- He will be at the OCCMA Conference in Bend the week of July 8th.
- Staff is working on the Sidewalk Café amendments to the Code allowing seating on the sidewalk and the ability to serve alcohol with restrictions.
- Dairy Queen has withdrawn their land use application; they feel there is not enough traffic in that location.
- The Municipal Court will be at the County Court room starting July 1st.
- Sydnee Dreyer will be our new City Attorney, she is present tonight. We are sorry to see Mr. Nolte retire but confident Mrs. Dreyer will be a good addition to the City.

XI. COUNCIL REPORTS

Council President Bruce Dingler asked what the next steps would be for the E. Pine Street Refinement Program. Mr. Clayton explained that staff will proceed with the plan that was just adopted. After funding is acquired there will be time for public comment and specific design items. This plan will overlap into the Urban Renewal Plan.

Council Member Ellie George had no report.

Council Member David Douglas reported that he attended the Battle of the Bones and it was a great event. He is currently working with City Staff on the Freedom Run and the 4th of July events.

Council Member Rick Samuelson reported that he attended the Battle of the Bones event and had a great time. It is a great event for the City. He also attended an RVCOG meeting where they discussed the Medford Water Commission on a Regional level.

Council Member Kelly Geiger stated that he attended the Battle of the Bones and it was very well organized and orderly. This is great exposure for the City and all who attend have a good time. He also attended the SOREDI Annual Dinner on the Rogue.

XII. DEPARTMENT REPORTS

Human Resource Director Barb Robson reported that she has been working with the Department Directors on moving employees around and filling empty positions.

Parks and Public Works Director Matt Samitore reported that:

- The Battle of the Bones went well. He thanked the Council for their support of the event.
- There will be discussions regarding the second and third options for a community center with Dennis Richardson.
- The Front Street improvements are on schedule and should be complete soon.

Police Chief Kris Allison reported that the Battle of the Bones event went well. There were no issues of significance. Staff is getting ready for the 4th of July events. Community Service Officer Derreck Moore has been working on Neighborhood Watch meetings around town.

Community Development Director Tom Humphrey reported that:

- Staff is working on the sidewalk cafe additions to the code book which would allow for serving alcohol when associated with a family type restaurant.
- The big tent at the Grange Co-op is for a temporary sale event.
- The City will be presenting a gift basket and check to the Quarter Horse Association tomorrow if any Council Members would like join him and Mr. Clayton.
- There will be a Planning Commission meeting in July and August to discuss several Ordinance amendments.

XIII. EXECUTIVE SESSION – ORS 192.660 (2)(d) Labor Negotiations

Kelly Geiger made a motion to adjourn into executive session under ORS 192.660 (2)(d) Labor Negotiations. Ellie George seconded. Roll call: Bruce Dingler, yes; David Douglas, yes; Kelly Geiger, yes; Ellie George, yes; and Rick Samuelson, yes. Motion approved.

Council went into Executive Session at 8:45 to discuss the Police Department Labor Negotiations. They returned to regular session at 9:06 p.m. no action was taken.

XIV. ADJOURNMENT

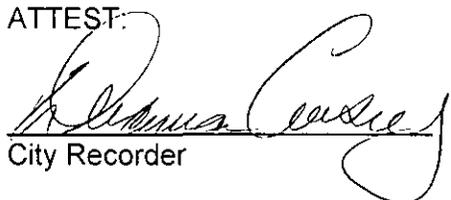
Rick Samuelson moved to adjourn, Kelly Geiger seconded, all said "aye" and the Council Meeting was adjourned at 9:10 p.m.

The foregoing minutes of the June 27, 2013, Council meeting were approved by the City Council at its meeting of July 25, 2013.

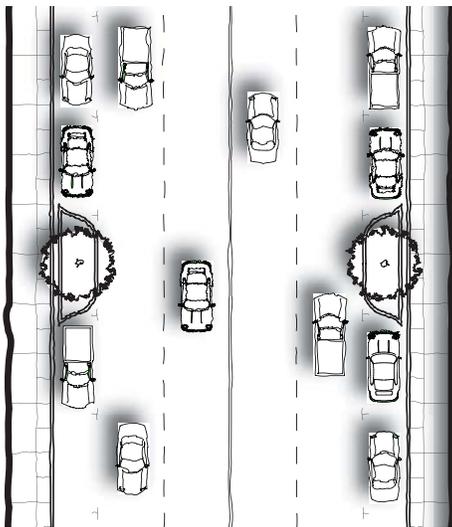
Dated: 7/25/13


Mayor Hank Williams

ATTEST:


City Recorder

East Pine Street Corridor Refinement Plan Streetscape Design Alternatives



City of Central Point
Tech Memo 6

July 2012

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The East Pine Street Corridor Refinement Plan (Plan) is an opportunity to identify solutions for recognized problems with the current configuration and conditions of Pine Street, and to address aspiration for revitalization of the downtown area. The following challenges were key to initiating the project, and confirmed by technical analysis and community input.

Vehicular Safety. Traffic often moves at a fast rate and motorists change lanes frequently to avoid vehicles making left-hand turns. Intersections along this corridor have the highest crash rates in the City.

Pedestrian Safety. Pedestrian crossing on Pine Street can be difficult and dependent upon drivers observing pedestrians and stopping to allow them to cross. This is a critical safety issue for Crater High School and Central Point Elementary School students crossing the street.

Bicycle Safety. There are limited bicycle facilities on Pine Street even though it is a designated bicycle route. Cyclists must ride in the flow of automobile traffic resulting in greater risk of bicycle-vehicle collisions.

Sidewalks and Storefront Activity. Existing sidewalks are narrow, which limits the ability to implement a streetscape design that will make the downtown area more attractive.

DOWNTOWN REVITALIZATION

Community visioning and city policies are supportive of a revitalized downtown with Pine Street as an attractive setting for walking and shopping. At the policy level, the *Central Point Downtown Revitalization Plan, 2000* and *Central Point Forward: Fair City Vision 2020* set forth goals for downtown revitalization and recommend strategies for meeting those goals. The goals share a common vision of downtown as the heart of the community, with a diversity of business and economic activity, and Pine Street as a comfortable environment for pedestrians.

WHAT IS A MAIN STREET?

Traditionally, Main Street is the most important street in town. It is a good address for businesses, and creates an identity for the community. It tells residents and visitors alike something about the place, its people, and its history. When communities anywhere set forth a vision for downtown revitalization, they nearly always articulate a desire for a good Main Street.

By design, a Main Street becomes more than a means to get to places - it becomes a place. It is actually a balance between two kinds of places. One is a place to walk around and look. It becomes a social space, as well as a business space. The other is a place to drive to and park. When a Main Street effectively strikes that balance, it will become the heart of the community and a center of commerce.

WHAT DOES A MAIN STREET LOOK LIKE?

A Main Street has short blocks, and is usually three to five blocks long. Buildings are close to the sidewalk and one another. There is a variety of businesses and, ideally, a few places to live. Traffic speeds are reduced and the street is easy to cross for people of all ages and abilities. The sidewalks are broad enough for two people to comfortably walk side by side. Attention has been given to storefronts, window displays, and the streetscape design (Figure 1).



Pine Street today

INTRODUCTION

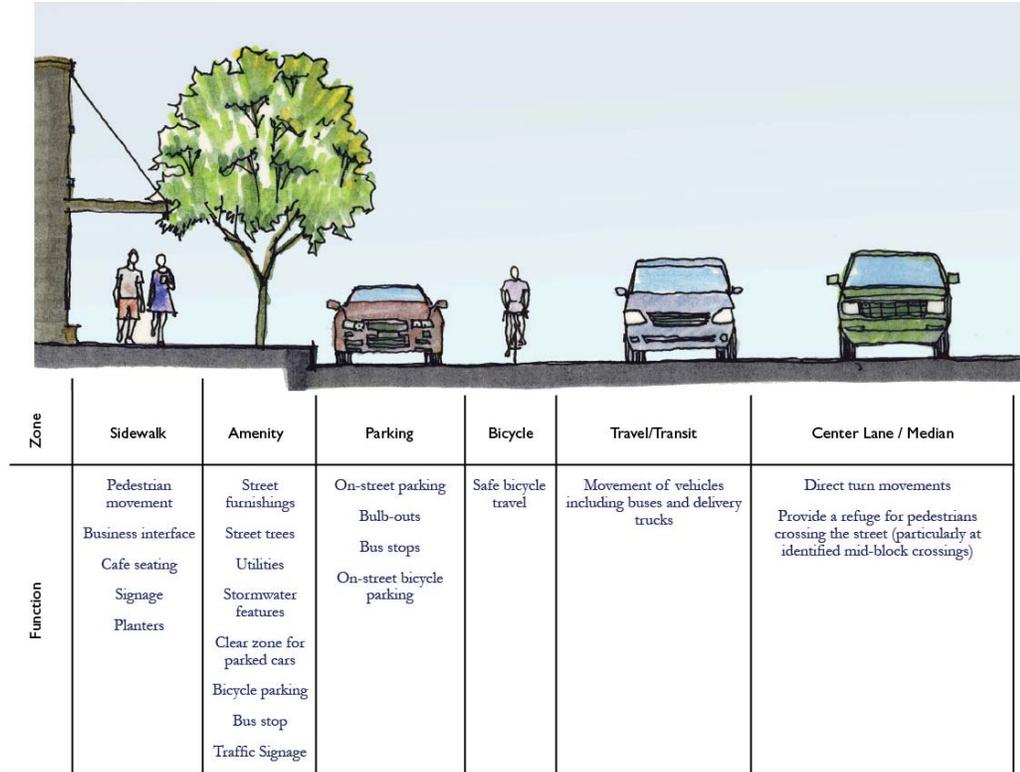


Figure 1: Main Street Zones

MAIN STREET DESIGN

The underlying goal of Main Street design is a street for everyone (Figure 1). This approach recognizes the need for safe and efficient operation of vehicles, while striving to balance transportation choices and improve mobility for everyone. The distinguishing characteristics of Main Street are in the sidewalk and amenity zones, the ease of pedestrian movement, and diversity of businesses and storefronts along the street. Vehicle operations and safety are also important. The roadway area between the sidewalks includes on-street parking, travel lanes, turn lanes, intersections, and sometimes medians and pedestrian refuges.

SIDEWALK AND AMENITY ZONES

These zones are typically designed as a single element, and are the primary aspects of an attractive pedestrian environment. Pedestrian comfort is strongly related to the width of the sidewalk, buffering from traffic provided by an amenity zone,

and qualities of building fronts along the street. Together the sidewalk and amenity zones provide access to businesses and support community interaction in comfortable social spaces along the street.

Sidewalk Zone. Sidewalks serve multiple functions. It is important they be designed to support the activities and features expected of Main Street. At a minimum, they need to provide continuous and unobstructed walking space of at least 5-feet in width to meet contemporary ADA requirements. However, for a downtown or a central business district, where there are greater expectations for storefront and pedestrian activities, a minimum width of 10 to 12-feet is preferred.

Amenity Zone. The amenity zone should complement the sidewalk zone. A minimum width of 4-feet is needed to accommodate street furnishings without encroachment into the sidewalk zone. An amenity zone also provides space for passenger loading and unloading from on-street parking. If there is a bus stop, a 5-foot by 8-foot clear loading area must be provided to meet ADA standards.

Street Furnishings. Street furnishings located in the amenity zone play an important role in creating a positive pedestrian environment and downtown identity. A comprehensive plan should be developed based on a fairly simple palette of benches, bike racks, kiosks, lighting, etc. Furnishings should never obstruct the minimum clear zone for the sidewalk. It is also best if the location of street trees and light poles are coordinated with the marked spaces for on-street parking to avoid conflicts with opening of doors.

Street Trees. Street trees are also an important element of an attractive streetscape. A growing body of research indicates a clear relationship between the presence of street trees and favorable perceptions of a downtown or central business district, as well as more favorable descriptions of the shopping experience. Trees need adequate room to thrive and a plan for maintenance. If an amenity zone has a width of less than 4-feet, trees should not be considered.

PARKING ZONE

On-street parking is critical to a successful downtown. It is as much a part of the place as walking. The parking zone also provides a buffer between pedestrian movement and social interactions of the sidewalk and moving vehicles in the roadway. If vehicle speeds are reliably less than 30 mph, the width of the parking zone can be as little as 7-feet. However, maintaining an 8-foot width is usually a safer design, especially if bicyclists are expected to share the adjacent travel lane.

INTERSECTIONS

Intersection design is complex, and is often completed on a case-by-case basis. A number of factors need to be considered such as: traffic speeds, pedestrian visibility and crossing distances, expectations regarding bicycle use, and expectations about vehicle operations including large vehicles. For pedestrians, how they are accommodated at intersections is as important as the sidewalk and amenity zone. Physical design measures that support comfortable pedestrian movement include shortening the crossing distance with curb bulb-outs, reducing the curb radii, enhanced pavement markings to delineated crosswalks, and increased intersection illumination.

BICYCLE TRAVEL

None of the streetscape alternatives include a dedicated bike lane on Pine Street. Instead, each alternative recommends the use of shared lanes (sharrows) with appropriate markings in the outside lane. Pavement markings let motorists know to expect cyclists on the street and remind cyclists not to ride too close to parked cars whose doors may unexpectedly open.

While sharrow pavement markings are a nationally recognized form of traffic control for public streets and are described in the Oregon Driver Manual, their use may be new to Central Point. If early experience suggests motorists and bicyclists are not understanding the message being communicated by these symbols, it is recommended that an education campaign be employed.

In addition to sharrows, bike routes are recommended on Oak and Manzanita Streets for cyclist traveling east-west through downtown. These are low-volume streets and could be designed for efficient bike travel by reassigning stop signs to the north-south streets. No removal of a travel lane or on-street parking would be required.

INTRODUCTION

EAST PINE STREET ALTERNATIVES

The streetscape design alternatives illustrated on the following pages explore options to make Pine Street a street for everyone; balancing the needs of traffic capacity and operations, and the needs of bicyclists and pedestrians. Recommended design elements are also intended to satisfy the stated local aspirations for a more attractive streetscape through incorporation of amenities such as street trees, ornamental lighting, and street furniture. The alternatives also reflect challenges previously noted, and the technical analysis confirms it is operationally feasible to reconfigure a portion of Pine Street from four lanes to three lanes. Careful attention has been given to the need for safe travel for all modes, and to accommodate emergency vehicles and oversized vehicles such as buses and freight. On-street parking is retained in each alternative and several measures are proposed to improve vehicle safety and operations.

Alternative A - 1st Street through 6th Street.

Retains the current four-lane configuration and travel lane widths, with sidewalks remaining at current widths.

Alternative B - 1st Street through 6th Street.

Retains a four-lane configuration but with a one-foot reduction in lane widths, which allows for the construction of slightly wider sidewalks.

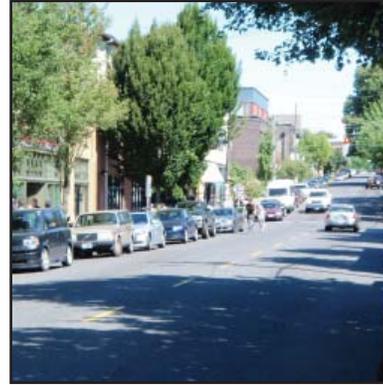
Alternative C - 1st Street through 6th Street.

Assumes the reconfiguration of Pine Street as a three-lane roadway in this segment, with one travel lane in each direction and a continuous center turn lane. This would allow for a significant widening of the sidewalks.

7th Street through 10th Street Improvements.

For each alternative, sidewalk improvements could occur in this segment by obtaining up to two-feet of additional right-of-way or easements from the front yard setbacks of existing properties. In Alternative C, the 8th Street to 7th Street block would be used as the transition from four to three lanes.

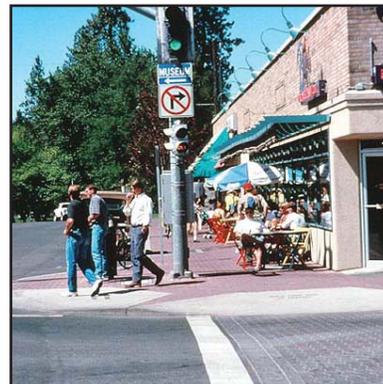
Streets for Everyone



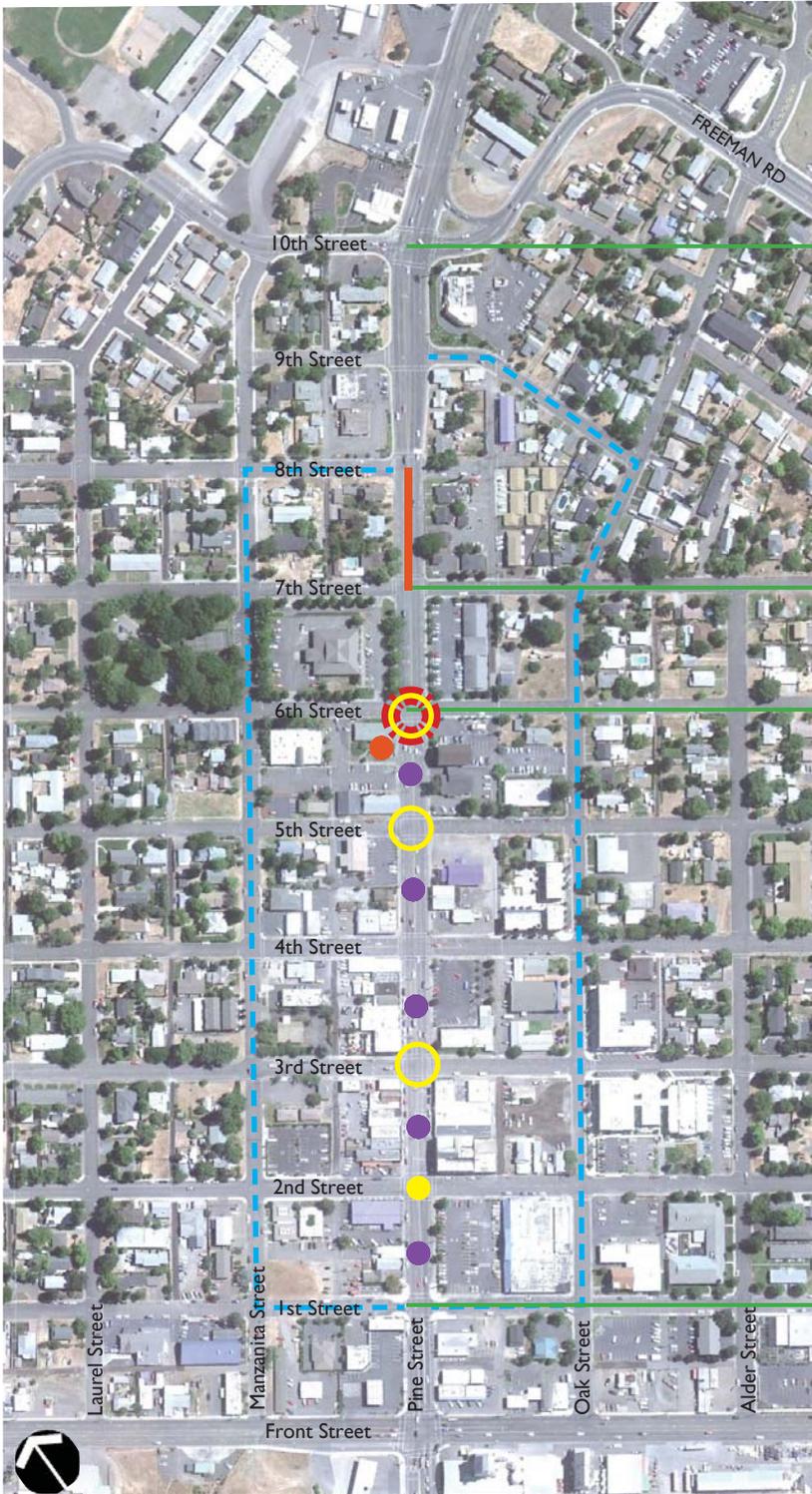
Driving and parking



Riding a bike



Walking around and sitting down



Pine Street
Blocks 6th-10th
Improvements
(Page 11)

Alternative A (Page 6)

Alternative B (Page 12)

Alternative C (Page 18)



Enhanced Pedestrian Crossing at 6th St.



Enhanced Crosswalks with Special Paving & New Bulb-outs at Intersections



Bike Routes on Oak St. & Manzanita St.



Mid-block Bulb-out (Alternative A only)



Transition from 3 to 4 Lanes (Alternative C only)

2nd St. Improvements (Page 24)

Enhanced Bus Bulb-out (Page 17)

ALTERNATIVE A - RECOMMENDED DESIGN ELEMENTS

Roadway and Intersections

Four travel lanes would be maintained at their existing widths (12-feet), with no reconstruction of the existing curb. Intersection bulb-outs are recommended at 3rd Street, 5th Street and 6th Street to improve pedestrian visibility and crossing. Design of the bulb-outs should conform to technical analysis of Technical Memorandum 4 with respect to size and turning radii. Intersection bulb-outs are not recommended at 2nd and 4th Streets in order to accommodate truck and bus turning movements even though pedestrian crossing counts are relatively high at 2nd Street. Specially paved crosswalks should be added at each intersection, using durable concrete materials rather than stamped concrete or thermoplastic treatments.

Sidewalk and Amenity Zones

Widths of the sidewalk and amenity zones remains unchanged at approximately 8-feet total. This is a constrained condition that falls short of optimal Main Street design. An amenity zone of 3-feet will not support street trees. The 5-foot width the of sidewalk meets ADA requirements but does not allow two people to walk comfortably side-by-side. Sidewalks should be reconstructed to a consistent concrete finish, using an attractive scoring pattern throughout. The number of signs located in the sidewalk should be reduced in order to reduce visual clutter.

Street Trees and Furniture

Small curb bulb-outs could be added mid-block to accommodate street trees and understory landscape planting. Given the constrained width of the sidewalks, these bulb-outs are the only opportunity to introduce trees as a streetscape element (see page 9). Street furniture such as bike racks, benches, and vending machines will be difficult to locate in the constrained amenity zone, with the possible exception of including a small bike rack at intersection bulb-outs.

Street Lighting

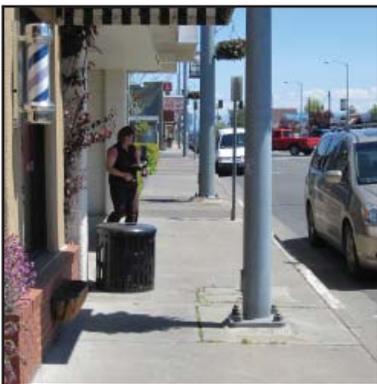
All existing street lights should be replaced by ornamental street lights to match those already in place between Front Street and 1st Street. Use two poles per corner at each intersection, and one pole on each side of the street at mid-block locations.

Parking Zone

One space per block face would be lost to construction of mid-block bulb-outs, and one space would be lost to the enhanced bus stop at 6th street, for a total loss of 11 parking spaces on Pine Street. Street corner curb bulb-outs at intersections will not reduce on-street parking.

Bicycle Facilities

Painted sharrow markings and bike racks located within the intersection bulb-outs are recommended enhancements to supplement marked bike routes on Oak and Manzanita Streets.



Existing sidewalks

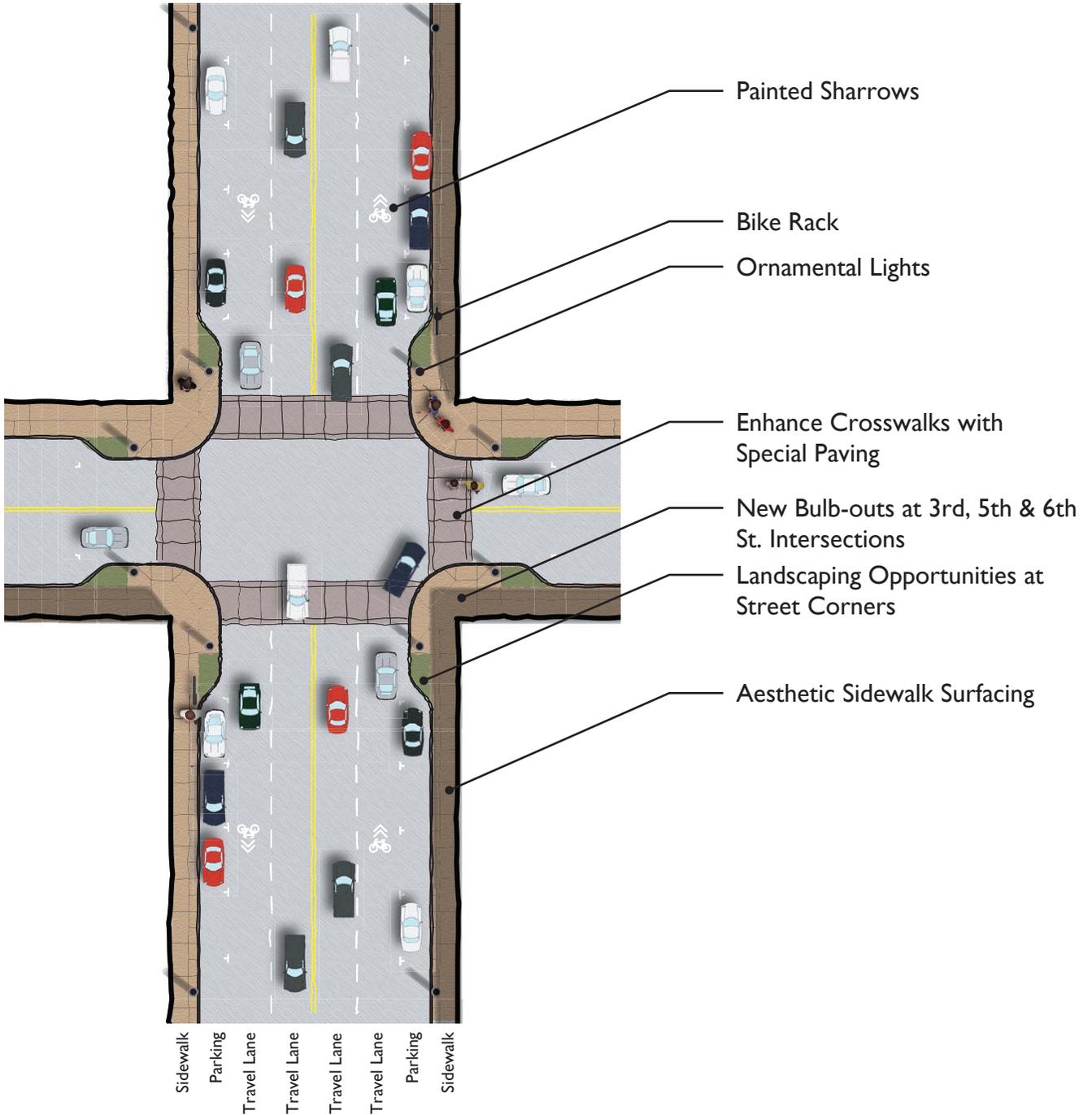
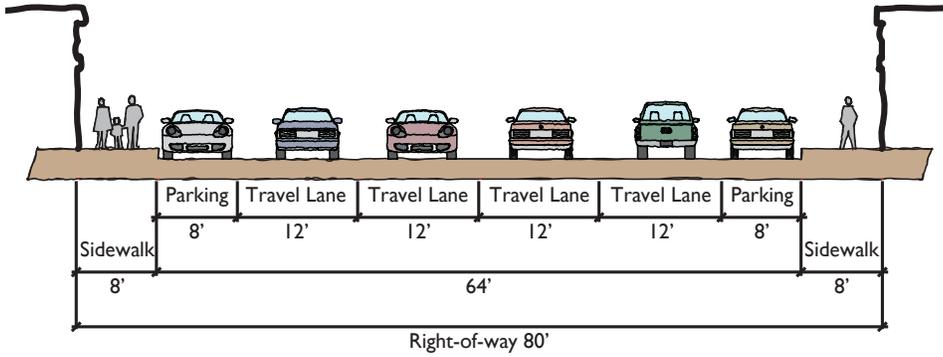


Continuous ornamental street lights



Crosswalk pavement

ALTERNATIVE A - PINE STREET BLOCKS 1ST-6TH



ALTERNATIVE A - BEFORE AND AFTER ILLUSTRATIONS

Existing Intersection Conditions



Alternative A - Intersection Improvements



Improvements:

- Enhance Crosswalks with Special Paving
- New Bulb-outs at Selected Intersections
- Ornamental Street Lights
- Bike Racks in Bulb-outs
- Sharrow Markings
- New Sidewalk Surfacing
- Clean up Visual Sign Clutter from Sidewalks

ALTERNATIVE A - BEFORE AND AFTER ILLUSTRATIONS

Existing Mid-block Conditions



Alternative A - Mid-block Improvements



Improvements:

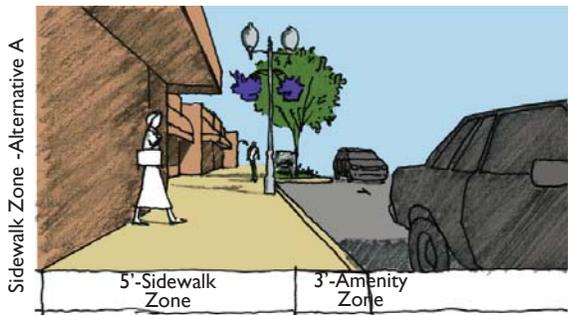
- Mid-block Bulb-out with Street and Landscaping
- Ornamental Street Lights
- Clean up Visual Sign Clutter from Sidewalks
- Sharrow Markings
- New Sidewalk Surfacing

Small bulb-outs should be constructed at mid-block, and landscaped with a street tree and low understory plantings. The bulb-out should be constructed with curbing that allows for a narrow channel between it and the sidewalk for stormwater flow. One parking space is lost per bulb-out.

ALTERNATIVE A - AXONOMETRIC

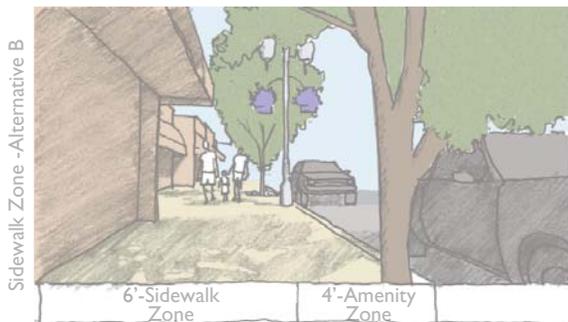


Looking East at 3rd Street



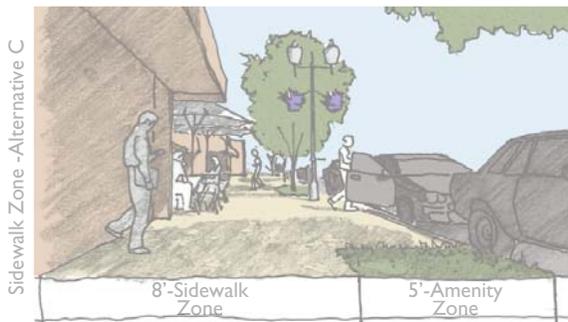
Sidewalk Zone - Alternative A

5'-Sidewalk Zone | 3'-Amenity Zone



Sidewalk Zone - Alternative B

6'-Sidewalk Zone | 4'-Amenity Zone



Sidewalk Zone - Alternative C

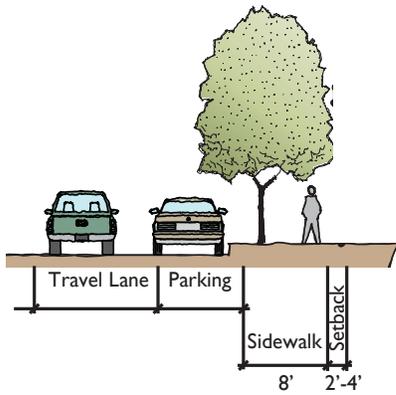
8'-Sidewalk Zone | 5'-Amenity Zone



Existing sidewalk conditions

This illustrates existing constrained conditions along most of Pine Street. Street trees, landscaping, and street furniture cannot be accommodated. The sidewalk lacks comfortable space for outdoor sitting or other storefront activities.

PINE STREET BLOCKS 6TH-10TH SIDEWALK IMPROVEMENTS



Street Trees Added as Sidewalks are Widened

New Sidewalk Surfacing

Painted Sharrows

Existing Street Lighting to Remain



Sidewalks widened to 10 -12 feet

Beyond 6th Street in Alternatives A and B, and beyond 8th Street in Alternative C, basic frontage improvements could be incrementally implemented as opportunities present themselves. Buildings along this section are setback, making it possible to widen sidewalks through additional right-of-way dedication. For these segments, no reduction in the width of roadway lanes, or in the number lanes is assumed in any alternative. Existing sidewalks could be widened to 10-feet to 12-feet in width by acquiring additional right-of-way or easements from property owners with front yard setbacks between buildings and the current sidewalks. With wider sidewalks, street trees could be introduced into the streetscape. Improvements could occur with property redevelopment or as a series of smaller capital projects carried out by the City.

ALTERNATIVE B - RECOMMENDED DESIGN ELEMENTS

Roadway and Intersections

Four travel lanes would be maintained but their widths reduced to 11-feet by constructing new curbs that are moved 2-feet into the existing roadway on each side. As with Alternative A, intersection bulb-outs are recommended at 3rd Street, 5th Street, and 6th Street to improve pedestrian visibility and crossing and should conform to the analysis of turning movements from Technical Memorandum 4. No mid-block bulb-outs are included with this alternative since street trees can be accommodated in the wider sidewalks. Intersection bulb-outs are not recommended at 2nd Street and 4th Street in order to accommodate truck and bus turning movements. Specially paved crosswalks should be added at each intersection, using durable concrete materials rather than stamped concrete or thermoplastic treatments.

Sidewalk and Amenity Zones

Sidewalk and amenity zone widths would be increased to 10-feet total as a result of reconstructing the curbs. This width provides the minimum conditions for Main Street design. The amenity zone has been increased to 4-feet, which will support street trees and other street furniture. The six-foot sidewalk width is the functional minimum for two people to comfortably walk side-by-side, but is still constrained for outdoor seating and sidewalk business displays. Sidewalks should be reconstructed to a consistent finish and pavement detail throughout.

Street Trees and Furniture

Street trees could be located in small tree wells (approximately 4-feet by 8-feet) that could be planted or finished with pervious concrete pavers set in sand to allow water infiltration to the zone. Root barriers are also recommended for each tree. Other furniture such as bike racks, benches, and vending machines may now be located in the amenity zone.

Street Lighting

All existing street lights should be replaced by ornamental street lights to match those already in place between Front Street and 1st Street. Use two poles per corner at each intersection and one pole on each side of the street at mid-block locations.

Parking Zone

One space would be lost to the enhanced bus stop at 6th Street. Street corner curb bulb-outs at intersection will not reduce on-street parking.

Bicycle Facilities

Painted sharrow markings and bike racks located within the intersection bulb-outs or the wider sidewalk amenity zone are recommended enhancements to supplement marked bike routes on Oak and Manzanita Streets.



Intersection Bulb-outs

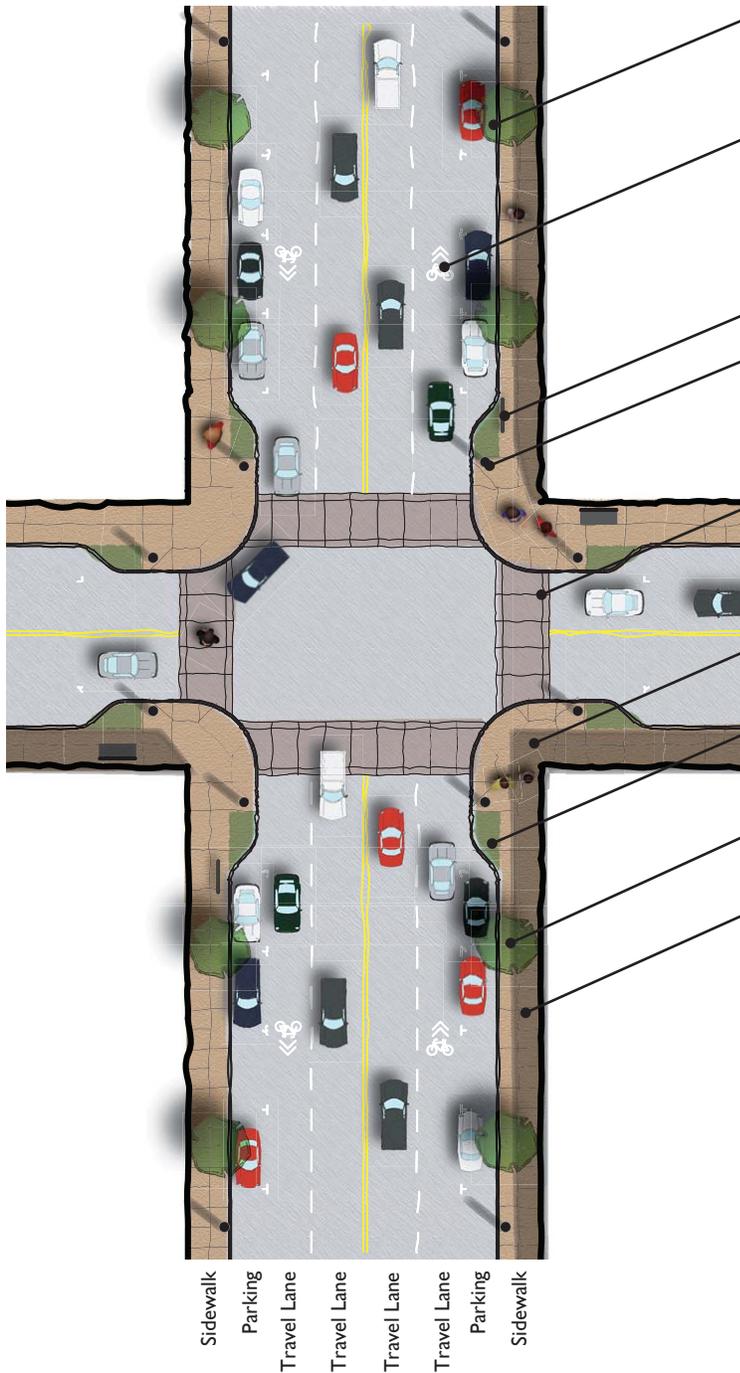
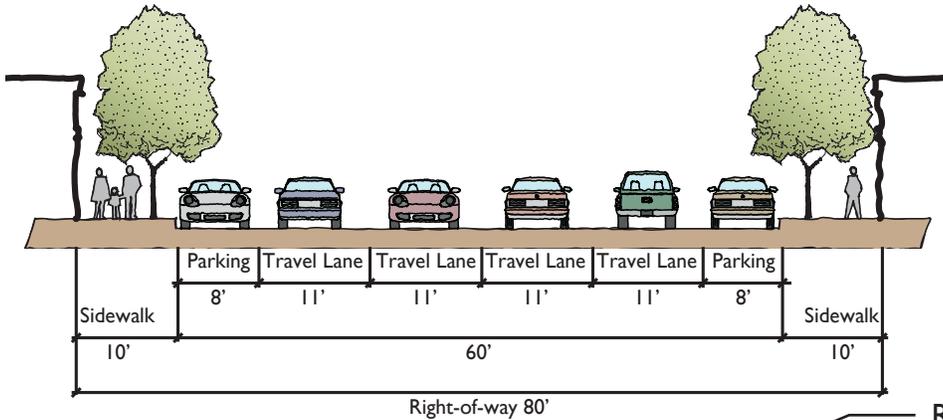


Trees in pavers



Bike racks and landscaping

ALTERNATIVE B - PINE STREET BLOCKS 1ST-6TH



- Reconstructed Curbs and 10 foot-wide Sidewalks
- Painted Sharrow
- Bike Rack
- Ornamental Lights
- Enhance Crosswalks with Special Paving
- New Bulb-outs at 3rd, 5th & 6th St. Intersections
- Enhanced Landscaping in Sidewalk Furnishing Zone
- Street Trees
- Aesthetic Sidewalk Surfacing

Sidewalk
 Parking
 Travel Lane
 Travel Lane
 Travel Lane
 Travel Lane
 Parking
 Sidewalk

ALTERNATIVE B - BEFORE AND AFTER ILLUSTRATIONS

Existing Intersection Conditions



Alternative B - Intersection Improvements



Improvements:

- Enhance Crosswalks with Special Paving
- New Bulb-outs at Selected Intersections
- Ornamental Street Lights
- Bike Racks at Bulb-outs
- Sharrow Markings
- New and Wider Sidewalks
- Street Trees in Sidewalks

ALTERNATIVE B - BEFORE AND AFTER ILLUSTRATIONS

Existing Mid-block Conditions

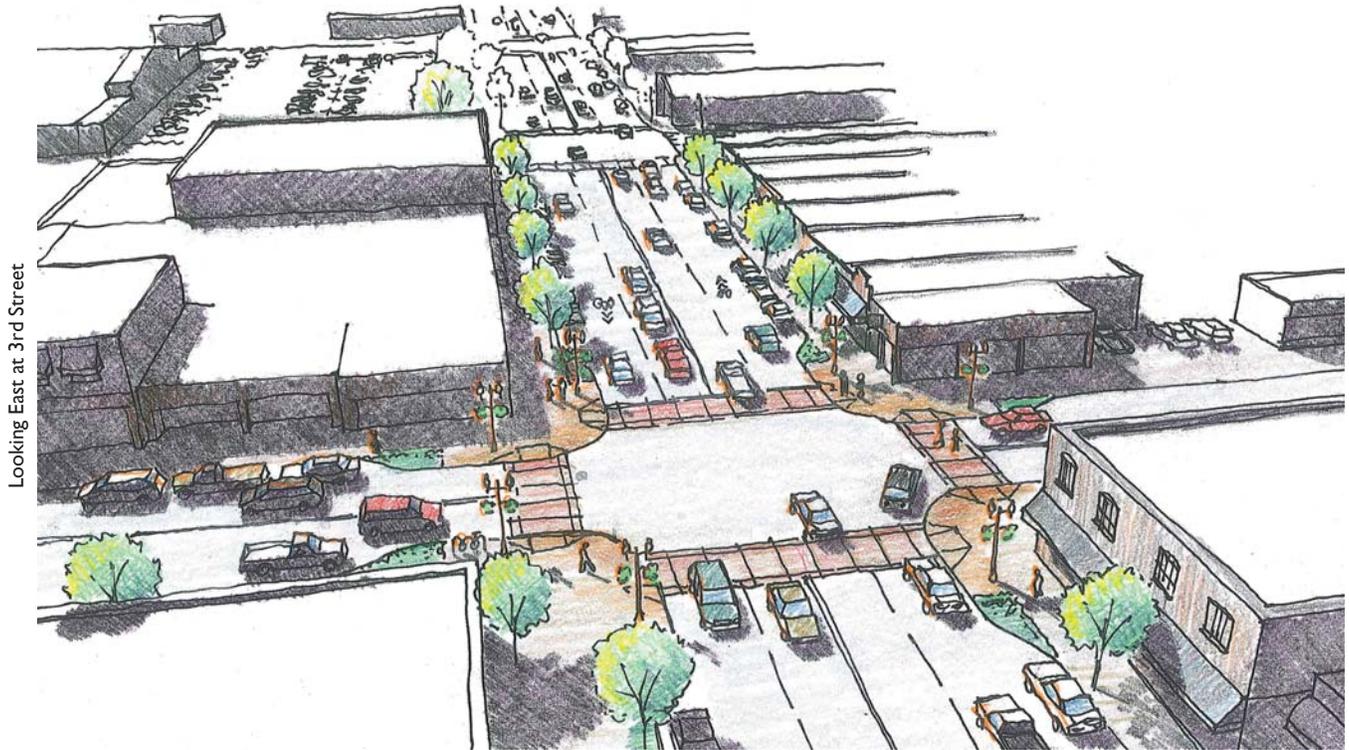


Alternative B - Mid-block Improvements

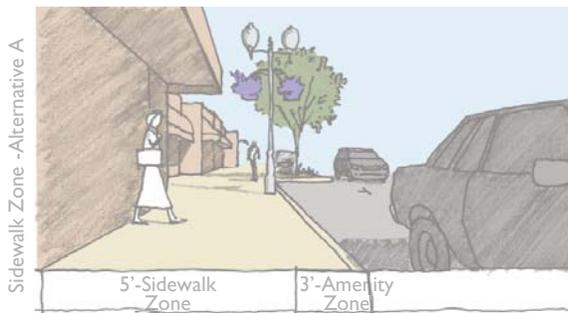


- Improvements:
- Ornamental Street Lights
 - Sharrow Markings
 - New and Wider Sidewalks
 - Street Trees

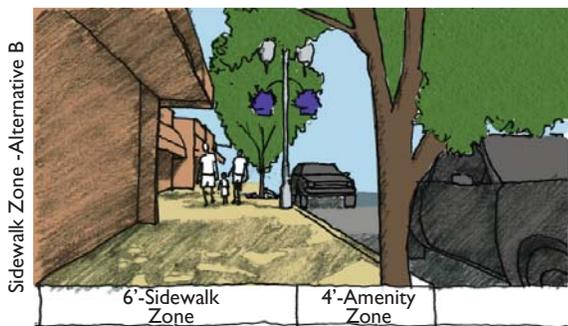
ALTERNATIVE B - AXONOMETRIC



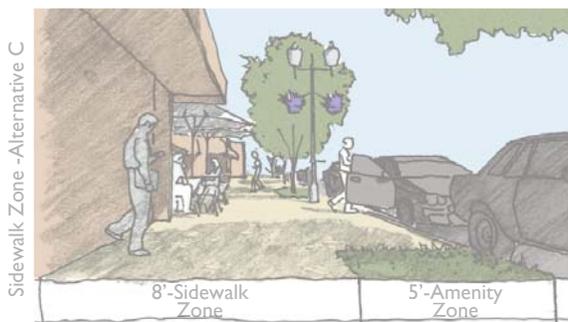
Looking East at 3rd Street



Sidewalk Zone - Alternative A



Sidewalk Zone - Alternative B



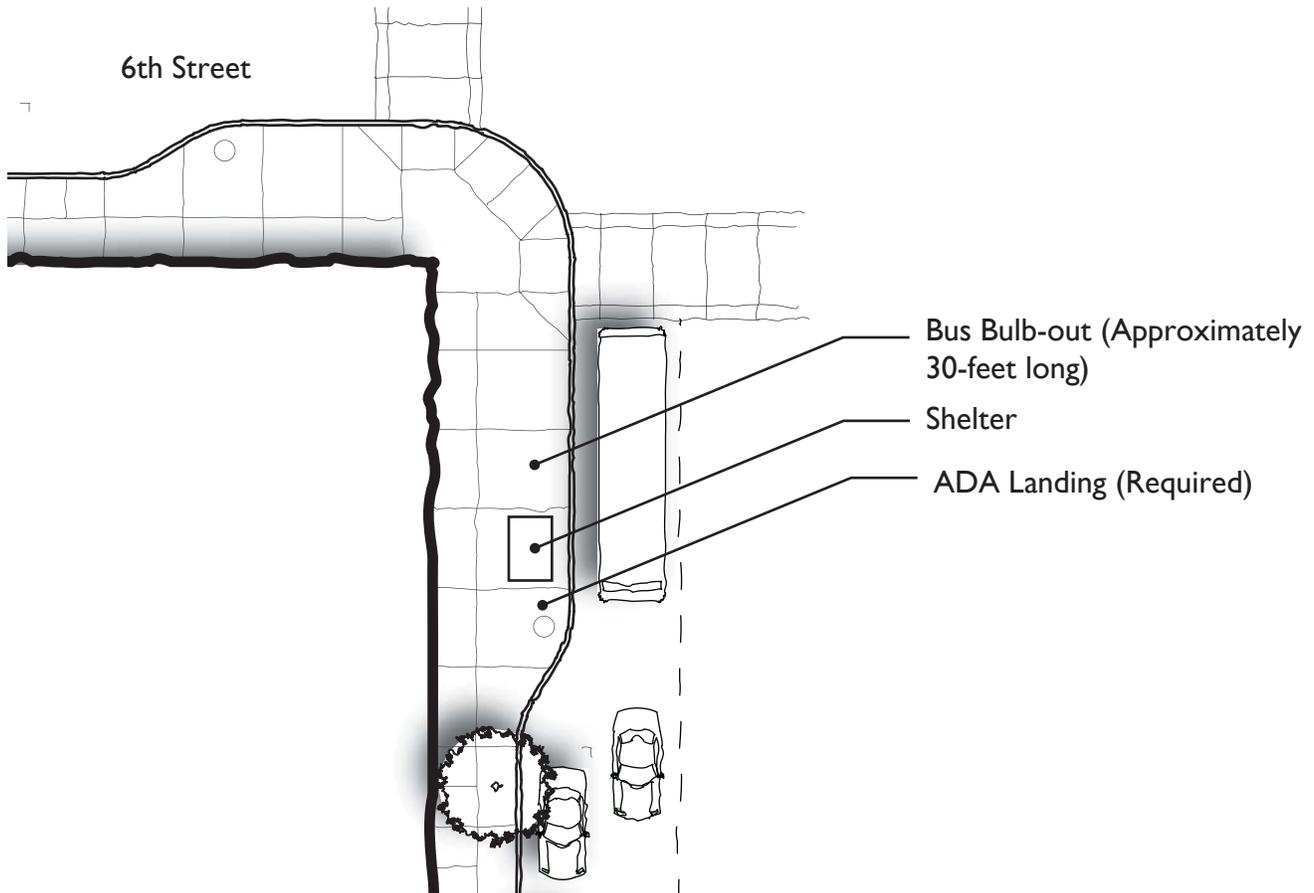
Sidewalk Zone - Alternative C



Sidewalks at minimum Main Street widths

At this width, new sidewalks are fully comfortable for two people walking side-by-side, and passing people walking in the opposite direction. Limited storefront activities may be possible. The amenity zone is at the minimum width for street trees and furniture.

ENHANCED BUS BULB-OUT ON 6TH AND PINE STREET



Transit service is likely to play an increasingly important role in Central Point. The existing bus stop at 6th and Pine Streets should be improved by: a street corner extended bulb-out of approximately 30-feet in length, sufficient to load front and rear doors of a bus; a small shelter; and an ADA compliant landing with the bulb-out and at the front door loading area. Smaller bulb-outs should be constructed at the other three corners of the intersection with specially paved crosswalks.

ALTERNATIVE C - RECOMMENDED DESIGN ELEMENTS

Roadway and Intersections

The roadway would be reduced to the three lanes with a single travel lane in each direction and a continuous center lane between 1st and 7th Streets. The roadway transition from four lanes to three lanes would occur between 8th Street and 7th Street. New curbs would be constructed to define this narrower roadway cross-section. As with the other alternatives, intersection bulb-outs are recommended at 3rd Street, 5th Street, and 6th Street to improve pedestrian visibility and crossing, and should conform to the analysis of turning movements from Technical Memorandum 4. Specially paved crosswalks should be added at each intersection, using durable concrete materials rather than stamped concrete or thermoplastic treatments. Crossing distances for pedestrians would now be significantly shorter.

Sidewalk and Amenity Zones

Widths of the sidewalk and amenity zones can be increased to 13-feet total with construction of the new curbs. This is an optimal Main Street design. A wider amenity zone will support a greater variety of streetscape elements including Green Street planters if the City desires a demonstration project for innovative stormwater management. The sidewalk width allows multiple people to walk comfortably side-by-side, and will support outdoor seating and outside displays for businesses.

Street Trees and Furniture

Furniture such as bike racks, benches, and vending machines could easily be located in the amenity zone, along with street trees, landscaping, and ornamental street lights. Street lights and street trees should be located at the beginning and end of on-street parking to avoid conflicts.

Street Lighting

All existing street lights should be replaced by ornamental street lights to match those already in place. Use two poles per corner at each intersection, and for this alternative two mid-block poles on each side are recommended.

Parking Zone

One space would be lost to the enhanced bus stop at 6th Street. Street corner curb bulb-outs at intersections will not reduce on-street parking.

Bicycle Facilities

Painted sharrow markings and bike racks located within the intersection bulb-outs, or the wider sidewalk amenity zone, are recommended enhancements to supplement marked bike racks on Oak and Manzanita Streets.



Trees in planters

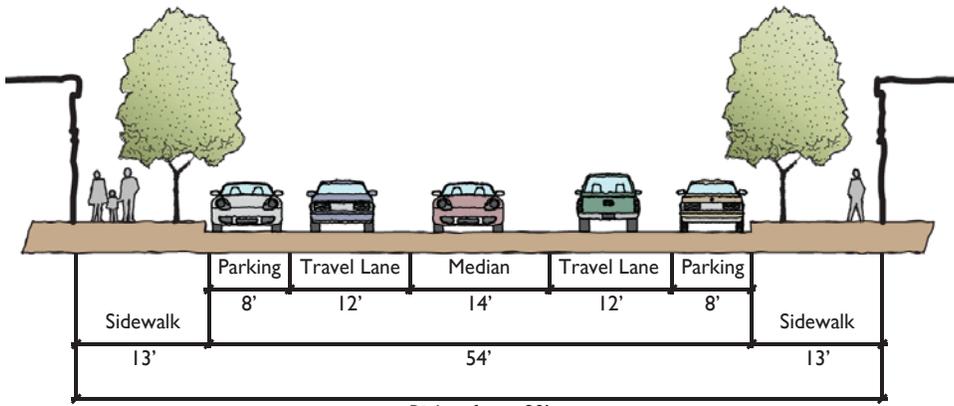


Trees in grates

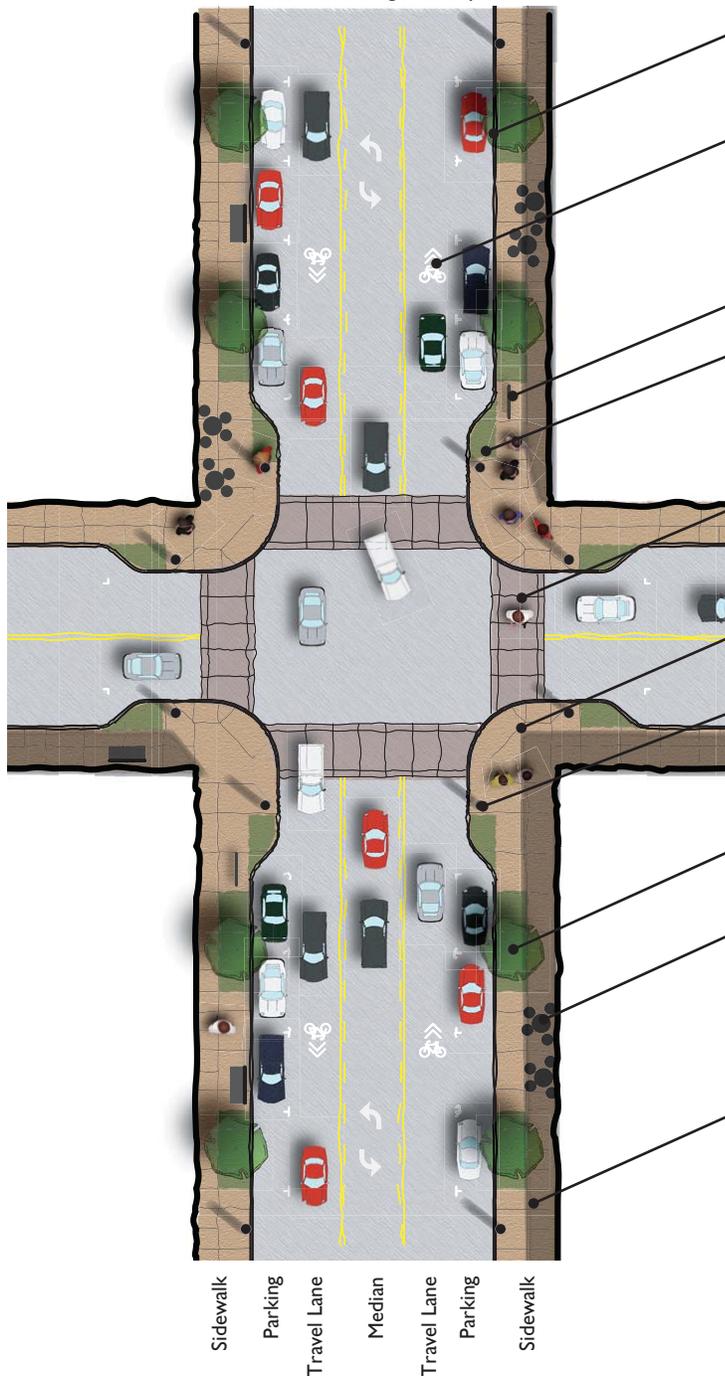


Sidewalk seating

ALTERNATIVE C - PINE STREET BLOCKS 1ST-6TH



Right-of-way 80'



- Reconstructed Curb and 13-Foot Sidewalks
- Painted Sharrows
- Bike Rack
- Enhanced Landscaping in Sidewalk Amenity Zone
- Enhance Crosswalks with Special Paving
- New Bulb-outs at 3rd, 5th & 6th St. Intersections
- Ornamental Lights
- Street Trees
- Potential for Outdoor Seating
- Aesthetic Sidewalk Surfacing

Sidewalk
Parking
Travel Lane
Median
Travel Lane
Parking
Sidewalk

ALTERNATIVE C - BEFORE AND AFTER ILLUSTRATIONS

Existing Intersection Conditions with 4-Lanes



Alternative C - Intersection Improvements with 3-Lane Street



Improvements:

- Enhance Crosswalks with Special Paving
- New Bulb-outs at Selected Intersections
- Ornamental Street Lights
- Bike Racks
- Sharrow Markings
- New and Wider Sidewalks
- Street Trees
- Enhanced Sidewalk Planting
- Sidewalks with Cafe Tables and Benches
- Green Street Stormwater Features (Optional)

ALTERNATIVE C - BEFORE AND AFTER ILLUSTRATIONS

Existing Mid-block Conditions with 4-Lanes



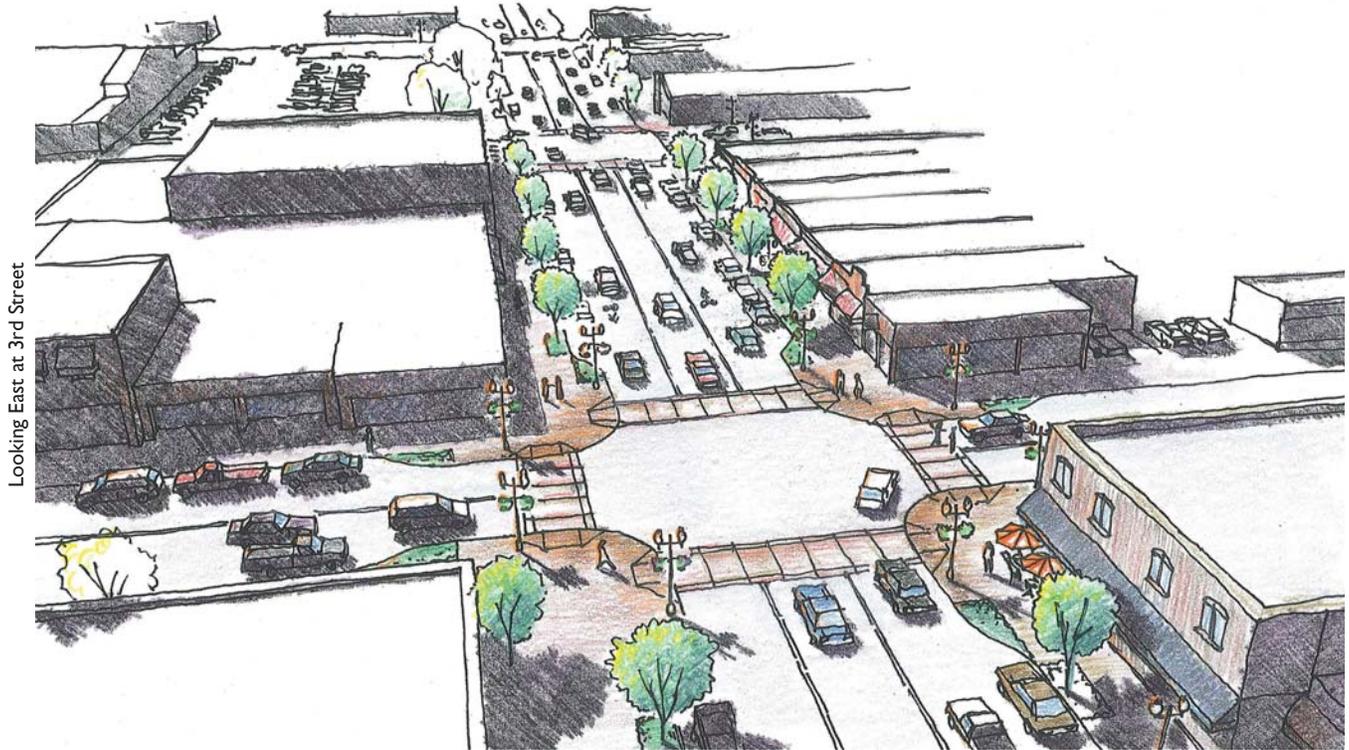
Alternative C - Mid-block Improvements with 3-Lane Street



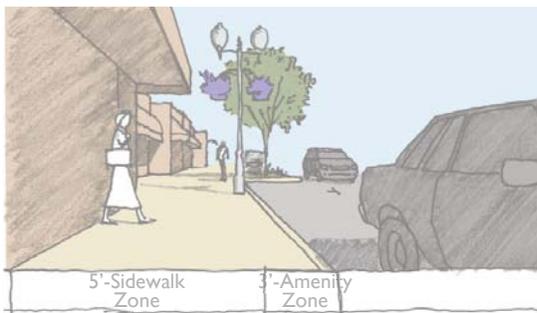
Improvements:

- Ornamental Street Lights
- Sharrow Markings
- New and Wider Sidewalks
- Street Trees
- Enhanced Sidewalk Planting
- Sidewalks with Cafe Tables and Benches
- Green Street Stormwater Features (Optional)

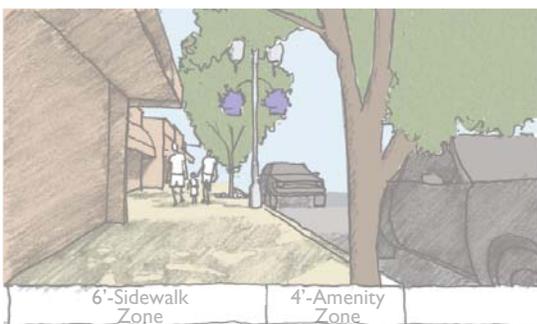
ALTERNATIVE C - AXONOMETRIC



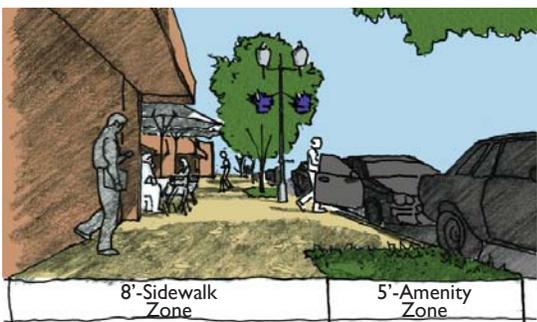
Looking East at 3rd Street



Sidewalk Zone - Alternative A



Sidewalk Zone - Alternative B



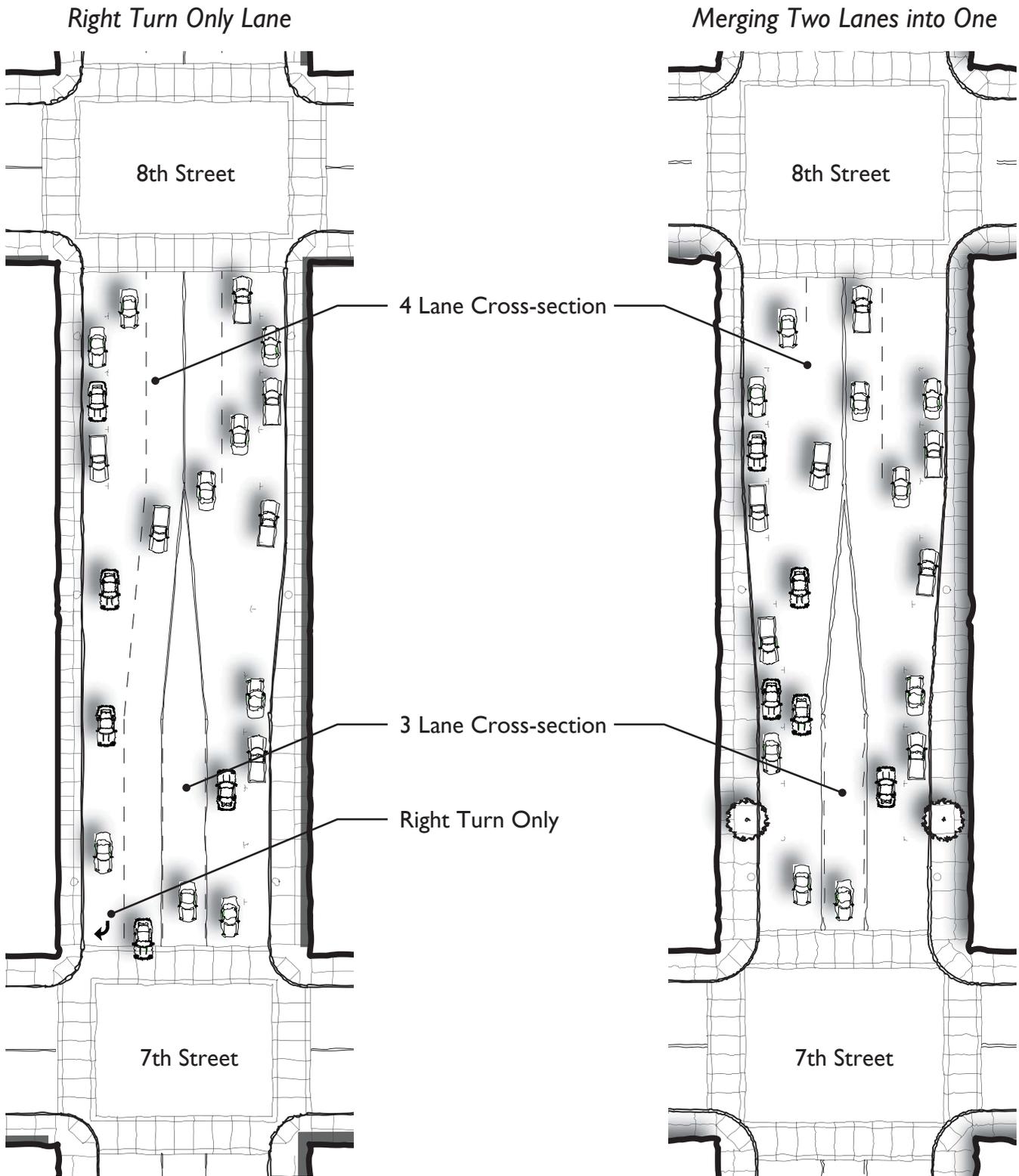
Sidewalk Zone - Alternative C



Sidewalks at full Main Street widths

This sidewalk width supports the full range of streetscape features, pedestrian movement, and storefront activities typical of a vibrant Main Street. The sidewalk character, coupled with the narrower roadway, can reasonably be expected to result in slightly slower vehicle travel speeds through this part of downtown.

ALTERNATIVE C - TRANSITION FROM 3 TO 4 LANES

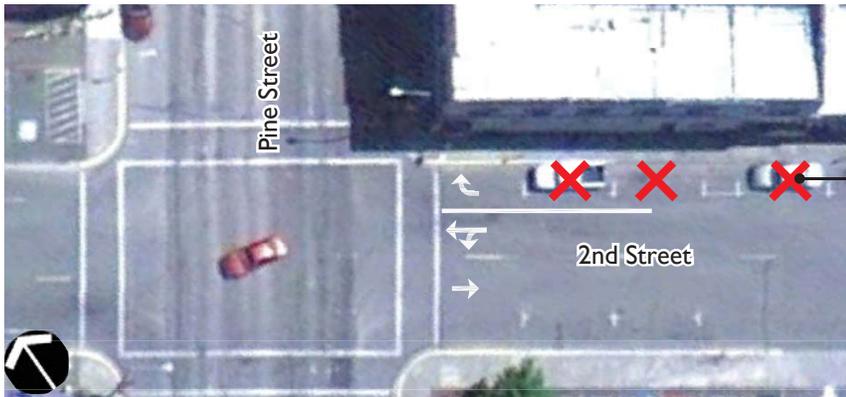


A full block length is required to make the transition from four vehicle travel lanes (four-lane configuration) to two vehicle travel lanes with a continuous third lane for turning and queuing for turns (three-lane configuration). The transition block can be handled with one of two options.

It could be designed with a right turn only lane at 7th Street, or by merging the two travel lanes into one. It is important the transition occurs over this block so drivers are fully accustomed to new driving conditions when they arrive at the next intersection.

2ND STREET ROADWAY AND TRAFFIC OPERATIONAL IMPROVEMENTS

Alternatives A, B & C



Remove 3 parking spaces and add northbound right turn lane

*Reduces delay

*Shortens queues

Additional improvements to vehicle operations can be achieved through removal of the existing traffic signal at 3rd Street, installation of a new signal at 2nd Street, and coordination of all signals on Pine Street.

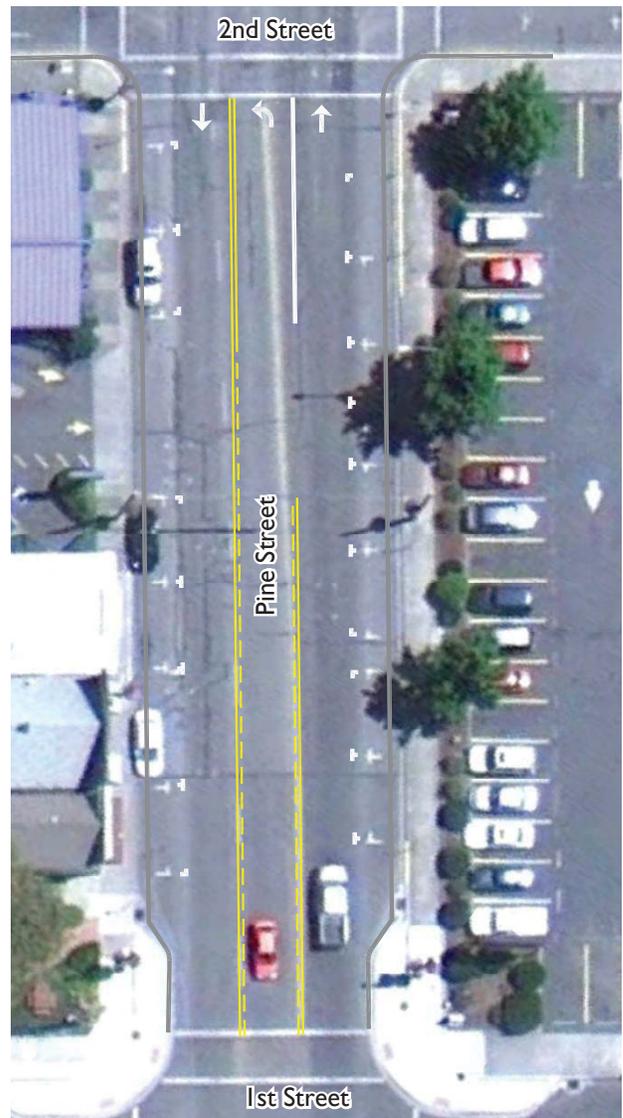
2ND STREET ROADWAY AND TRAFFIC OPERATIONAL IMPROVEMENTS

Alternatives A & B



Lengthen eastbound through and add lane at 2nd
*Increases queue storage for eastbound traffic.
*Reduces risk of queue “spillback.”

Alternative C



Reconfigure lanes to a 3-lane cross-section

PLAZA



The Second Street Plaza was originally conceived while developing the Central Point Downtown Revitalization Plan. At one community workshop there was a strong consensus that a small plaza along Pine Street, adjacent to Ray's Food Place, would be a very desirable amenity. Ray's deli is busy during the lunch hour so a place for outdoor dining and a focal gathering point seemed plausible. The Plaza was designed into the public right-of-way to minimize the loss of on-street parking, and to retain all of the grocery store's parking. Landscaping, street furniture, art, and shade structures were all envisions for the site.

ECONOMIC BENEFITS OF A GOOD MAIN STREET

Streetscape enhancement projects often signify a public investment in the revitalization of downtown and support for a healthy business community. The direct relationship between urban design, such as streetscape enhancement projects, and economic activity is complex, and it can be difficult to isolate physical design changes as a stand-alone economic factor. However, when streetscape enhancement is part of larger strategy that couples design with promotion, organization and economic structuring the positive outcomes are far more apparent. The best of strategies will, of course, struggle during periods of economic downturn as have been felt over the past few years. Nevertheless, good community planning should be ready to take advantage of an upturn in financial and economic conditions.

Understandably, questions about the economic return from Main Street investments and strategies are often raised. The citizens and business community of Central Point are no exception, and that question has been raised. The Plan will address that question more fully by providing an opportunity for a community dialogue based on understanding the comparable experiences of other communities, and setting realistic expectations for Central Point. However, a few things are worth noting as part of a discussion of these streetscape alternatives.

Main Streets have always been about commerce. They should provide one of the best business addresses in town. A recent University of Washington research project used visual preferences and on-site surveys of shoppers to assess the influence of attractive streetscapes, especially streetscapes with a strong element of trees, on four factors critical to business growth in downtowns and central business districts. The research included smaller communities with populations of 10,000 to 20,000. That study, along with multiple other studies, concluded that streetscape qualities do affect the favorable perception of those factors.

Visual Quality. Images of business districts with tidy sidewalks, a cohesive streetscape design with trees, and attractive buildings consistently receive the highest preference ratings. Multiple studies have

indicated a strong preference among consumers for a balance between more human activity and natural elements such as trees and architecture. This correlation of shopping preferences is highest where there is a cohesive core of historic or older commercial and mixed-use buildings representing the communities' architectural heritage, as well as sufficient businesses, buildings, and density to be effective, compact, and pedestrian-friendly. This has been true even in districts with numerous historic buildings along a Main Street.

Place Perceptions. Attractive streetscape create more favorable expectations about the shopping experience. Consumer surveys consistently demonstrate favorable expectations are associated with amenities such as street trees and benches, wide sidewalks, and a few pocket parks for socializing over a beverage from the local coffee shop. These expectations begin before consumers enter a shop and tends to influence their assessment of the experience while inside the store, including the product value.

Shopper Patronage. Several patronage variables increase when associated with attractive streetscapes. Shoppers are willing to come from a greater distance and to stay longer once there. They are also more likely to be return shoppers. From a Pine Street perspective, this can be part of increasing the customer base and attracting a certain amount of regional attention if the right mix of businesses are in place.

Product Pricing. There are three general types of goods and services - convenience goods, shopping goods that are compared, and specialty goods. Research suggests the relative values of shopping and specialty goods increases within the environment of an attractive streetscape.

Reinvestment and Employment. Data available from the Main Street programs of many states indicates a strong correlation between comprehensive Main Street revitalization efforts and private reinvestment in the downtown area. That investment takes form in new businesses, expansion of existing businesses, and storefront improvements.

ECONOMIC BENEFITS OF A GOOD MAIN STREET

This is typically accompanied by increased retail sales volumes, commercial property values, tax receipts, and the number of jobs downtown.

Community Wide Benefits. Research also suggests downtown revitalization can help create new economic activity across a larger part of the community. This often also correlates to new jobs and new housing opportunities.

The Main Street Approach

Many communities across the country have adopted a Main Street program of some kind. In Oregon, 76 communities participate in the Oregon Main Street Program. This approach is generally focused on four factors considered critical to success.

Organization. Establish consensus and cooperation through partnerships among stakeholders in the downtown or central business district. If everyone is moving toward the same goal, a wide number of perspectives, reflecting a broad cross-section of the community, can be accommodated. Good organization divides the workload and clearly delineates public sector and private sector responsibilities.

Promotion. Promotion can take many forms, but the goal should be to create a positive image for downtown. That positive image will help build consumer and investor confidence, communicate what is unique, and help sell the promise of the downtown Central Point of the future.

Design. Design matters in making a Main Street all it can be. An enhancement project for Pine Street would certainly be a major piece of the needed design work, creating a pleasant environment for shopping and for working while conveying a positive message about what Central Point has to offer. Design attention should also be paid to storefronts, window displays, parking areas, and public spaces.

Economic Restructuring. Retaining and supporting existing businesses is absolutely critical. Along with that, it is also important over time to consider ways to diversify and restructure the mix of businesses in downtown, including converting

unused or under-used properties. A broad and well-balanced mix of commerce helps boost the profitability for all and sharpens everyone's merchandising skills. Goals must be based upon an understanding of today's consumer, and on a good assessment of consumer changes to come.

Urban Renewal. Downtowns play an important role in a community's economic development strategy. They often account for as much as 30 percent of employment, and 40 percent of the tax base. The City of Central Point has implemented an Urban Renewal District to strengthen the economic and aesthetic vitality of the Downtown and East Pine Street Corridor Area. Streetscape improvements along Pine Streets is one of the identified projects for the district.



The shopping experience



The display window

Project descriptions and an opinion of probable costs have been provided for Alternatives A through C as illustrated on the preceding pages. The opinion of costs is intended to guide funding strategies to implement a preferred alternative for East Pine Street improvements, once a preference has been determined. The estimates of cost (see page 30) include probable construction costs of the key elements, a construction cost contingency, and estimates of mobilization and erosion control, construction survey, and temporary traffic control based on a typical percentage of construction costs. For Alternatives B and C, an allowance for utility adjustments within the right-of-way has been made, as well as for meeting stormwater treatment requirements likely to be triggered by the reconstruction of impervious surfaces (e.g. roadway and sidewalks).

All costs are expressed as 2012 dollars. No costs have been included for on-going operation and maintenance costs, nor have soft costs for design and engineering been included.

Alternative A - 1st Street through 6th Street

Retains the Existing 4-Lane Cross-Section

Although the existing sidewalks would be reconstructed throughout, this alternative requires no reconstruction of the existing curbs. It assumes there would be no significant re-pavement of the roadway other than the specially paved crosswalks. The mid-block bulb-outs would be constructed with separate curbs, allowing stormwater to flow behind them through narrow gaps. The highest cost streetscape elements would be the ornamental street lighting and the construction of mid-block bulb-outs with street trees and landscaping.

Estimated Construction Budget in 2012 Dollars

\$1.6M - \$1.7M

Alternative B - 1st Street through 6th Street

Modified 4-Lane Cross-Section to Reduce Travel Lane Widths

This alternative includes new sidewalks and curbs, along with new bulb-outs at three intersections. The curb line on each side of the street would be moved two-feet into the existing road surface. That would likely require partial to complete roadway reconstruction in the affected blocks, along with adjustments to the existing utilities and meeting stormwater treatment requirements. Allowances for those costs have been made. Ornamental street lighting and crosswalk costs would be roughly the same as Alternative A, but a more extensive street tree planting has been assumed given the wider sidewalks.

Estimated Construction Budget in 2012 Dollars

\$2.1M - \$2.2M

Alternative C - 1st Street through 8th Street

Reconfiguration from 4-Lane Cross-Section to a 3-Lane Cross-Section

This alternative assumes new curbs, sidewalks and roadway work similar to Alternative B, but extending for an additional two blocks in order to allow for the four-lane to three-lane transition between 8th Street and 7th Street. The three-lane segment would begin at 7th Street. Cost assumptions for re-pavement and stormwater treatment are similar to Alternative B. Ornamental street lighting would include two mid-block ornamental street lights rather than a single mid-block light as in Alternatives A and B. Consequently, the lighting cost is higher.

Estimated Construction Budget in 2012 Dollars

\$2.9M - \$3.0M

PLANNING-LEVEL COSTS

7th Street through 10th Street Improvements

Improvements in this segment consist of incrementally widening the existing sidewalk frontage through right-of-way acquisition or easements in the front yard setback of properties. These improvements could be completed on a property-by-property basis if redevelopment or building expansions occur, or as publicly-funded capital projects. For informational purposes a probable lineal foot cost for frontage improvements has been included.

Estimated Construction Budget in 2012 Dollars

\$5,500 - \$6,000 per 100 LF

Potential Phasing

The following approaches to phasing for the East Pine Street improvements could be considered.

Alternatives A and B. The improvements between 1st Street and 6th Street could be constructed in two separate phases. A Phase I project could be 1st Street through 4th Street, which corresponds to the current downtown core, with the greatest density of business activity and continuous building fronts along the. Most participants in the walking tour conducted as part of this project said their feeling of being “downtown” was strongest in these blocks. Since corner bulb-outs are not recommended for the intersection of 4th Street, construction could be terminated at either the west or east side of the intersection without creating a dangerous misalignment of curbs.

Phase II project would complete the improvements from 4th Street through the 6th Street intersection, making sure the curb bulb-outs were constructed on both sides of the intersection in order to facilitate safe vehicle and bike movements through the intersection.

6th Street to 10th Street Sidewalk Improvements.

These improvements would likely be constructed as opportunities arise along individual property frontages. They would be phased as complete and continuous capital improvements project.

Alternative C. This alternative does not lend itself to phasing since it involves the reconfiguration of the roadway between 1st Street and 7th Street from a 4-lane cross-section to a 3-lane cross-section. The transition block from 8th Street to 7th Street is also required at this time to maintain vehicle safety.

Second Street Plaza. If Alternative A, with no reconstruction of existing curbs, is the preferred alternative, this plaza could be implemented at any time as a separate project. It would only require the removal of on-street parking spaces. The other alternatives require moving and reconstructing the existing curbs. The plaza project could not be completed prior to completion of the streetscape project. However, once the streetscape project was completed, with the new curbs in place, the plaza could be completed later as a separate project.

Streetscape Enhancement Budgets

The following estimates of construction costs are based on the conceptual illustrations of streetscape enhancements in Alternatives A through C. They do not include the probable incremental costs for sidewalk improvements only between 7th Street and 10th Street (see previous page). They do not include any costs associated with traffic signalization changes. All costs reflect 2012 dollars.

East Pine Street

Revised Streetscape Enhancement Budgets

Alternative A				
4-Lane with existing cross-section	Qty	Unit	Unit Cost	Total Cost
Demolition and Clearing	1	LS	\$30,585.60	\$30,585.60
Mobilization and Erosion Control	1	LS	\$122,342.40	\$122,342.40
Construction Survey	1	LS	\$20,390.40	\$20,390.40
Temporary Traffic Control	1	LS	\$30,585.60	\$30,585.60
Curb 'Bulb-Out'	12	EA	\$25,000.00	\$300,000.00
Concrete Sidewalks, 8' width	16,800	SF	\$5.50	\$92,400.00
Concrete Paver Crosswalks	9,840	SF	\$18.00	\$177,120.00
Street Trees and Associated Landscape	12	EA	\$1,000.00	\$12,000.00
Street Lights	50	EA	\$8,000.00	\$400,000.00
Signing and Striping	1	LS	\$8,000.00	\$8,000.00
Drainage and Utility Adjustment Allowance	1	LS	\$30,000.00	\$30,000.00
Total:				\$1,223,424.00
Construction Contingency 30%:				\$367,027.20
Alt A Total:				\$1,590,451.20
Cost Per Block:				\$320,000.00

Alternative B				
4-Lane with modified cross-section (narrower lanes)	Qty	Unit	Unit Cost	Total Cost
Demolition and Clearing	1	LS	\$33,089.40	\$33,089.40
Mobilization and Erosion Control	1	LS	\$132,357.60	\$132,357.60
Construction Survey	1	LS	\$22,059.60	\$22,059.60
Temporary Traffic Control	1	LS	\$33,089.40	\$33,089.40
Roadway Surface	1,400	LF	\$100.00	\$140,000.00
Curb 'Bulb-Out'	12	EA	\$15,000.00	\$180,000.00
Curb and Gutter	1,050	LF	\$20.00	\$21,000.00
Concrete Sidewalks, 10' width	21,000	SF	\$5.50	\$115,500.00
Concrete Paver Crosswalks	9,360	SF	\$18.00	\$168,480.00
Street Trees and Associated Landscape	40	EA	\$1,000.00	\$40,000.00
Street Lights	50	EA	\$8,000.00	\$400,000.00
Signing and Striping	1	LS	\$8,000.00	\$8,000.00
Drainage and Utility Adjustment Allowance	1	LS	\$30,000.00	\$30,000.00
Total:				\$1,323,576.00
Construction Contingency 30%:				\$397,072.80
Alt B Total:				\$1,720,648.80
Cost Per Block:				\$340,000.00

Alternative C				
3-Lane reconfiguration	Qty	Unit	Unit Cost	Total Cost
Demolition and Clearing	1	LS	\$44,152.80	\$44,152.80
Mobilization and Erosion Control	1	LS	\$176,611.20	\$176,611.20
Construction Survey	1	LS	\$29,435.20	\$29,435.20
Temporary Traffic Control	1	LS	\$44,152.80	\$44,152.80
Roadway Surface	1,680	LF	\$100.00	\$168,000.00
Curb 'Bulb-Out'	12	EA	\$15,000.00	\$180,000.00
Curb and Gutter	1,260	LF	\$20.00	\$25,200.00
Concrete Sidewalks, 13' width	32,760	SF	\$5.50	\$180,180.00
Concrete Paver Crosswalks	8,640	SF	\$18.00	\$155,520.00
Street Trees and Associated Landscape	40	EA	\$1,000.00	\$40,000.00
Stormwater Treatment Planters	5,443	SF	\$20.00	\$108,860.00
Street Lights	72	EA	\$8,000.00	\$576,000.00
Signing and Striping	1	LS	\$8,000.00	\$8,000.00
Drainage and Utility Adjustment Allowance	1	LS	\$30,000.00	\$30,000.00
Total:				\$1,766,112.00
Construction Contingency 30%:				\$529,833.60
Alt C Total:				\$2,295,945.60

I-5 INTERCHANGE 33 AREA MANAGEMENT PLAN

The ongoing I-5 Interchange 33 Interchange Area Management Plan (IAMP) project is considering multiple alternatives to preserve and enhance the long-range safety and efficiency of travel through the Pine Street interchange area. The alternatives analysis is focusing on four areas for consideration within the Interchange 33 influence area:

- **Enhanced Network** – This network incorporates most of the improvements identified in the East Pine Street Plan, which are not currently included in the financially-constrained list of projects in the 2009-2034 Regional Transportation Plan (RTP).
- **Interchange Improvements** – These concepts identify potential improvements that address deficiencies at the interchange ramps that would still remain with the Enhanced Network. Seven different interchange improvement alternatives are being considered, ranging from ramp modifications to full interchange reconstruction.
- **West Side Improvements** – These concepts focus on the 10th Street/Freeman Road intersection and identify potential improvements to address deficiencies remaining with the Enhanced Network. The concepts build on the downtown Pine Street 4-lane and 3-lane design option work that has been done to-date, and focus on the area between the southbound ramp terminal and the 10th Street/Freeman Road intersection. Four West Side Improvements alternatives are being considered, which mainly include increasing the capacity of the 10th Street/Freeman Road intersection by either adding turning lanes or restricting side street movements and rerouting traffic down Oak and Manzanita Streets to a new signal on Pine Street at 7th Street.
- **East Side Improvements** – These concepts identify potential improvements east of the interchange ramp terminals that would still remain with the Enhanced Network. Three East Side Improvements alternatives are being considered that focus mainly on Hamrick and Table Rock Roads.

While a preferred alternative has not yet been adopted, early feedback may be pointing to an alternative that includes the following elements:

- Add second northbound to eastbound right-turn lane on northbound off-ramp. Could consider a management policy to focus on safety of northbound off-ramp at expense of East Pine Street operations, potentially until funding is available for physical improvements.
- Add second westbound to southbound left-turn lane on East Pine Street and widen southbound on ramp to have two receiving lanes. Could be initial phase without bridge widening and long-term plan for longer lane with widened bridge. Could consider a management policy to focus on safety of southbound off-ramp at expense of East Pine Street operations, potentially until funding is available for physical improvements.
- Add sidewalk on south side of East Pine Street between ramps by restriping travel lanes and replacing railing on south side to allow for one foot of additional width between rails across bridge. Could consider some roadway widening at either end of bridge to reduce lane narrowing off the structure. Should consider aesthetic aspects of railing replacement and fencing on both sides of bridge.
- Add second westbound to southbound left-turn lane on East Pine Street at Freeman Road and widen Freeman Road to have two southbound receiving lanes through Oak Street. Restripe East Pine Street to eliminate one of the eastbound through lanes to minimize widening.

Most alternatives being considered for the interchange area, including the anticipated preferred alternative, would not affect the design alternatives for Pine Street through the downtown. However, the West Side Improvements alternatives that include restricting turning movements at the intersection on Pine Street with Freeman Road/10th Street and rerouting traffic down Oak Street and Manzanita Street to a new signal at 7th Street could significantly impact the effectiveness of the design alternatives recommended for Pine Street.

One impact that should be carefully considered with any IAMP alternative that prohibits turning movements on Pine Street at 10th Street/Freeman Road is the potential conflict with the recommendation to create safe and comfortable bike routes on Oak and Manzanita Streets from 1st Street to 8th Street in lieu of constructing bike lanes on Pine Street. The IAMP alternatives that reroute traffic down Oak and Manzanita Streets will increase traffic on the bike routes, increasing conflicts and compromising the function of these streets as good biking alternatives to Pine Street.

Motor vehicle operations along Pine Street could also be significantly altered if a new traffic signal were installed at 7th Street. While this alternative has not been modeled, a new signal at this location could change vehicle queuing patterns and overall travel times from one end of Pine Street to the other.

Given the potential of some alternatives to significantly impact traffic operations and safety along Pine Street in the downtown area, further consideration should be given to the compatibility of alternatives between these two planning efforts prior to selecting preferred alternatives for Pine Street and the I-5 interchange area.